Appendix A1

Draft Design Plans

DESIGN	

ABBR.	DESCRIPTION	ABBR.	. [ESCRIPTION	ON
AH	AHEAD	ABU1		ABUTMENT	
AZ	AZIMUTH	AOBE	_		BY ENGINEER
BK	BACK	ASPI	_	SPHAL T	
BRG	BASELINE BEARING	BD)	_	OUNDARY BUILDING	
c	CENTERLINE	BN	_	ENCH MARK	<u> </u>
CS	CURVE TO SPIRAL	CC	+	ENTER TO	
е	SUPERELEVATION RATE (CROSS SLOPE)	CON	: c	ONCRETE	
EO	EQUALITY	CONST	_	ONSTRUCTI	
EXT	EXTERNAL CONTROL LINE	CF	_	OUNTY ROA	
HCL HSD	HORIZONTAL CONTROL LINE HEADLIGHT SIGHT DISTANCE	DM	_	ieed dista Direct mea	
1.35 L	LENGTH OF CIRCULAR CURVE	DWI	_	RIVEWAY	SUNEMENT
LS	LENGTH OF SPIRAL	EF	_	DGE OF PA	VEMENT
LVC	LENGTH OF VERTICAL CURVE	ES	6 E	DGE OF SH	10ULDER
E	CENTER CORRECTION OF VERTICAL CURVE	FEE	_	EE ACQUIS	
f	MAIN LINE	FEE WO/A	_		ITION WITHOUT ACCESS
PC PI	POINT OF CURVATURE POINT OF INTERSECTION	FF F(_	ENCE POST OUNDATION	
POL	POINT ON LINE	FL	_	ENCE LINE	
PSD	PASSING SIGHT DISTANCE	GAF	_	ARAGE	
PT	POINT OF TANGENT	GF	₹ 0	RAVEL	
PVC	POINT OF VERTICAL CURVE	HC	_	iouse	
PVI	POINT OF VERTICAL INTERSECTION	HW1	_	IIGHWAY	
PVT R	POINT OF VERTICAL TANGENT RADIUS	IF	_	<u>RUN PIN UF</u> IAILBOX	R IRON PIPE
SC	SPIRAL TO CURVE	MON	_	ONUMENT	
SSD	STOPPING SIGHT DISTANCE	N&V	_	IAIL AND W	ASHER
ST	SPIRAL TO TANGENT	00	3 0	RIGINAL GF	ROUND
STA	STATION	0/1	_	VERHEAD	
To	TANGENT LENGTH		_	ARCEL	
TGL	THEORETICAL GRADE LINE TANGENT TO SPIRAL	PAV1	_	PAVEMENT	FACEMENT
VC	VERTICAL CURVE	PED POLE	+	PERMANENT EASEMENT PEDESTRIAN POLE	
	TOPOGRAPHY (DRAINAGE)	ρ	_	ROPERTY L	
4000		POF	₹ P	ORCH	
ABBR.	DESCRIPTION	RF	_	AILROAD	
BB	BOTTOM OF BANK (STREAM) BOTTOM OF CURB	RTE	_	OUTE	AV
BC BO	BOTTOM OF CORB BOTTOM OF OPENING	ROV RV		RIGHT OF W RETAINING N	
CAP	CORRUGATED ALUMINUM PIPE	Si		TATE HIGH	
СВ	CATCH BASIN	SHLDF	_	HOULDER	
CIP	CAST IRON PIPE	SP	_	PIKE	
c STRM	CENTERLINE OF STREAM	S1		TREET	
CMP CP	CORRUGATED METAL PIPE CONCRETE PIPE	STA	_	TAKE	
CSP	CORRUGATED STEEL PIPE	SW		IDEWALK	
CULV	CULVERT	TE		EMPORARY	EASEMENT
DIA	DIAMETER	TC	ז כ	EMPORARY	OCCUPANCY
DMH	DRAINAGE MANHOLE	U/C	_	INDERGROUN	ID
DS	DRAINAGE STRUCTURE PIPE	WV	۷ W	ING WALL	
D'XING EHW	DITCH CROSSING EXTREME HIGH WATER	_			1
EL	ELEVATION WHITEN		_	andard	ITEM PAYMENT UNI
ELEV	ELEVATION			MBOL ANS)	ESTIMATE OF
ELW	EXTREME LOW WATER		(FL	HIJO	QUANTITIES SHEET
ES	END SECTION		•		-
HW	HEADWALL				LF M
INV	INVERT MANHOLE		fto		MI SF
MHW	MEAN HIGH WATER		YDc		SY
OHW	ORDINARY HIGH WATER		AC		AC
OLW	ORDINARY LOW WATER	[YDr		CY
RCP	REINFORCED CONCRETE PIPE		GAL		GAL
SICPP	SMOOTH INTERIOR CORRUGATED POLYETHYLENE PIPE	-	lb TON	<u> </u>	LB
TB	TOP OF BANK (STREAM) TOP OF CURB	L	TON	1	TON
TG	TOP OF GRATE		ſ	AC-DIJUT D	EVICIONS
VCP	VITRIFIED CLAY PIPE		J	AS-BUILT R DESCRIPTION	N OF ALTERATIONS:
_		•			
			- 1		

ALIGNMENT

- 1	I LMI OMAINI I	LHOLINI		w	WH				
TO	TEMPORARY (OCCUPANCY		х					OF THE ABOVE CANNOT
U/G	UNDERGROUN)				DEFINE(MADE	AT	THE	TIME THE EXPLORATION
WW	WING WALL]	13	MHUE			
	STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	NOME	VALENT NCLATURE: CS/PROPOS					
	•	-	INCHE	S					
	•	LF	LINEA	R FEET					
	m1	MI	MILES	1					
	ftq	SF	SOUAF	RE FEET					
	YDq	SY	SQUAF	re yard					
	AC AC ACRES		3						
	YDr CY CUBIC		YARD						
	GAL	GAL	GALLO	ON					
Г	lb	LB	POUND)					
	TON	TON	TON						

TOPOGRAPHY (MISCELLANEOUS)

UTILITIES

GAS SERVICE BOX (HOUSE LINE)

DESCRIPTION

ELECTRIC MANHOLE

GAS VALVE (MAIN LINE)

LOW PRESSURE GAS

TRAFFIC CONTROL BOX

SANITARY SEWER

TELEPHONE POLE

TELEPHONE MANHOLE

WATER SERVICE BOX (HOUSE LINE)

SUBSURFACE EXPLORATION

WATER VALVE (MAIN LINE)

REPLACE ABBREVIATION "AB" WITH:

DN 4 INCHES CASED DRILL HOLE

RP 1 INCH SAMPLER (RETRACTABLE PLUG)

TO BE DEFINED AT THE TIME OF EXPLORATION

FH HOLLOW FLIGHT AUGER PA POWER AUGER

PT PERCOLATION TEST HOLE

ABBREVIATION "C" IN CATEGORIES: DA, DM, DN, AND FH WITH:

CABLE TELEVISION

DESCRIPTION

CP CONE PENTROMETER DA 21/4 INCHES CASED DRILL HOLE

ELECTRIC

GUY POLE

HYDRANT

LIGHT POLE

POWER POLE

SMH SANITARY MANHOLE

TELEPHONE

TELBOX TELEPHONE BOX

WATER

AH HAND AUGER

DM DRILLING MUD

SP SEISMIC POINT TP TEST PIT

PH PROBE

B BRIDGE

C CUT

D DAM

F FILL

W WALL

K CULVERT

STORM SEWER

GAS

ABBR.

GP

GSB

G۷

HYD

LP

LPG

PP

SA

ST T

TCB

TEL P

TMH

CTV

w

WSB

W۷

	EETS			
SHEET NUMBER	DESCRIPTION		DRAWING NUMBER	
1	1 INDEX AND ABBREVIATIONS			
2-3	LEGEND, LINE AND POINT SYMBOLOGY		LEG-1 TO LEG-2	
4-19	TYPICAL SECTIONS		TYP-1 TO TYP-16	
20-28	20-28 MAINTENANCE ROW PLANS			
29-37	29-37 EXISTING PLANS			
38-121	38-121 GENERAL PLANS			
122-148	122-148 ROADWAY PROFILES			
149-155	49-155 LANDSCAPE PLANS			
156-164	156-164 REMOVAL PLANS			
165-173	TUNNEL UTILITY PLANS		TU-1 TO TU-9	
174-177	77 CENTER WALL ELEVATION PLANS		TN-1 TO TN-4	
178	NORTH PORTAL ELEVATION PLAN	TN-5		
179	TYPICAL TUNNEL SECTION AT JET FAN LOCATION PLAN		TN-6	
180	80 ITS DMS LOCATION PLAN		ITS-1	
181-187	181-187 BRIDGE PLANS			

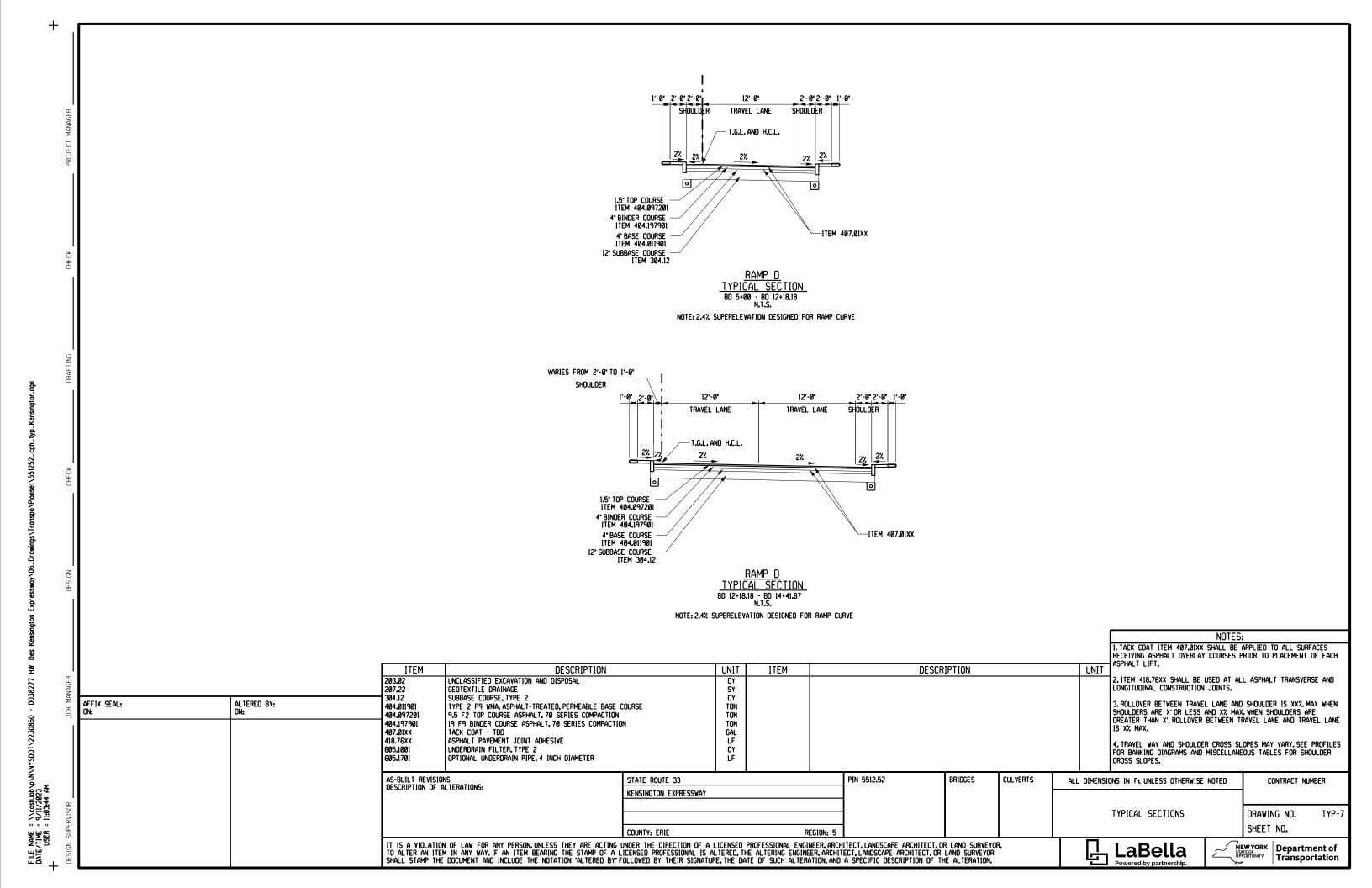
AC DULLT DEVICIONS	1	Low	Indiacto	CHI VEDTO		
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		PIN	BRIDGES	CULVERTS	ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED	CONTRACT NUMBER
		1			INDEX AND ABBREVIATIONS	
		1			THOEX HID HOUNE THI TORS	DRAWING NO. IND-1
	COUNTY	+				SHEET NO.
	COUNTY: REGION:					0.121
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A L SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" F	LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHI	TECT, LANDSCAPE ARCHITECT, OR	LAND SURVEYOR	R,	LaBella Powered by partnership.	NEW YORK STATE OF OPPORTUNITY. Department of Transportation

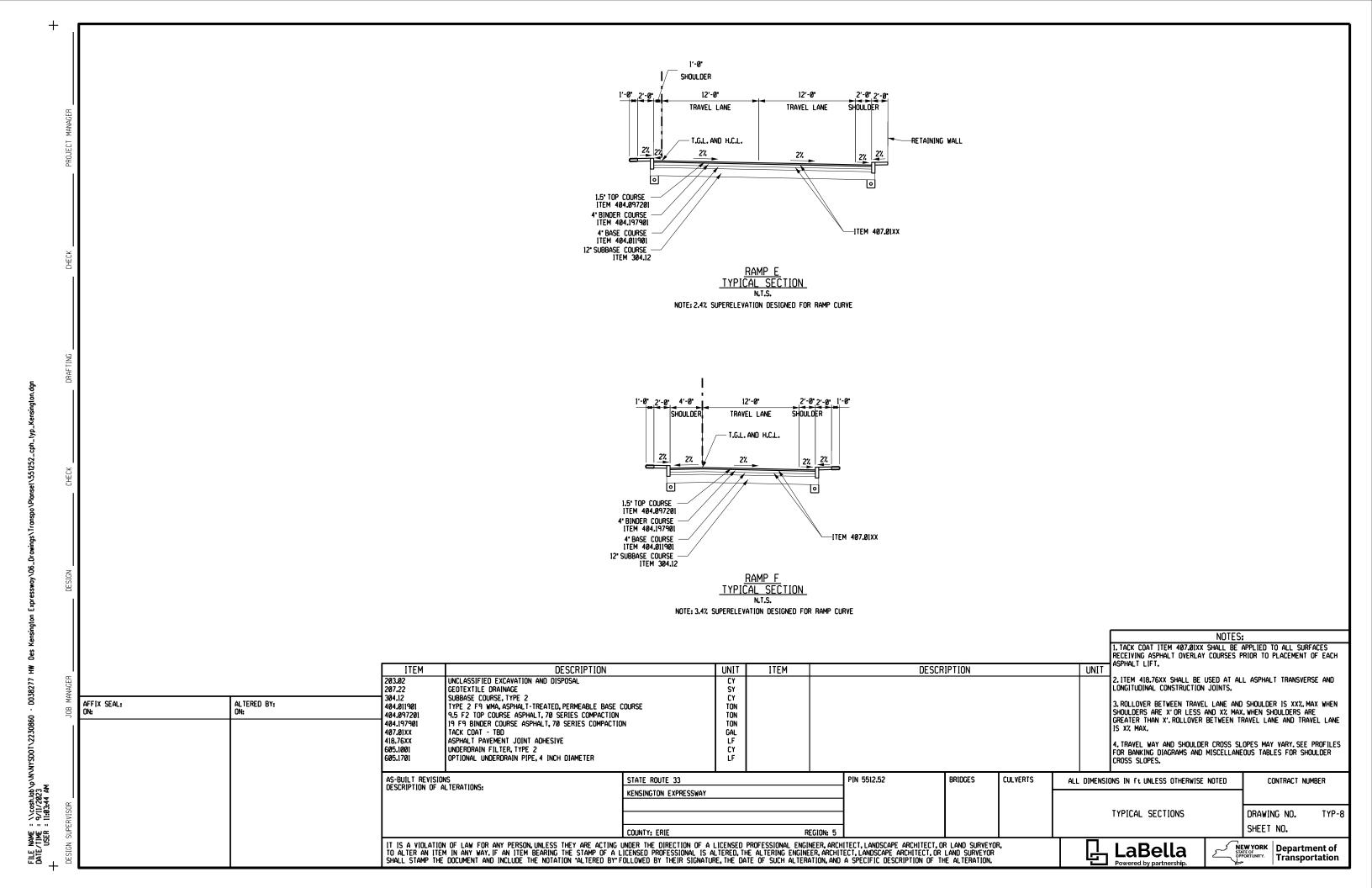
ROADWAY **ALIGNMENT** LANDSCAPE TRAFFIC WORK ZONE STYLE NAME DESCRIPTION STYLE NAME DESCRIPTION STYLE NAME DESCRIPTION BARRIER, TEMPORARY, W/ WARNING CONTROL (CENTERLINE) L ABL AREA, BRUSH LINE CZ RCZ_P CLEAR ZONE **DE TOUR** TWZCD_P CHANNELIZING DEVICE I AHR AREA, HEDGE ROW GUIDE RAIL, MISCELLANEOUS PAVEMENT MARKING REMOVAL OR TWZPMRC_ TRANSITION CONTROL AT_P LAPB AREA. PLANTING BED $\neg \neg$ \neg RGB GUIDE RAIL, BOX BEAM UTILITIES **BRIDGE** LAWA AREA, WOODED AREA OUTLINE **RGBM** GUIDE RAIL, BOX BEAM, MEDIAN —п-___ ___ LAWE AREA. WATERS EDGE GUIDE RAIL, CABLE STYLE NAME DESCRIPTION RAIL <u> —</u>Б **-**□-CONDUIT, UNDERGROUND SHEET PILING **BSHT** LCUT_P CUT LIMIT RGCB GUIDE RAIL, CONCRETE BARRIER RGP_F CONTROL LFILL_F FILL LIMIT 0 0 OC -CONDUIT, OVERHEAD $-\boxtimes$ RGW GUIDE RAIL. W BEAM **BASELINE** LFNC ELECTRIC LINE, UNDERGROUND *********** I TRC TREE ROW, CONIFEROUS **RGWM** GUIDE RAIL, W BEAM, MEDIAN BASELINE, PROJECTION ELECTRIC LINE. HANGING DRAINAGE L TRD TREE ROW, DECIDUOUS PARKING BUMPER UE0 ELECTRIC LINE, OVERHEAD WALL. H PILE RAIL ROAD, CATENARY DCF CULVERT PIPE ELECTRIC TRANSMISSION, OVERHEAD RRER RAIL ROAD, 3RD RAIL WALL. RETAINING CULVERT PIPE (DIR) $\times \times \times \times \times$ ELECTRIC, SUBSTATIONS OOOOOOOO LWS WALL, STONE RRPLS_P RAIL, PHOTO, LARGE SCALE FIBER OPTIC, UNDERGROUND DDG_P DITCH. GRASS LINED **ROW MAPPING** FIBER OPTIC, HANGING RRPSS RAIL, PHOTO, SMALL SCALE DDP_P DITCH, PAVED INVERT - OF O -UF 00 FIBER OPTIC, OVERHEAD RRS RUMBLE STRIP PE EASEMENT, EXISTING GAS. UNDERGROUND DDS_F DITCH. STONE LINED RRSLS_P RAIL, SURVEY, LARGE SCALE EASEMENT, PERMANENT FLOW LINE RRSSS EASEMENT, PERMANENT, APPROX. RAIL, SURVEY, SMALL SCALE OG UGO GAS. OVERHEAD DSSD SLOTTED DRAIN EASEMENT, TEMPORARY SIGNS UIC INFORM CABLE, UNDERGROUND DUD_P UNDERDRAIN ATE -META_F EASEMENT. TEMPORARY, APPROX. **BILLBOARDS** INFORM CABLE, HANGING ENVIRONMENTAL MULTIPLE POST FEE ACQUISITION, W/ ACCESS FEE OIL LINE, UNDERGROUND S **EBLHS** BALE, STRAW SS0 STRUCTURE, OVERHEAD AFEE MF AP FEE ACQUISITION. APPROXIMATE =====OIL LINE, HANGING CURTAIN, TURBIDITY MFS_P FEE ACQUISITION, SHAPE SSOC STRUCTURE. OVHD. CANTILEVER POLE, BRACE, PUSH BRACE 000000 EDMC DAM, COFFER MF WOA FEE ACQUISITION, W/O ACCESS **STRIPING** FEE W/OA POLE, GUY WIRE EDMEC_P DAM, EARTHEN CHECK HISTORICAL, ACQUISITION STB. USA SANITARY SEWER, UNDERGROUND STDB. DOUBLE BROKEN LINE HIGHWAY BOUNDARY HB SANITARY SEWER, HANGING USAH EDMGSC_P DAM, GRAVEL BAG/SAND BAG CHECK HIGHWAY BOUNDARY, APPROX. STDL • MHBA - AHB DOTTED LINE LONG USAF SANITARY SEWER, FORCE MAIN, UGND EDMPC_P DAM, PREFABRICATED CHECK MHBW HWY BOUNDARY, FACE OF WALL STDS. DOTTED LINE SHORT SANITARY SEWER, FORCE MAIN, HANG FULL BARRIER LINE MHBWOA HIGHWAY BOUNDARY, W/O ACCESS STFB. HR W/OA TELEPHONE. UNDERGROUND EDMSC_P DAM, STONE CHECK MJC JURISDICTION, CIT' HATCH LINE UTH TELEPHONE, HANGING MJCY JURISDICTION, COUNTY PARTIAL BARRIER LINE TELEPHONE, OVERHEAD **EFNSV** FENCE, SILT & VEGETATION JURISDICTION, HISTORIC DISTRICT MJHD STRCT ROUNDABOUT, CAT TRACKS IITV - CTV CABLE TV. UNDERGROUND FENCE, VEGETATION ********* MJLL JURIS., (GREAT, MILITARY) LOT LINE STRYL ROUNDABOUT, YIELD LINE -]CTV[-CABLE TV. HANGING EWAA_F WETLAND, ADJACENT AREA JURISDICTION, NATION STOP BAR UTV0 CABLE TV. OVERHEAD -OCTV WETLAND, FEDERAL MJPB JURISDICTION, PUBLIC LANDS STSE • SOLID, EDGE UUU UNKNOWN. UNDERGROUND WETLAND, FEDERAL AND STATE MJS JURISDICTION, STATE STXL X WALK, LADDER LINE UNKNOWN. HANGING E WM WETLAND, MITIGATION AREA JURISDICTION, TOWN UUO UNKNOWN. OVERHEAD 01111-EWS WETLAND, STATE STXLB X WALK, LADDER BAR LINE M.IV JURISDICTION, VILLAGE WATER LINE, UNDERGROUND • = W (WHITE) OR Y (YELLOW) PROPERTY LOT LINE WATER LINE, HANGING TRAFFIC CONTROL PROPERTY LOT LINE, APPROXIMATE OW WATER LINE, OVERHEAD 0 TCSW SIGNAL, SPAN WIRE 1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED). SUB LOT LINE 2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.). 3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDINGLINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER AS-BUILT REVISIONS BRIDGES **CUL VERTS** (0.015 in ON B SIZE DRAWINGS). ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER DESCRIPTION OF ALTERATIONS: MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF LEGEND TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS. LINE SYMBOLOGY DRAWING NO. LEG-1 FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT SHEET NO. HAVE CORRESPONDING EXISTING FEATURES. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR. TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. _aBella NEW YORK | Department of Transportation

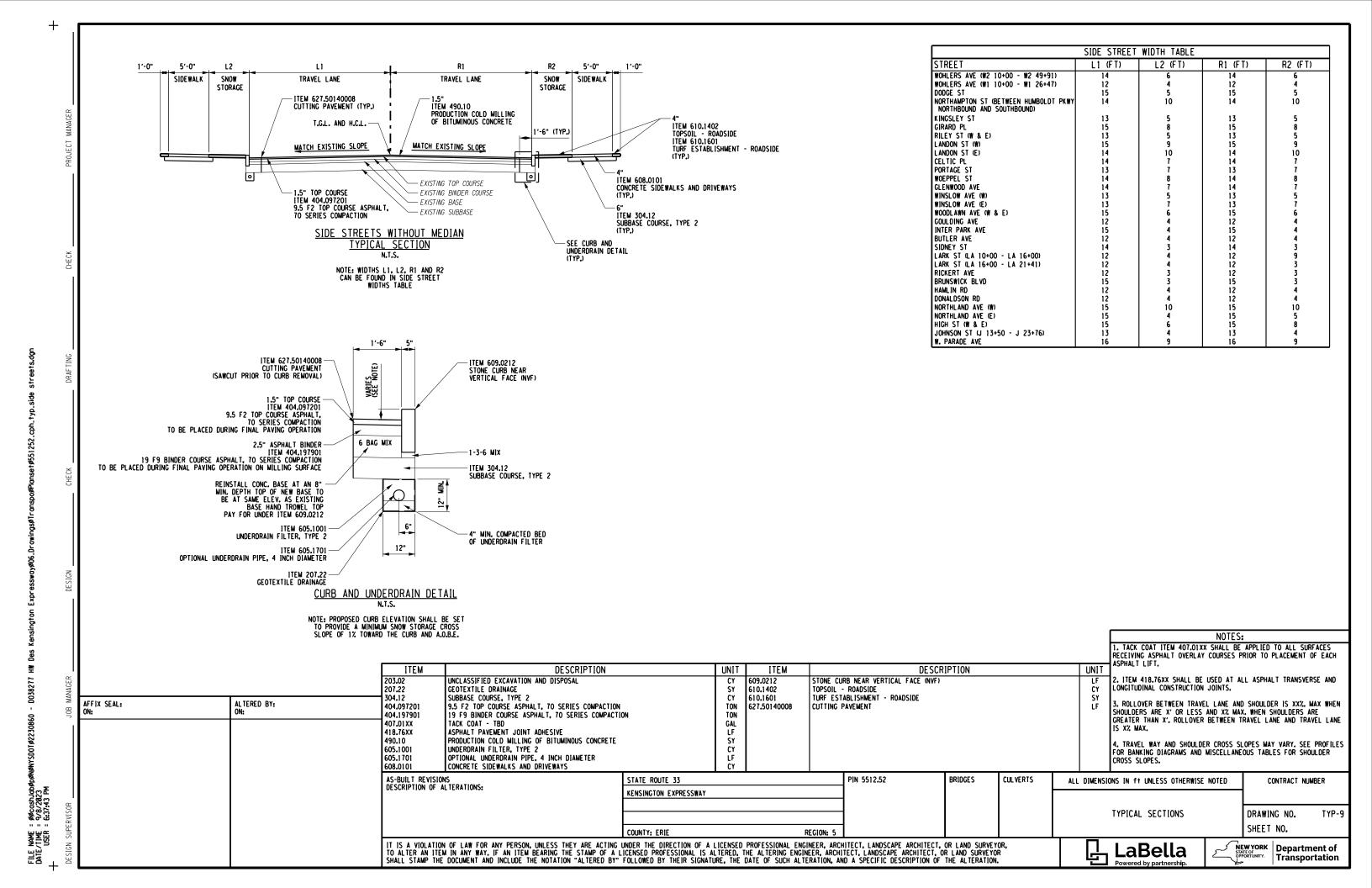
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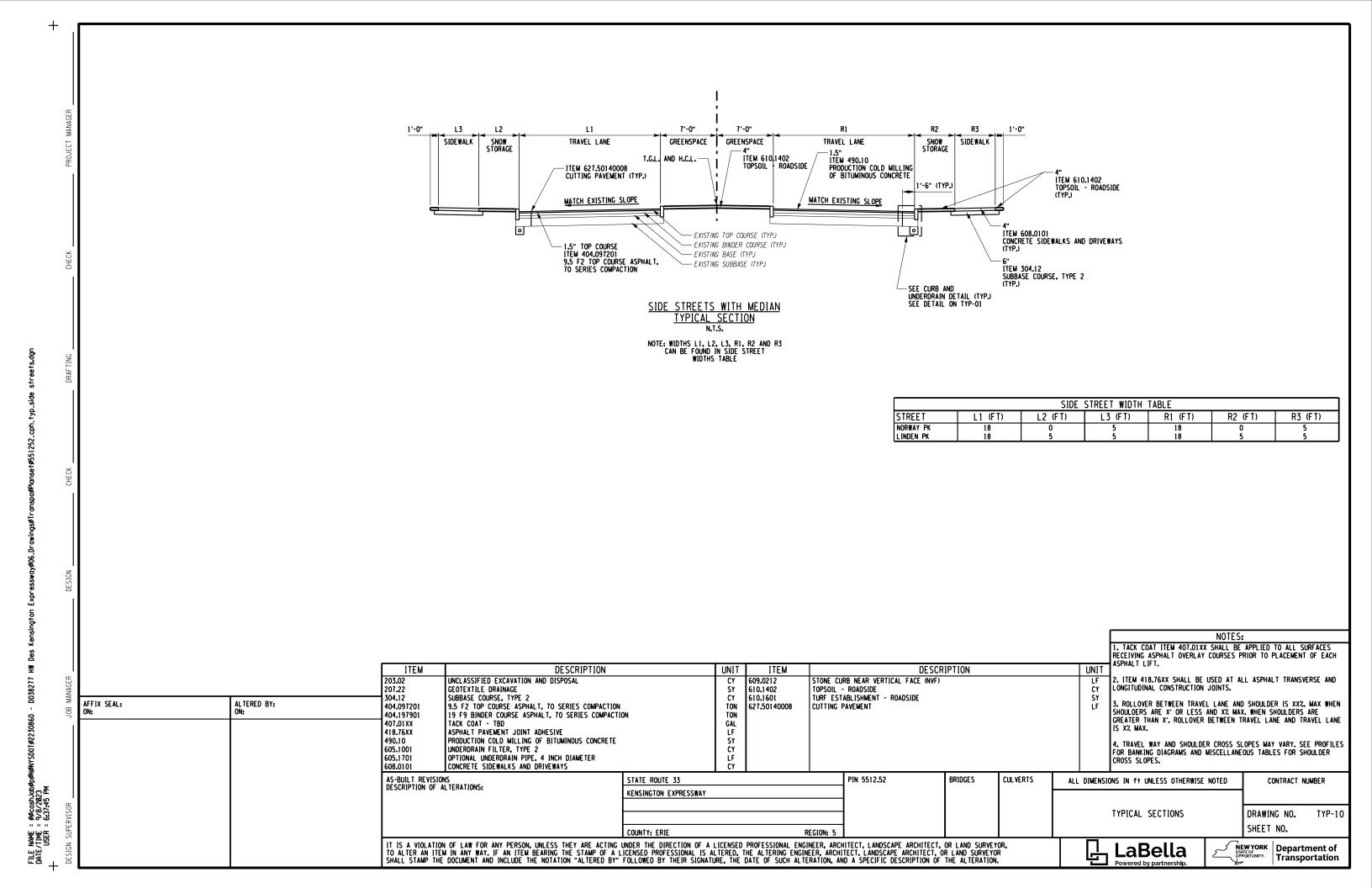
ALIGNMENT DRAINAGE ITS ROW MAPPING SIGNS UTILITIES CELL NAME DESCRIPTION CELL NAME CELL NAME DESCRIPTION CELL DESCRIPTION CELL DESCRIPTION CELL NAME DESCRIPTION DESCRIPTION NAME ΙΝΔΜΕ \oplus ACC CENTER OF CURVATURE **(**) Œ) ELECTRIC. BOX DINV INVERT -⟨∆}-IANT P ANTENNAS MDL 1 P DEED LINE. TYPE 1 -∳-SINGLE POST HER S_P Ε **ACOGO** \mathbb{A} **IASCTS** 2 SINGLE POST, PROPOSED UEM ELECTRIC, METER ACCOU. SPEED/COUNT SNSR.S MDL 2P DEED LINE, TYPE 2 STRUCTURE, RECTANGULAR (6) Р (E) ACS CURVE TO SPIRAL 3 SB_F BACK TO BACK, PROPOSED LIEMH ELECTRIC, MANHOLE **ICARPAD** CABINET & PAG MDI 3P DEED LINE. TYPE 3 STRUCTURE, INVERT DSI Δ ADPI_P DETOUR, POINT OF INTERSECT. \bigoplus Φ UFPT ELECTRIC. POLE. TRANS. \square k ICCTV CCTV SITE MDL 4P DEED LINE, TYPE 4 SDEI DELINEATORS DSM STRUCTURE. MANHOLE) COPD (0 ADPL_P DETOUR, POINT ON LINE ICDPD CDPD TRANSCEIVER 9 MDL 5P DEED LINE, TYPE 5 \bigoplus SPM PARKING METER G LIGM GAS. METER STRUCTURE, MANHOLE, DSMTXX TYPE "XX" \odot **AEQN** EQUATION 0 (G) ICELL CELL PHONE TOWER MEEP EASEMENT. EXISTING RFMSRM REFERENCE MARKERS LIGMH GAS. MANHOLF "XX" = 48, 60, 72, 96 (A) **FQUATION AHEAD** AF QNAHD **ICJB** CONDUIT JACK OR BORING **(A)** MEPAP_F EASEMENT, PERM., APPROX. SRSC3 SHLD, CTY, 123 DIG. -⟨Ĝ>-**UGLM** GAS, LINE MARKER DSR STRUCTURE, ROUND B AEQNBK **EQUATION BACK** \boxtimes **ICNTL CAB** CONTROLLER CABINET 0 MEPP_P EASEMENT, PERM., BACK LINE SRSC4 SHLD, CTY, 4 DIG. FΡ LIGP GAS/FUEL PUMP STRUCTURE, RECT., WITH CURB \odot TYPE "X **AEVT EVENT STATION** \bigcirc 0 Ω ICPR MEPSP. SRSCT2 SHLD, CTY TOUR, 1-2 DIG UGV COMMUNICATION PULL BOX EASEMENT, PERM., SHAPE GAS. VALVE 0 APC POINT OF CHRYATHRE $-\otimes$ ICTD CONDUIT TURNING DOWN ♦ MF AP_F FEE ACQUISITION, APPROX. SRSCT4 SHLD, CTY TOUR, 3-4 DIG. ∞ UGVT GAS. VENT STRUCTURE, RECT., TYPE "X" \Box APCC POINT OF COMPOUND CURVATURE "X" = I, K, L, M, O, P, U \odot ICTU MFP_P SRSI SHLD. INTERSTATE \bigcirc ULP LIGHTING, POLE —⊙ CONDUIT TURNING UF 0 FEE ACQUISITION. BACK LINE ΔPI POINT OF INTERSECTION)@(\Box Э-О-С Α ICVTR1 COMM. VEH. ROAD TRANSCEIVER MFSP_F FEE ACQUISITION, SHAPE SRSN2 SHLD, NATIONAL, 2 DIG **ULPM** LIGHTING, POLE, MEDIAN ENVIRONMENTAL APOB POINT OF BEGINNING IDF F AUI W MHRAF HIGHWAY BNDRY .. APPROX SRSN3 SHLD, NATIONAL, 3 DIG. **(UL PP** LIGHTING, POLE, PED. CUL V FIOP P STR., INLET, OUTLET PROT. POINT OF CURVATURE \odot APOC ΕZ • O SRSS2 SHLD. STATE, 2 DIG. UMFC MISC. FILLER CAP IF 7R E-ZPASS READER MHRCP HISTORICAL, BLDG, CORNERS \bigcirc APOF POINT OF FND Δ **IEZTR** TRANSMITTAL READER MHRE HIGHWAY BNDRY, PT. SRSS3 SHLD, STATE, 3 DIG **-**ô> **UOL M** OIL. LINE MARKER (B) EIPGB_P STR., INLET PROT., GRAVEL BAG \bigcirc \odot **APOL** POINT ON LINE (0) **IFOXCAB** FIBER OPTIC X-CONNECT CABINET MJCP PT., JURIS. CITY SRSS4 SHLD. STATE, 4 DIG POLE. WITH UTILITY H/S EIPHS_P STR., INLET PROT., HAY/STRAW \odot APOS POINT ON SPIRAL • MPBC PT. BUILDING CORNER \odot UPD POLE, DEAD (NO UTILITY) IFUSSPL FUSION SPLICE TRAFFIC CONTROL \odot APOT 0 POINT ON TANGENT IHARADV HAR ADVISORY SIGN MPCC PT., CROSS CUT (Эна UPL POLE. WITH LIGHT PRFB EIPP_P STR., INLET PROT., PREFAB. TCBJ BOX. JUNCTION APOVC POINT ON VERTICAL CURVE 位 \wedge **IHARST** HAR SITE MPDH PT., DRILL HOLE \bigcirc USME SANITARY SEWER MANHOLE BOX. PULL BOX (SF) EIPSF_P STR., INLET PROT., SILT FENCE \times **APOVT** POINT ON VERTICAL TANGENT * P Α MPF HTR TELEPHONE, BOOTH ILC LOAD CENTER PT. FENCE LOCATION TCBS BOX. SPLICE **APORC** POINT ON REVERSE CURVE 0 MPIP **◆** UTLM TELEPHONE, LINE MARKER IMECSPI MECHANICAL SPLICE PT., IRON PIPE **ERCB** RISER, CONCRETE BOX TCMC MICROCOMPUTER CABINET (0) ΔPT POINT OF TANGENCY PM) 0 \overline{T} HTMH TELEPHONE, MANHOLE IMSCS PORT, SPEED & COUNT SENSOR MPIR PT., IRON ROD \triangle ETRS_F TRAP. SEDIMENT PED POLE **(B)** APVC POINT OF VERTICAL CURVATURE **-**�> CABLE TV. LINE MARKER M [] IMSCTS MICRO SPEED & COUNT SENSOR MPM PT. MONUMENT UTVLM WETLAND FLAG TCSH SIGNAL HEADS \blacksquare **APVCC** POINT OF VERT. CMPND CURVE \bigcirc UTVPB Δ (M) IMI MICROWAVE TRANSCEIVER **МРММ** PT., MONUMENT, MISC. CABLE TV. PULL BOX \odot TCSP SIGNAL POLE GEOTECHNICAL **APVI** POINT OF VERT. INTERSECTION Ø VMS PERM. OVERHEAD VMS MPN PT., NAIL \Box UUB **IOVHVMS** UNKNOWN, BOX TRAFFIC WORK ZONE APVRC POINT OF VERT, REVERSE CURVE Θ GDH DRILL HOLE * Δ PA] **IPASCS** PORT. ACCOU. SPD & CNT. SENSOR **MPRS** PT., RAILROAD SPIKE \boxtimes UNKNOWN, JUNCTION BOX UUJB (0) APVT POINT OF VERTICAL TANGENCY **IPEDS** PEDESTRIAN SIGNAL HEAD ₩ MPSP PT., SPIKE TW7AP P \otimes UNKNOWN, MANHOLE UUMH LANDSCAPE **(** ASC SPIRAL TO CURVE \Diamond \bigcirc **IPSS** MPST PT., STAKE TWZAPC F ARROW PANEL, CAUTION MODE UUPB PAVEMENT SURFACE SENSOR UNKNOWN, PULL BOX LELS **ELEVATION. SPOT** SPIRAL POINT OF INTERSECTION **ASPI** \triangle PVMS ⊗ MPTW PT., TREE W/ WIRE ••• TWZAPT_P ARROW PANEL, TRAILER OR SUPPORT IPVMS PERM, VMS UUVL UNKNOWN. VALVE LEP FLAG POLE ASTS SPIRAL TO SPIRAL \odot IRM + PT., WALL LOCATION TWZBCD_P BARRICADE (TYPE III) RAMP METER MPWI \bigcirc UUV1 UNKNOWN, VENT MAILBOX \otimes AST SPIRAL TO TANGENT ∕\ RWI IRWIS RDWY WEATHER INFO. SENSOR TWZCMS_F CHANGEABLE MESSAGE SIGN (PVMS 0 UUW UNKNOWN, WELL **ROW ACQUISITION** LPB PAPER BOX \otimes ATS TANGENT TO SPIRAL <u>-ò</u>;-ISP TWZFLG_P FLAGGER α WATER, FIRE HYDRANT HWEH (}}) 0 LPST POST, SINGLE MFS_P_T FEE ACQUISITION AVEVT VERTICAL EVENT POINT TWZFT_P Δ ISST SPREAD SPECT, TRANSCEIVER FLAG TREE W UWM WATER. METER 0 LRB ROCK. BOULDER IMPACT ATTENUATOR / AVHIGH VERTICAL HIGH POINT \odot ITOB TELEPHONE DEMARCATION BLK TWZIA_P W UWMH WATER, MANHOLE CRASH CUSHION (TEMPORARY) MEPS_P_ EASEMENT, PERMANENT LSHC SHRUB, CONIFEROUS \odot AVLOW VERTICAL LOW POINT SUBSURFACE TEMP. PROBE TWZLUM_F LUMINAIRE (TEMPORARY) UWV WATER, VALVE () LSHD SHRUB, DECIDUOUS METS_P_T EASEMENT, TEMPORARY ➾ TWZSDT F SYMBOL, DIRECTION OF TRAFFIC **(W)** HWW WATER, WELL IVTRT VEHICLE TO ROWY TRANSCEIVER BRIDGE 꺘 LTC TREE. CONIFEROUS SYMBOL. DIRECTION OF TEMPORARY IWIMD WEIGHT IN MOTION DETECTOR TWZSDTD_ W/M METS_P_ OCCUPANCY, TEMPORARY BSC BRIDGE, SCUPPER LTD TREE, DECIDUOUS TWZSGN_P SIGN (TEMPORARY IWVR WIRELESS VIDEO REPEATER \bigcirc TREE, STUMP CONTROL MFS_P_T FEE ACQUISITION W/O ACCESS SIGNAL, TRAFFIC OR PEDESTRIAN \mathbb{V} TWZSIG_P IWVRC WIRELESS VIDEO RECEIVER TEMPORARY) Ø TREE, WELL OR WALL CBP IWVTT WIRELESS VIDEO TRANSMITTER മ TWZWL_P WARNING LIGHT Δ BASELINE. POINT ROADWAY LUKP UNKNOWN POINT \odot CBPOL BASELINE, POINT ON LINE TWZWV_P WORK VEHICLE RES P ELEVATION, SPOT WORK VEHICLE WITH TRUCK CBSP BASELINE, SPUR POINT TWZWVA_P MOUNTED ATTENUATOR \boxtimes RGA GUIDE RAIL, ANCHOR \Leftrightarrow CBTP BASELINE, TIE POINT \bigcirc 1. SEE NOTES ON DRAWING LEG-1. RGP GUIDE POST, SINGLE . CPBM BENCHMARK 1 CPH POINT, HORIZ, PHOTOGRAMMETRY AS-BUILT REVISIONS BRIDGES **CUL VERTS** ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER DESCRIPTION OF ALTERATIONS: **(** CPSM POINT, SURVEY MARKER, PERM. LEGEND CPSV POINT, VERT., PHOTOGRAMMETRY POINT SYMBOLOGY DRAWING NO. LEG-2 SHEET NO. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR. TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. LaBella NEW YORK | Department of Transportation

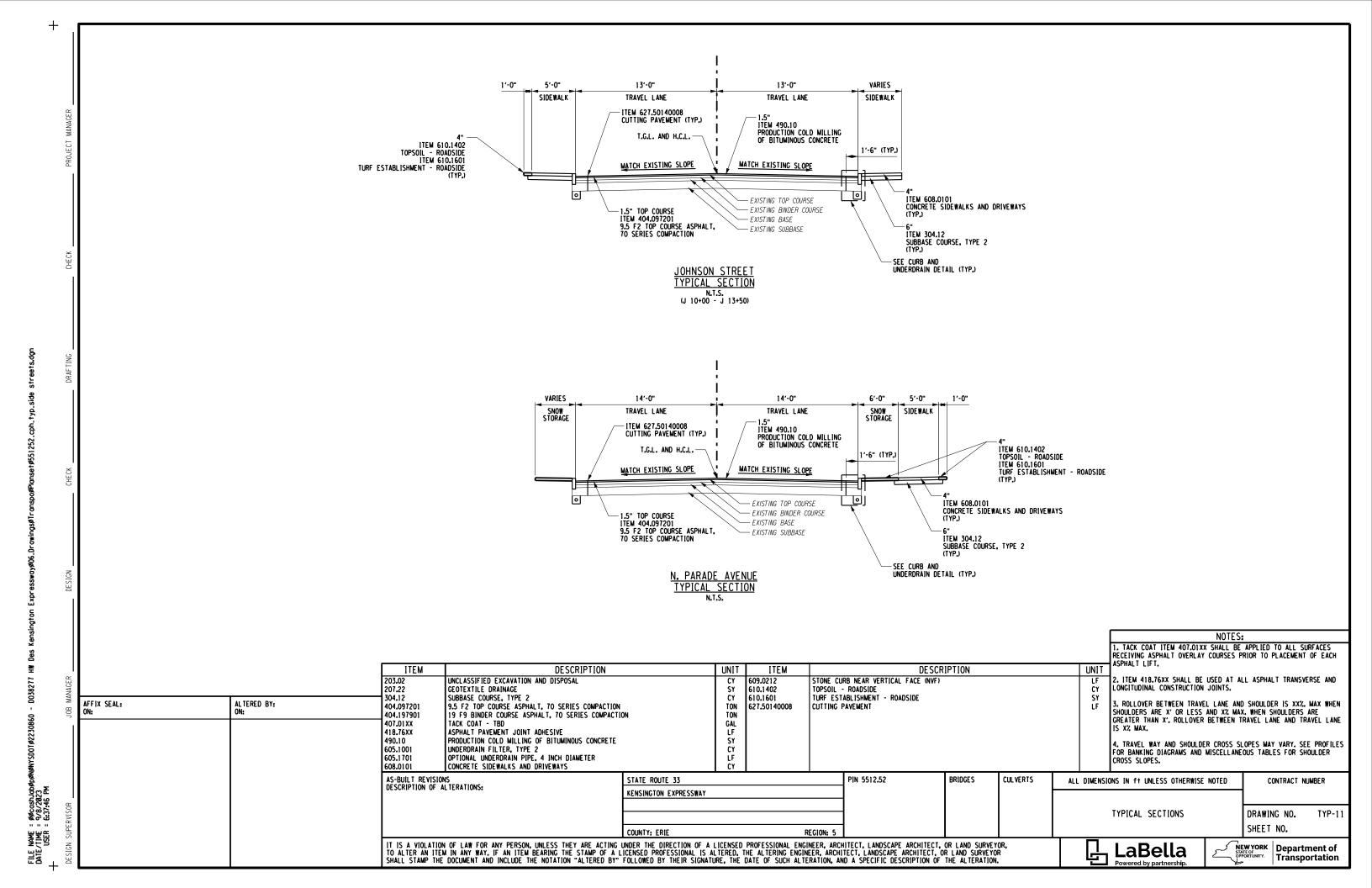
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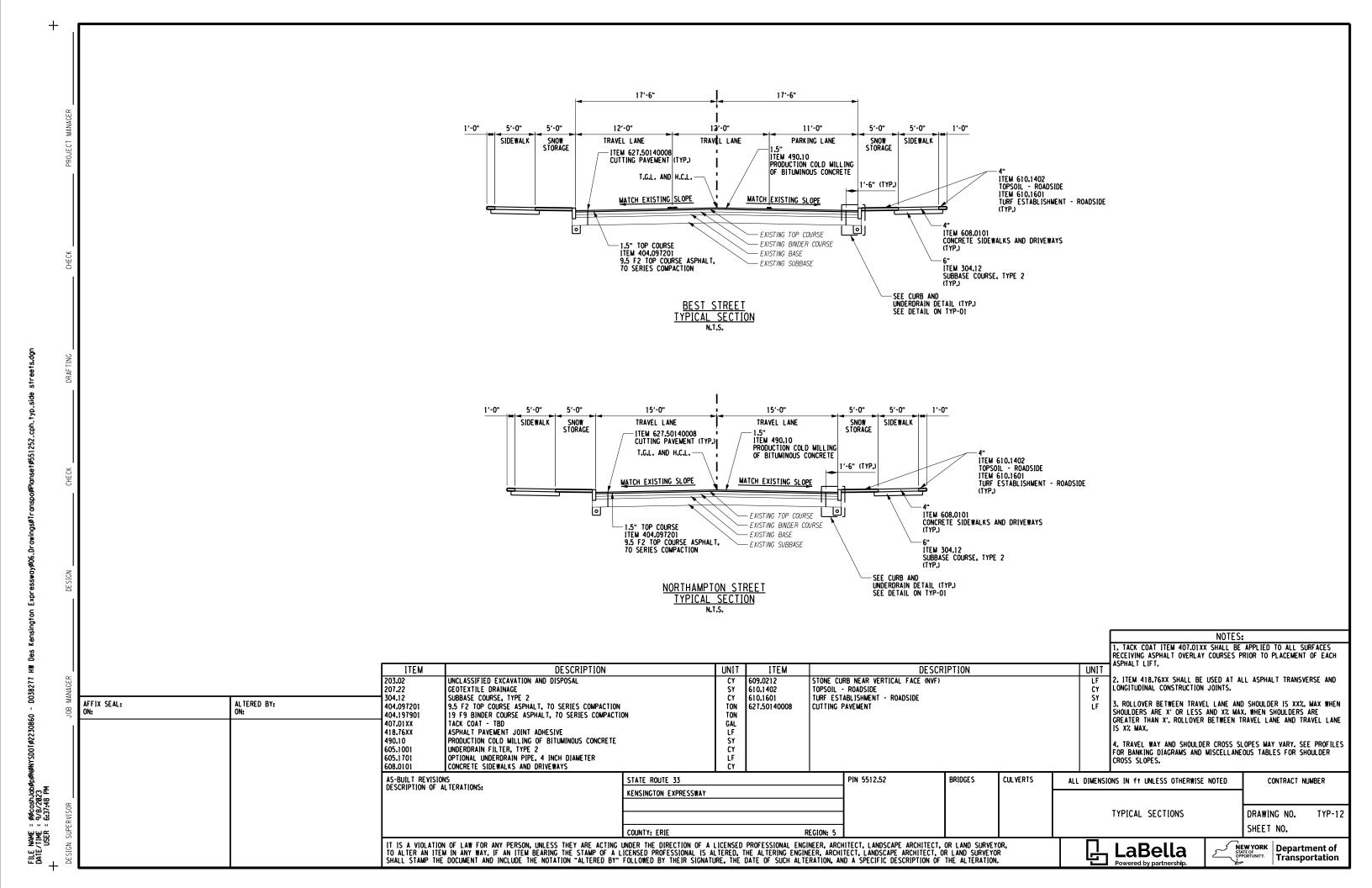


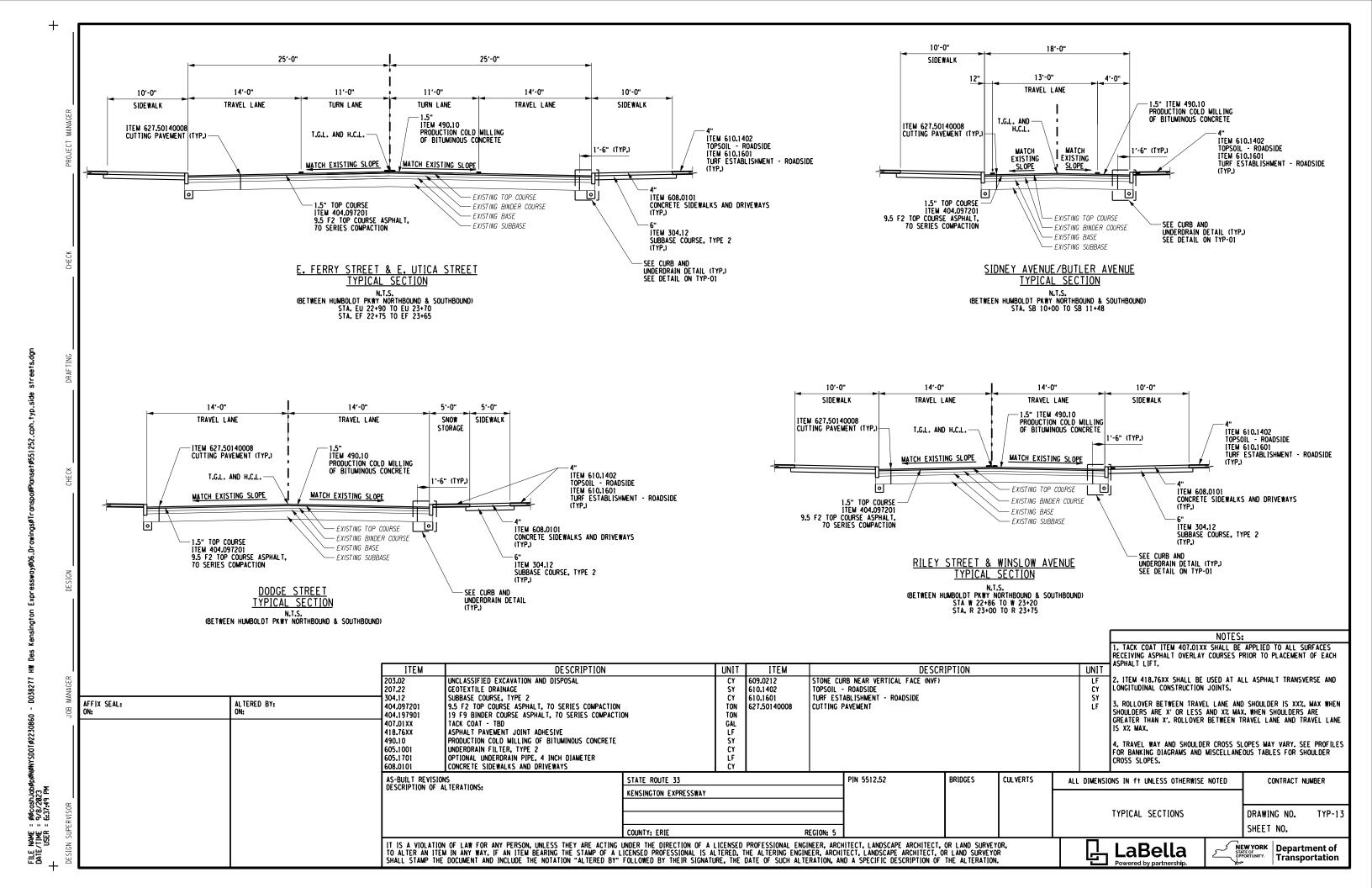


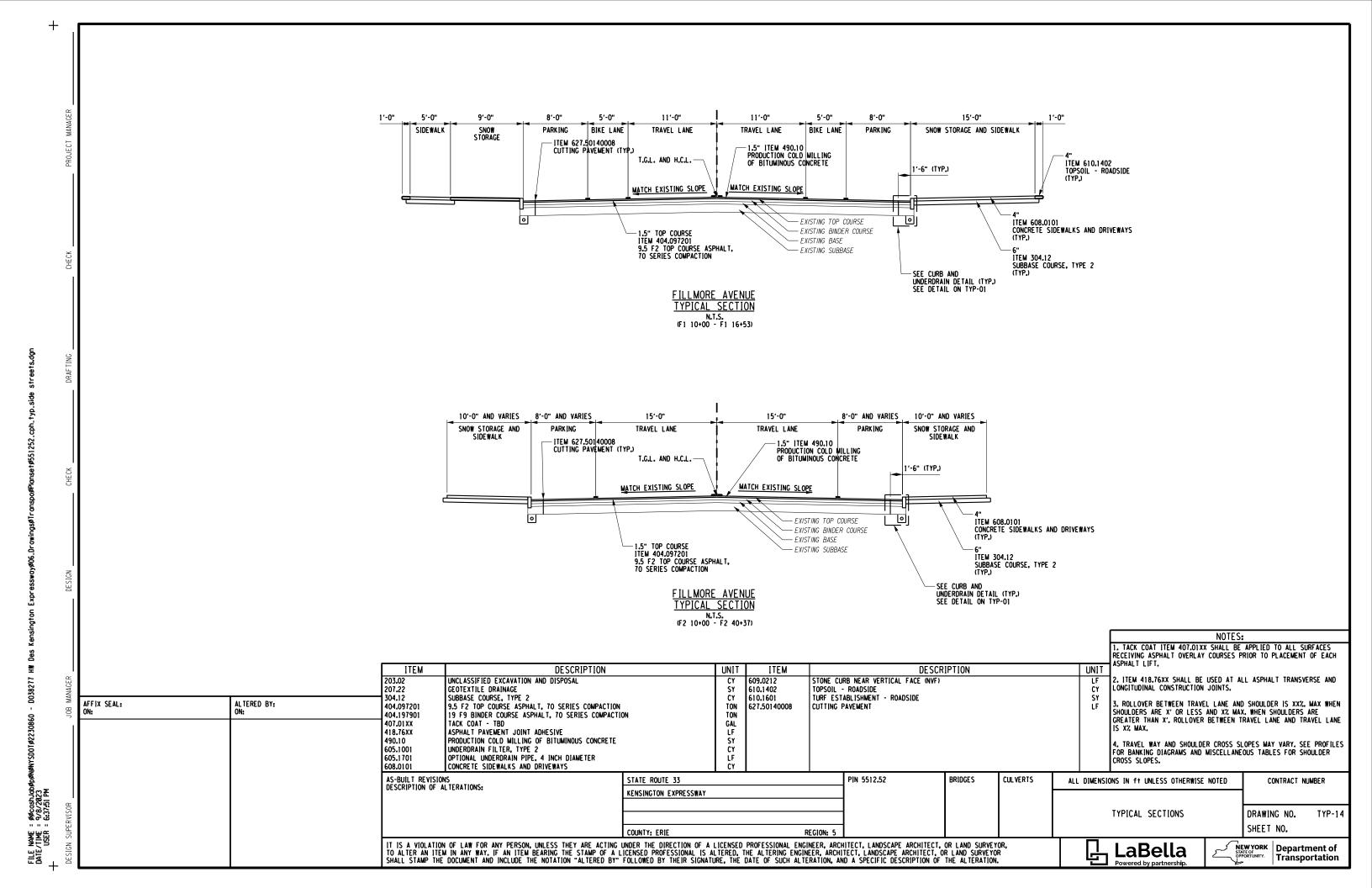


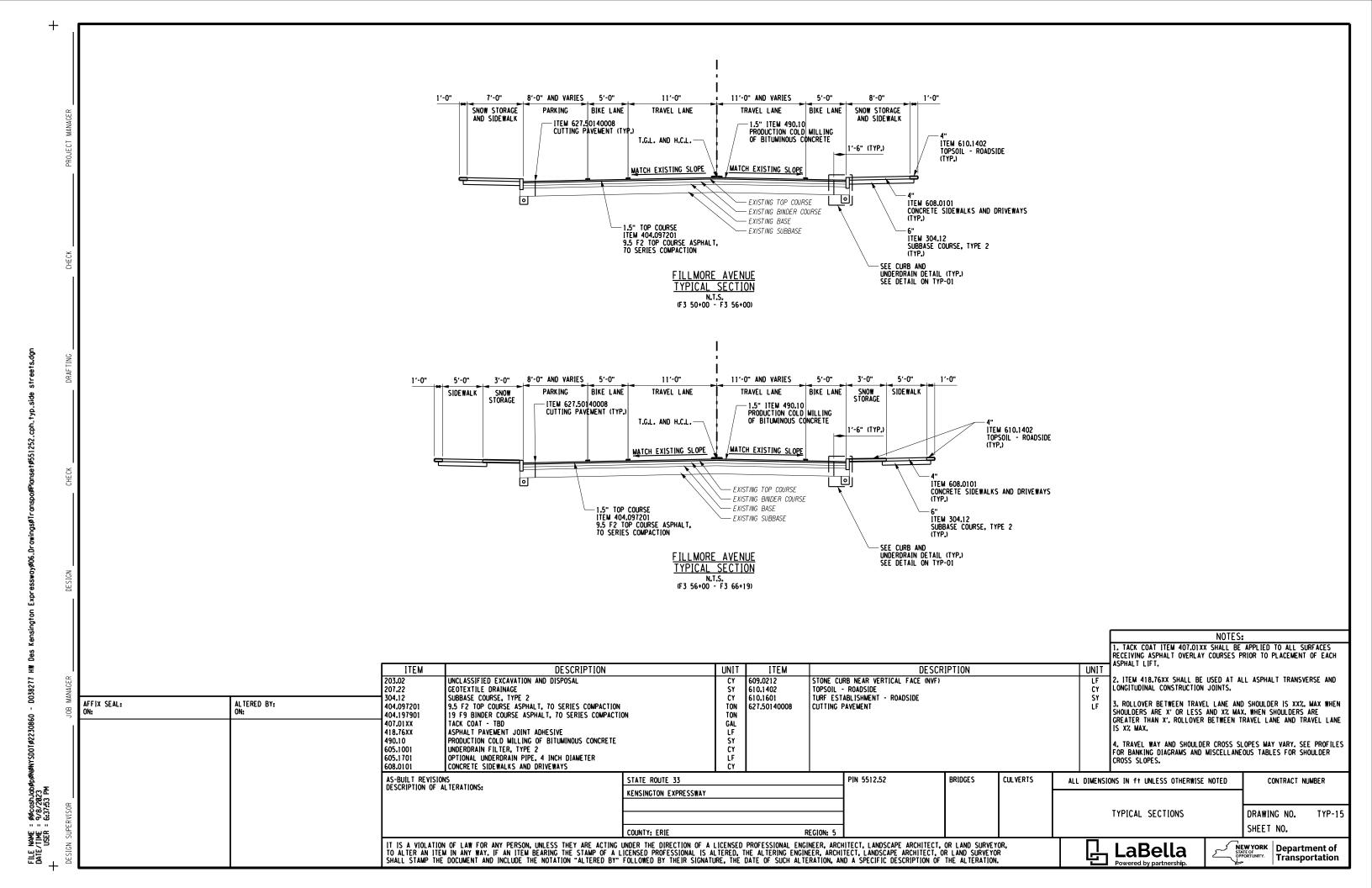








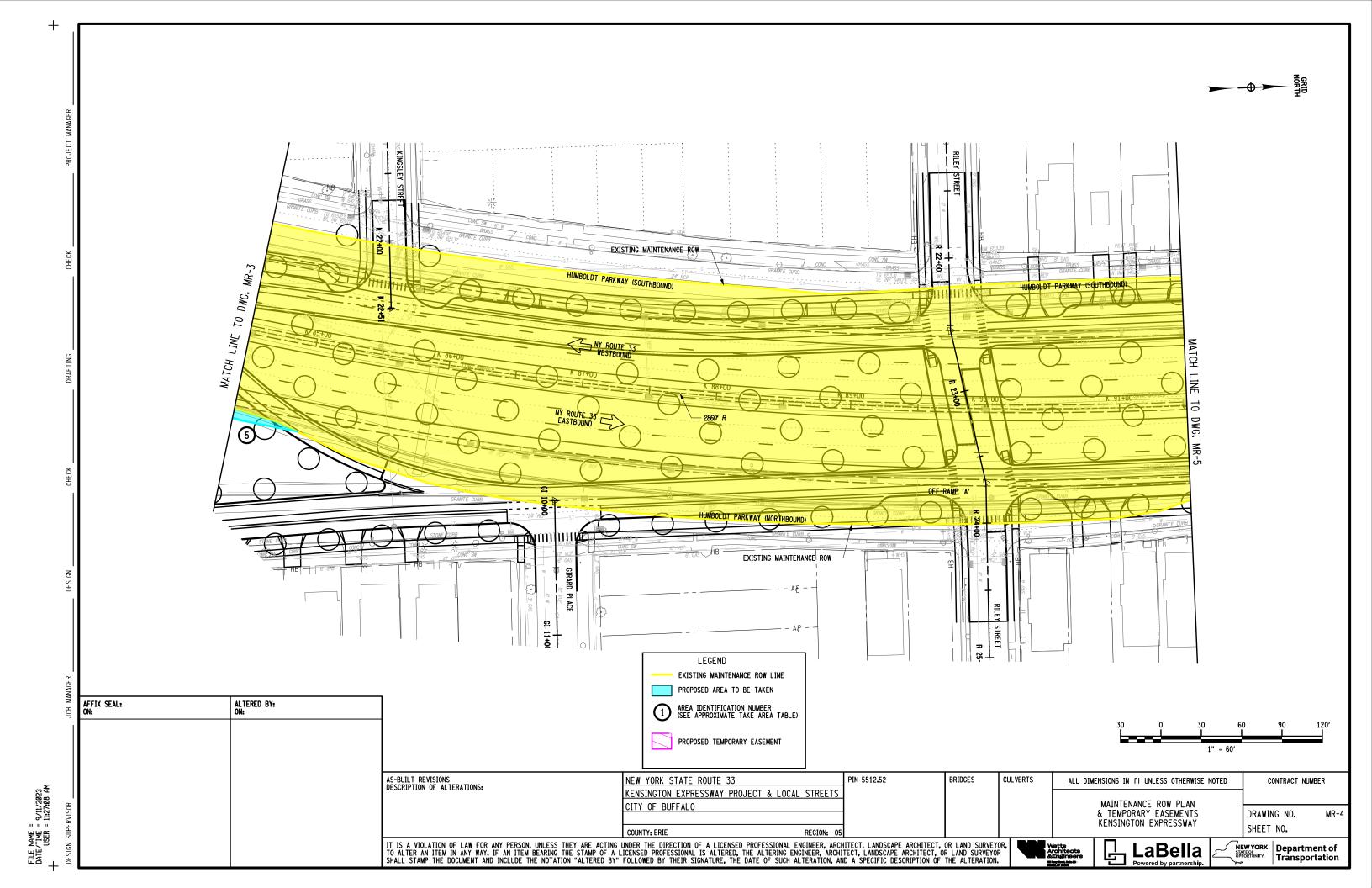


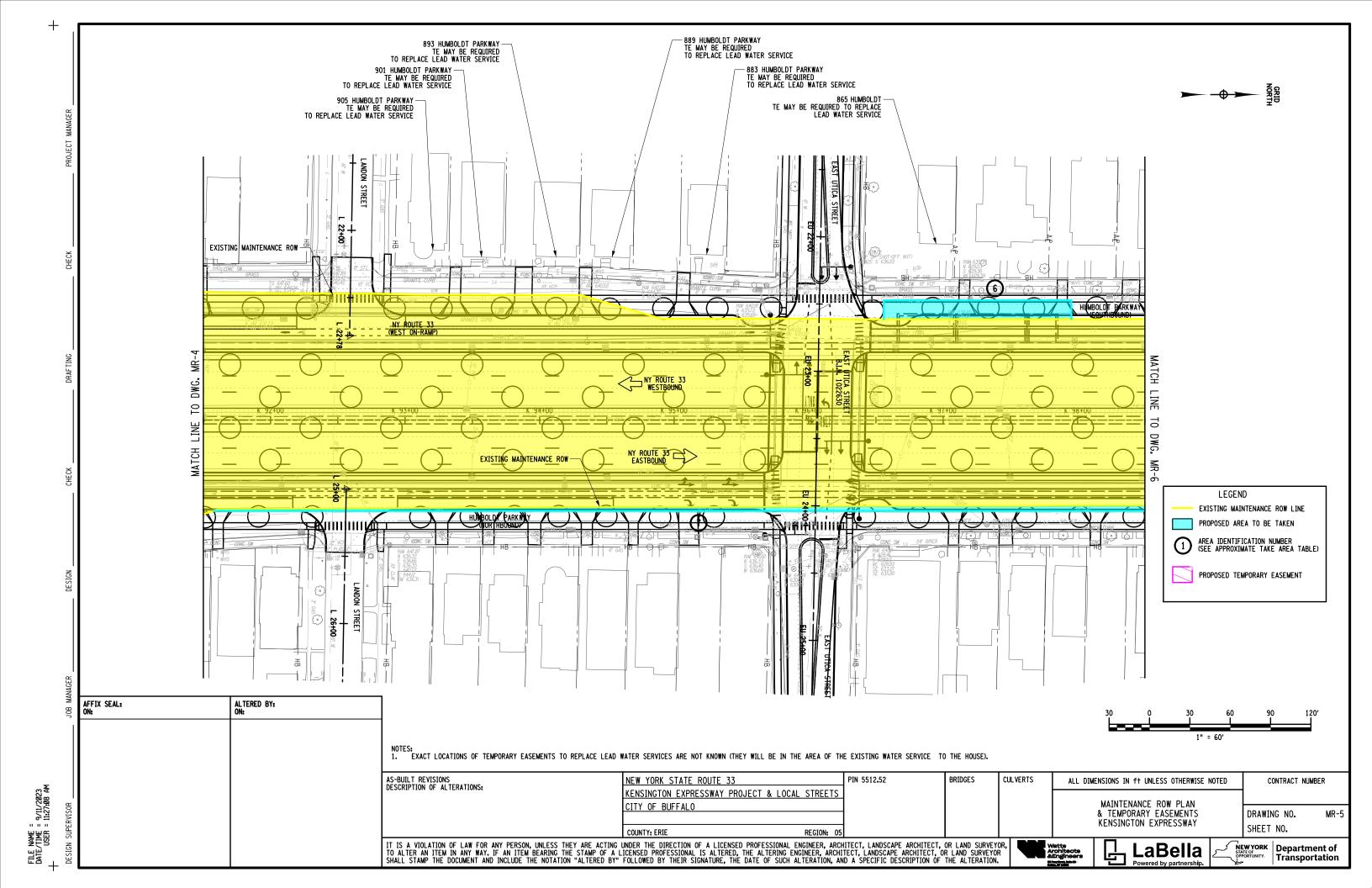


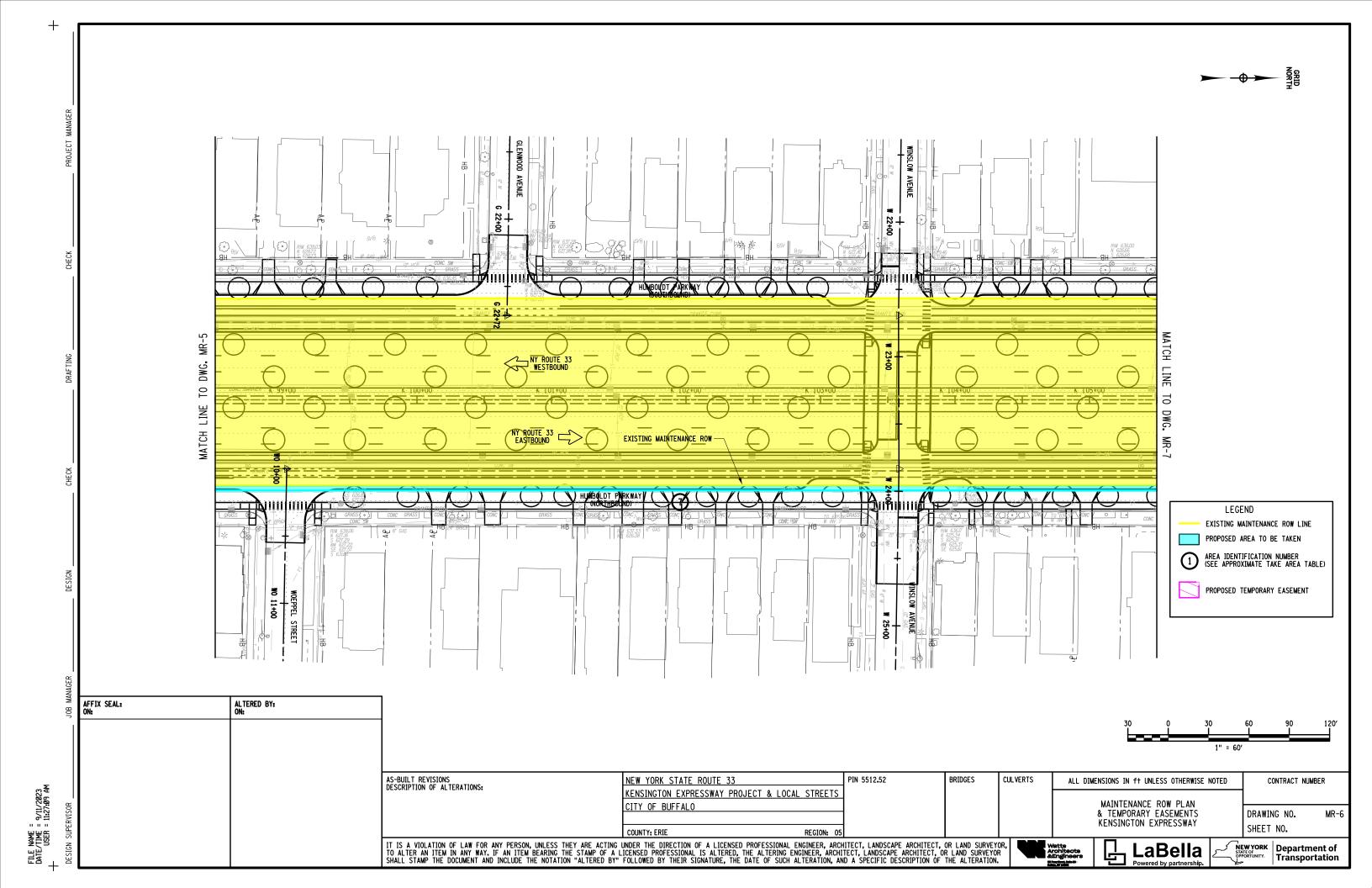
TE MAY BE REQUIRED TO REPLACE
LEAD WATER SERVICE 594 DODGE STREET-TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE LINDEN AVENUE LP 13+00 EXISTING MAINTENANCE ROW WEŞT PARADE STREET 11+00 MATCH NY ROUTE 33 WESTBOUND 10 EB 71+00 EB 72+00 EB 73+00 EB 74+00 MATCH BF 6+00 LEGEND EXISTING MAINTENANCE ROW LINE EXISTING MAINTENANCE ROW PROPOSED AREA TO BE TAKEN AREA IDENTIFICATION NUMBER (SEE APPROXIMATE TAKE AREA TABLE) TE FOR TIE BACKS PROPOSED TEMPORARY EASEMENT (i) TE FOR TIE BACKS ALTERED BY: ON: AFFIX SEAL: ON: NOTES:
1. EXACT LOCATIONS OF TEMPORARY EASEMENTS TO REPLACE LEAD WATER SERVICES ARE NOT KNOWN (THEY WILL BE IN THE AREA OF THE EXISTING WATER SERVICE TO THE HOUSE). AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: CULVERTS PIN 5512.52 BRIDGES NEW YORK STATE ROUTE 33 ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER KENSINGTON EXPRESSWAY PROJECT & LOCAL STREETS MAINTENANCE ROW PLAN & TEMPORARY EASEMENTS CITY OF BUFFALO MR-2 DRAWING NO. KENSINGTON EXPRESSWAY SHEET NO. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. NEW YORK STATE OF OPPORTUNITY. Department of Transportation LaBella

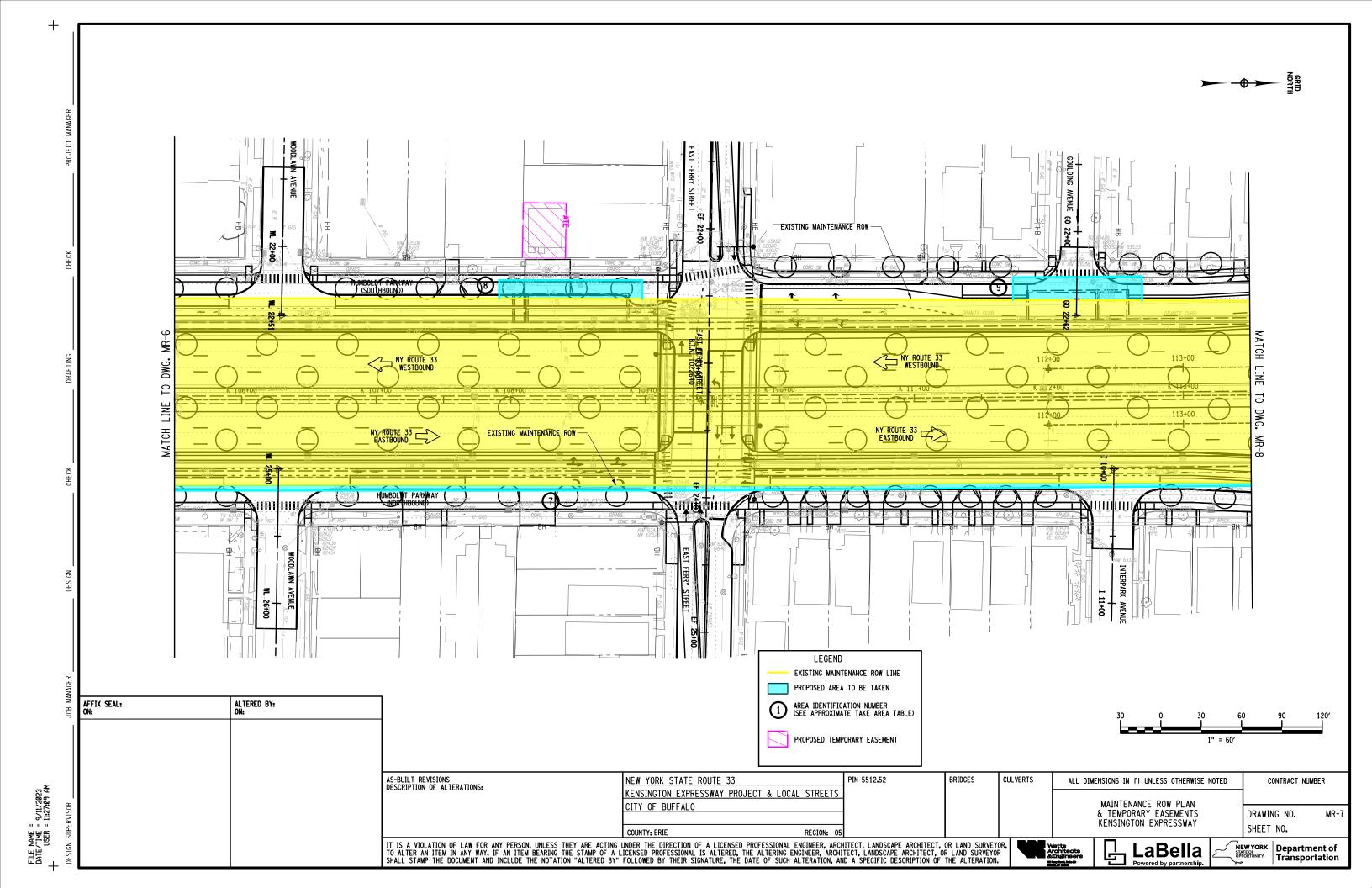
-102 WEST PARADE TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE 80 WEST PARADE-TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE 90 WEST PARADE TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE - 106 WEST PARADE TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE LEGEND 74 WEST PARADE-TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE EXISTING MAINTENANCE ROW LINE 96 WEST PARADE TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE -112 WEST PARADE TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE PROPOSED AREA TO BE TAKEN - 86 WEST PARADE TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE 70 WEST PARADE-TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE AREA IDENTIFICATION NUMBER (SEE APPROXIMATE TAKE AREA TABLE) 66 WEST PARADE-TE MAY BE REQUIRED TO REPLACE LEAD WATER SERVICE PROPOSED TEMPORARY EASEMENT TÉ FOR LANDSCAPING HUMBOLDT PARKWAY

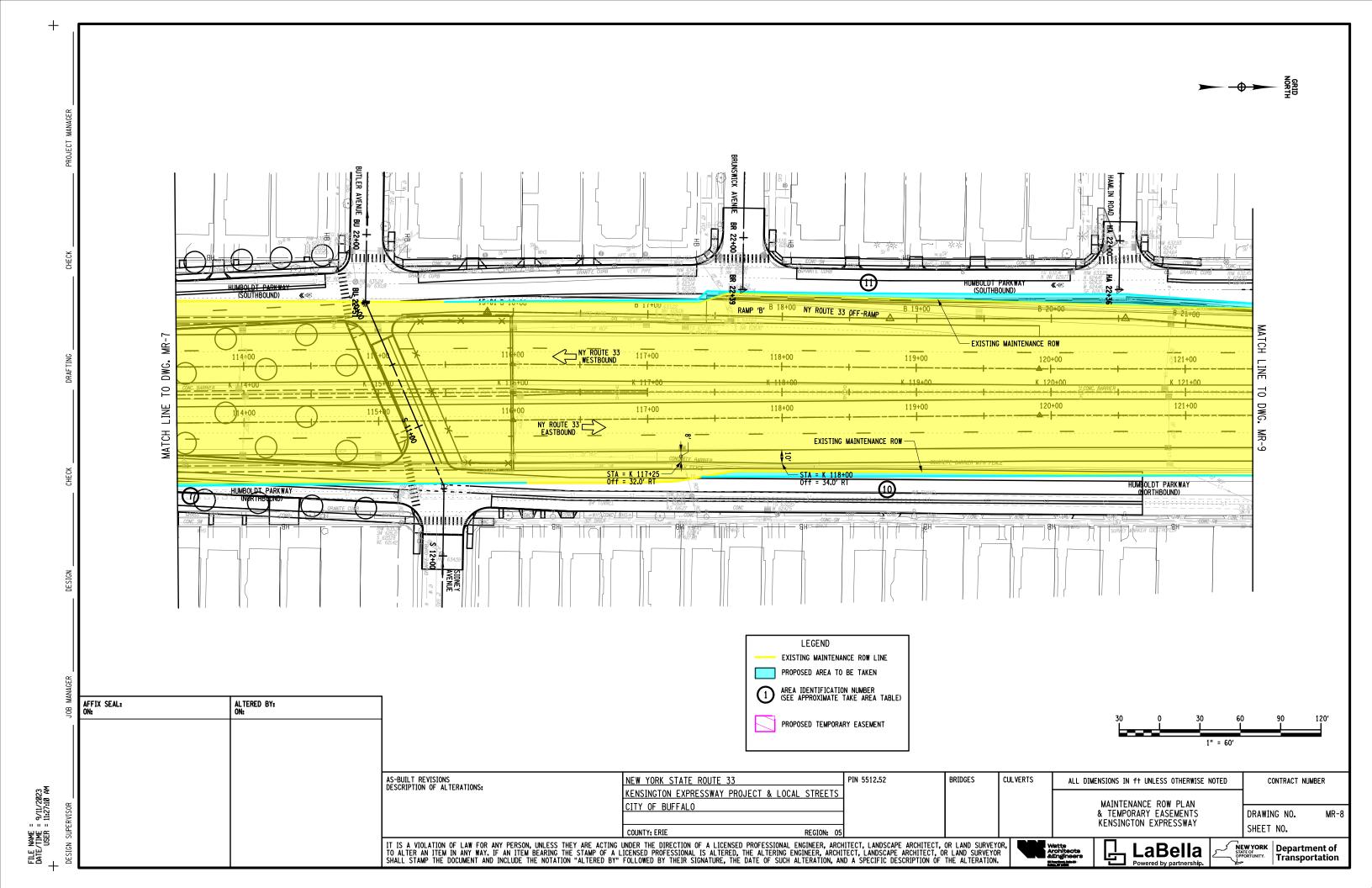
(SOUTHBOUND) EXISTING MAINTENANCE -WEST PARADE STREET 13+00 り NY ROUTE 33
WESTBOUND F-4 NY ROUTE 33 EASTBOUND WEST DRIVE EXISTING MAINTENANCE ROW ALTERED BY: ON: AFFIX SEAL: ON: TE FOR SOE WALL TIE BACKS DURING CONSTRUCTION OF TUNNEL ACCESS TO DRIVEWAY AND PARKING SHALL BE MAINTAINED AT ALL TIMES NOTES:
1. EXACT LOCATIONS OF TEMPORARY EASEMENTS TO REPLACE LEAD WATER SERVICES ARE NOT KNOWN (THEY WILL BE IN THE AREA OF THE EXISTING WATER SERVICE TO THE HOUSE). AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: CULVERTS PIN 5512.52 BRIDGES ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER KENSINGTON EXPRESSWAY PROJECT & LOCAL STREETS MAINTENANCE ROW PLAN &TEMPORARY EASEMENTS CITY OF BUFFALO MR-3 DRAWING NO. KENSINGTON EXPRESSWAY SHEET NO. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. LaBella

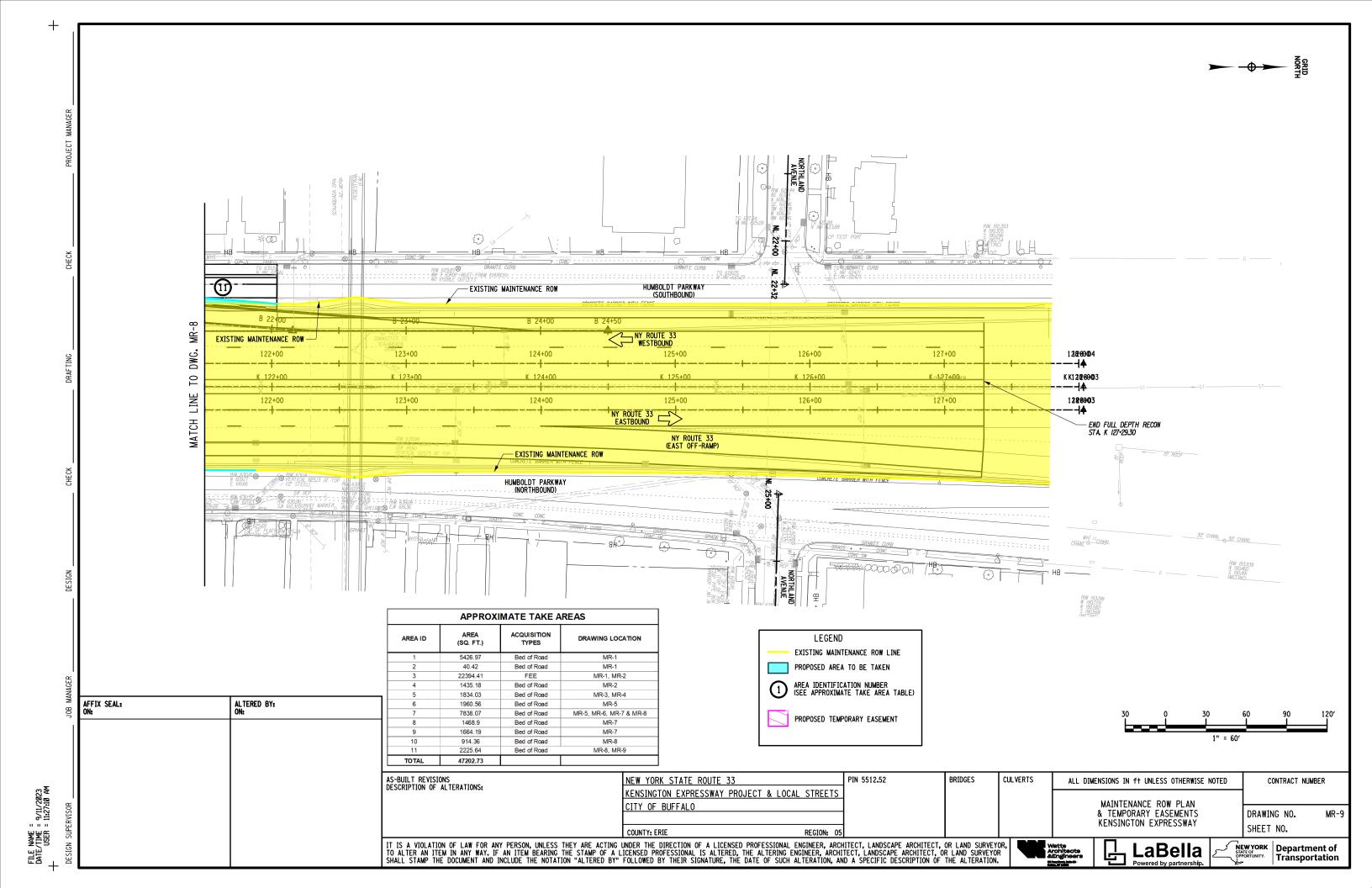


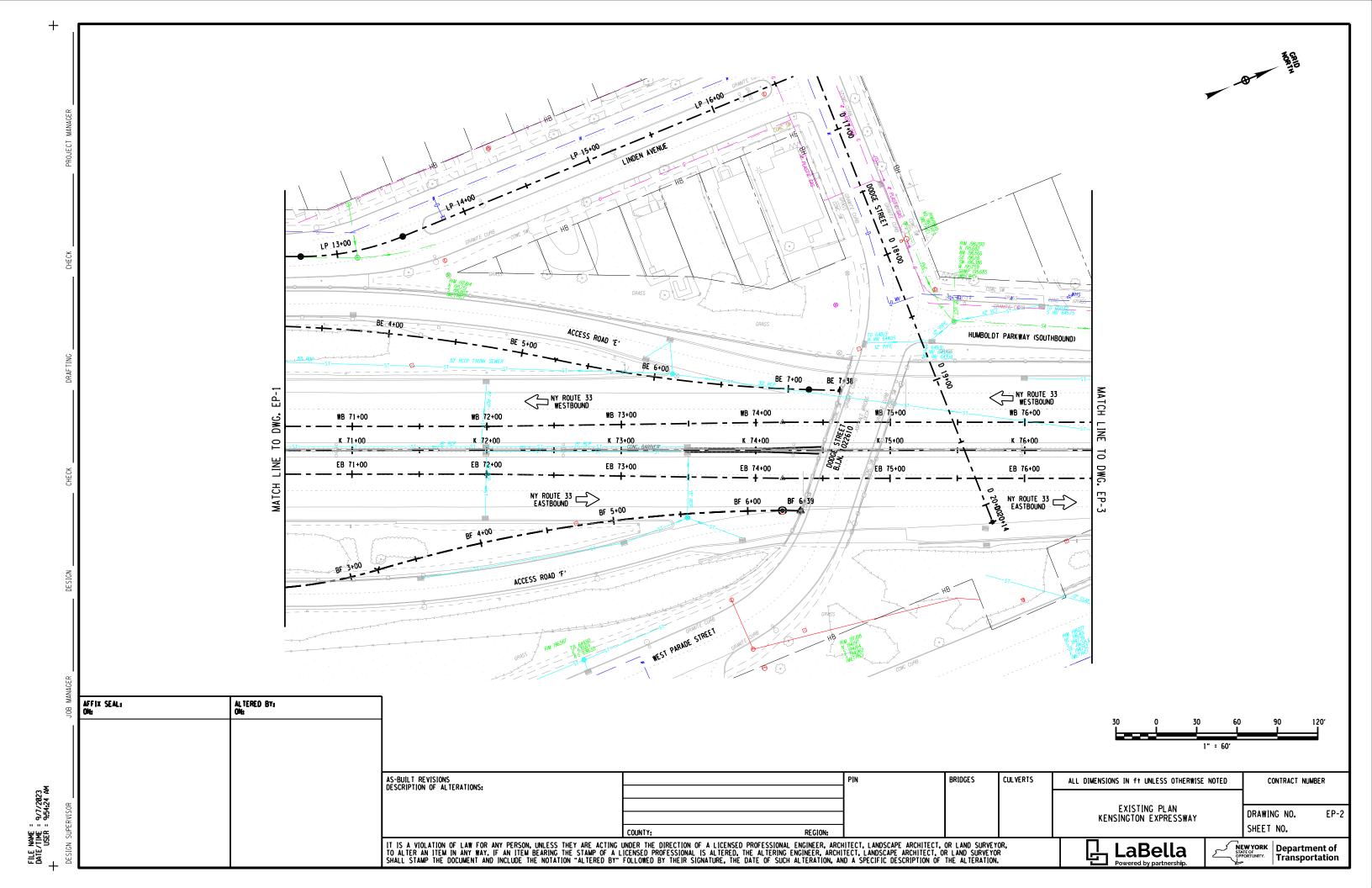


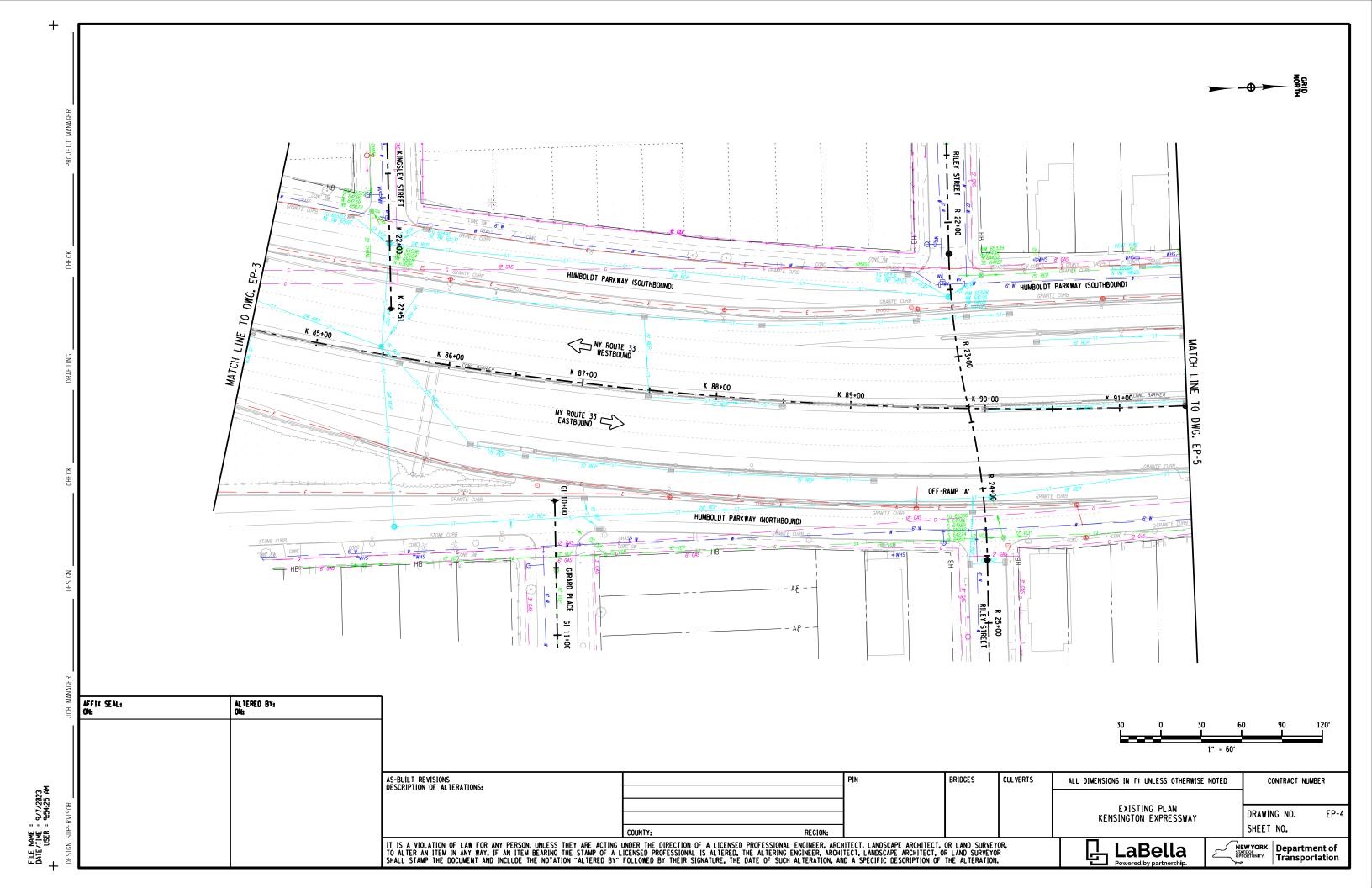


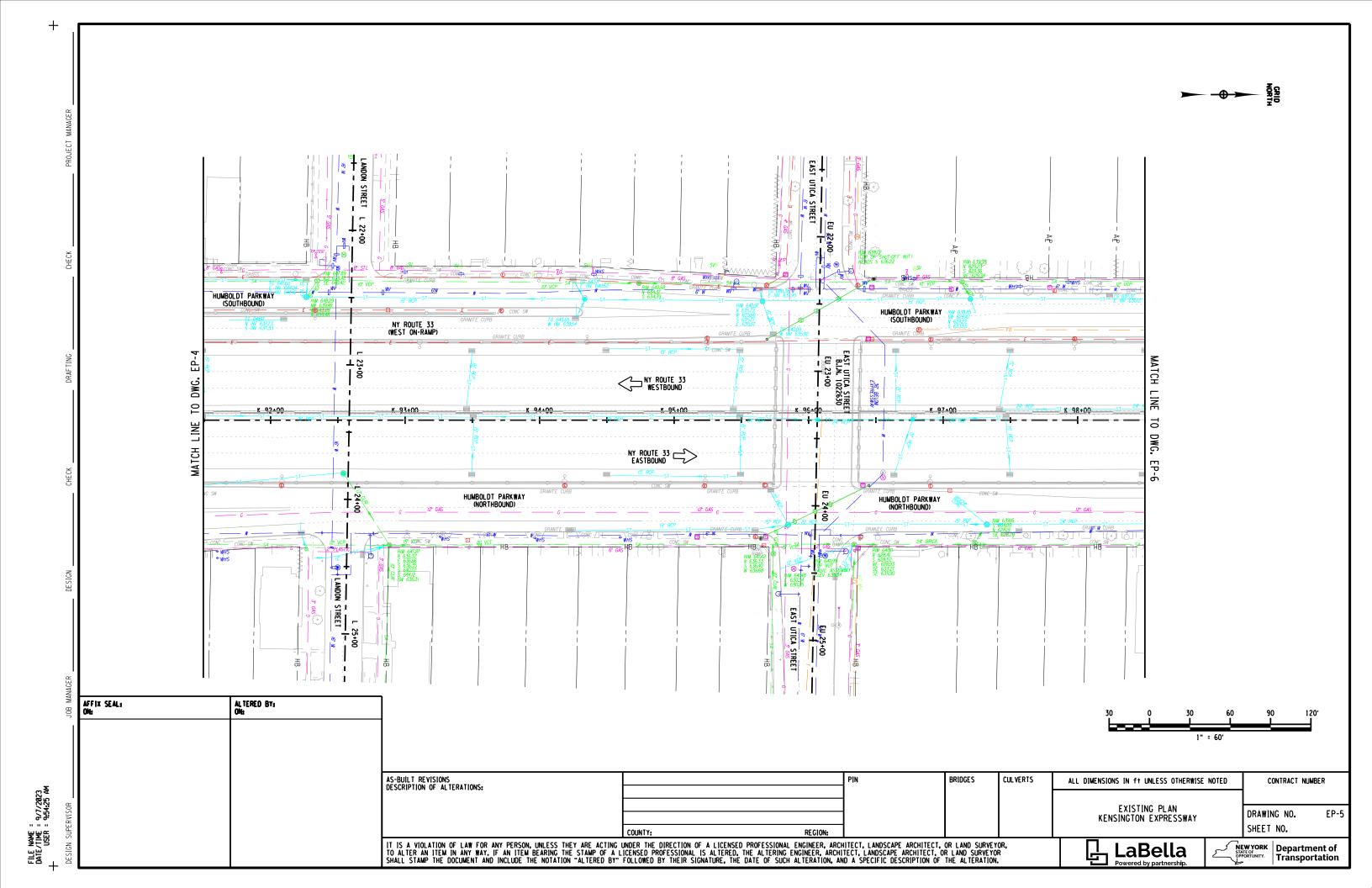


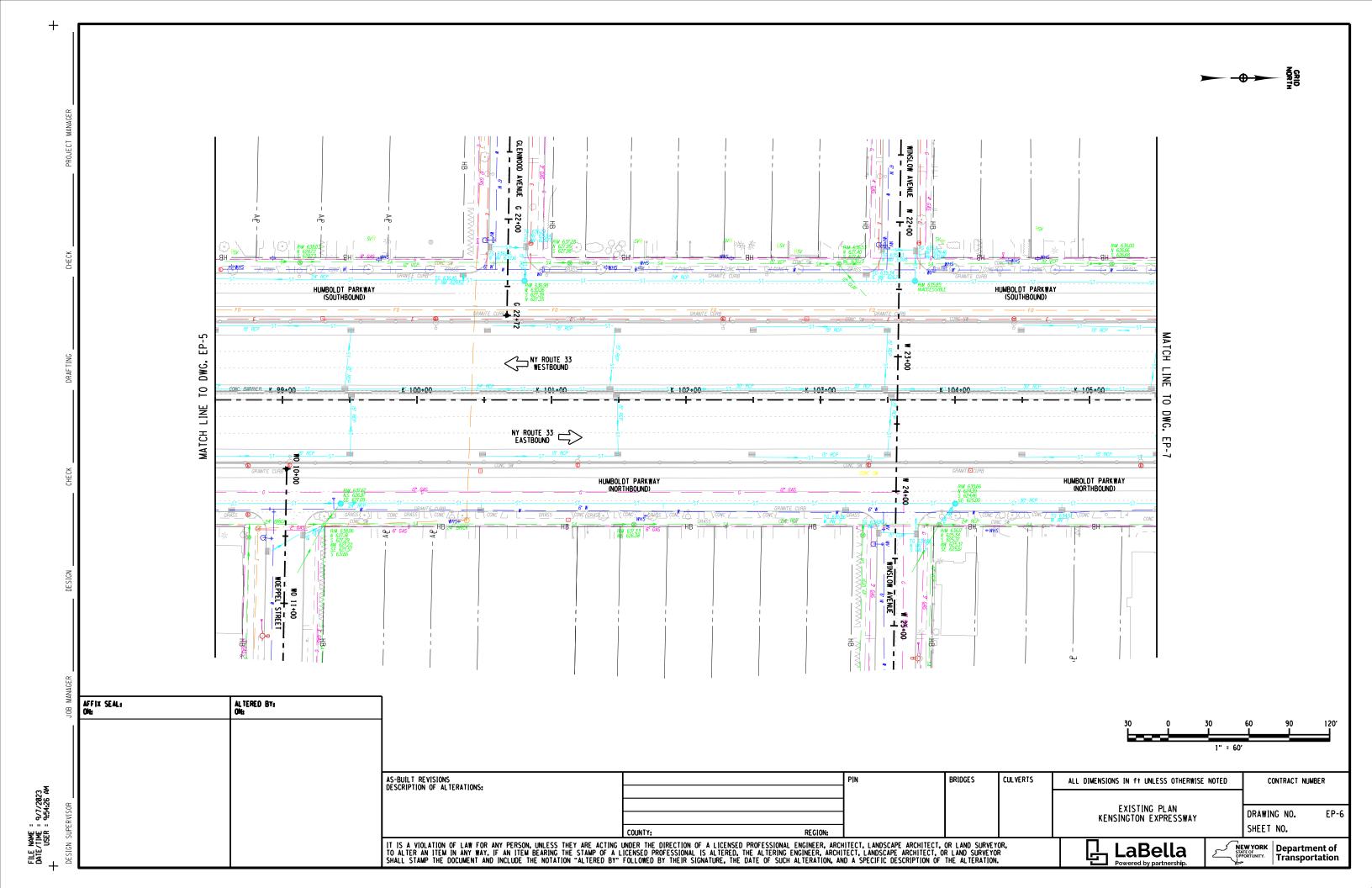


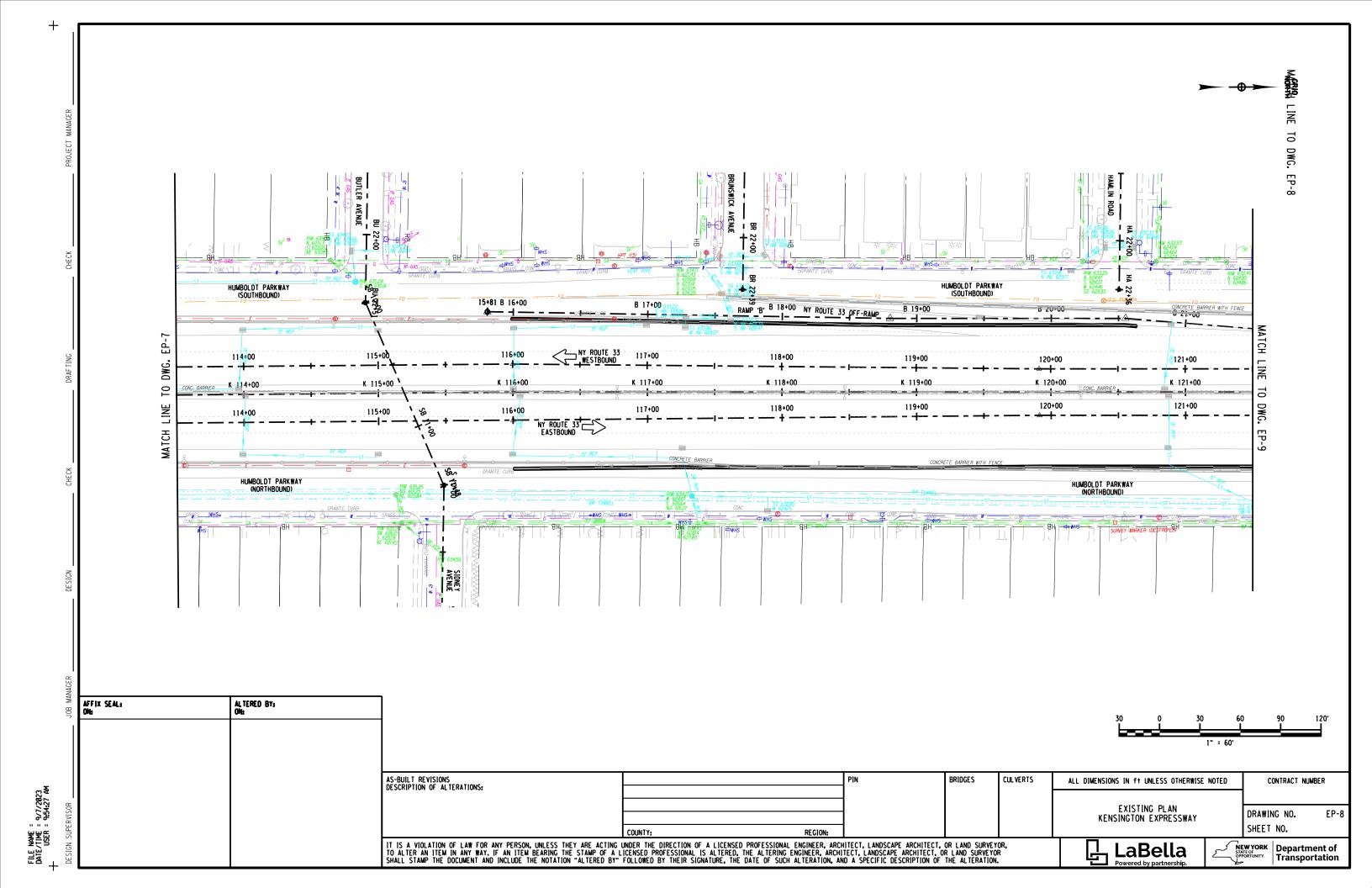


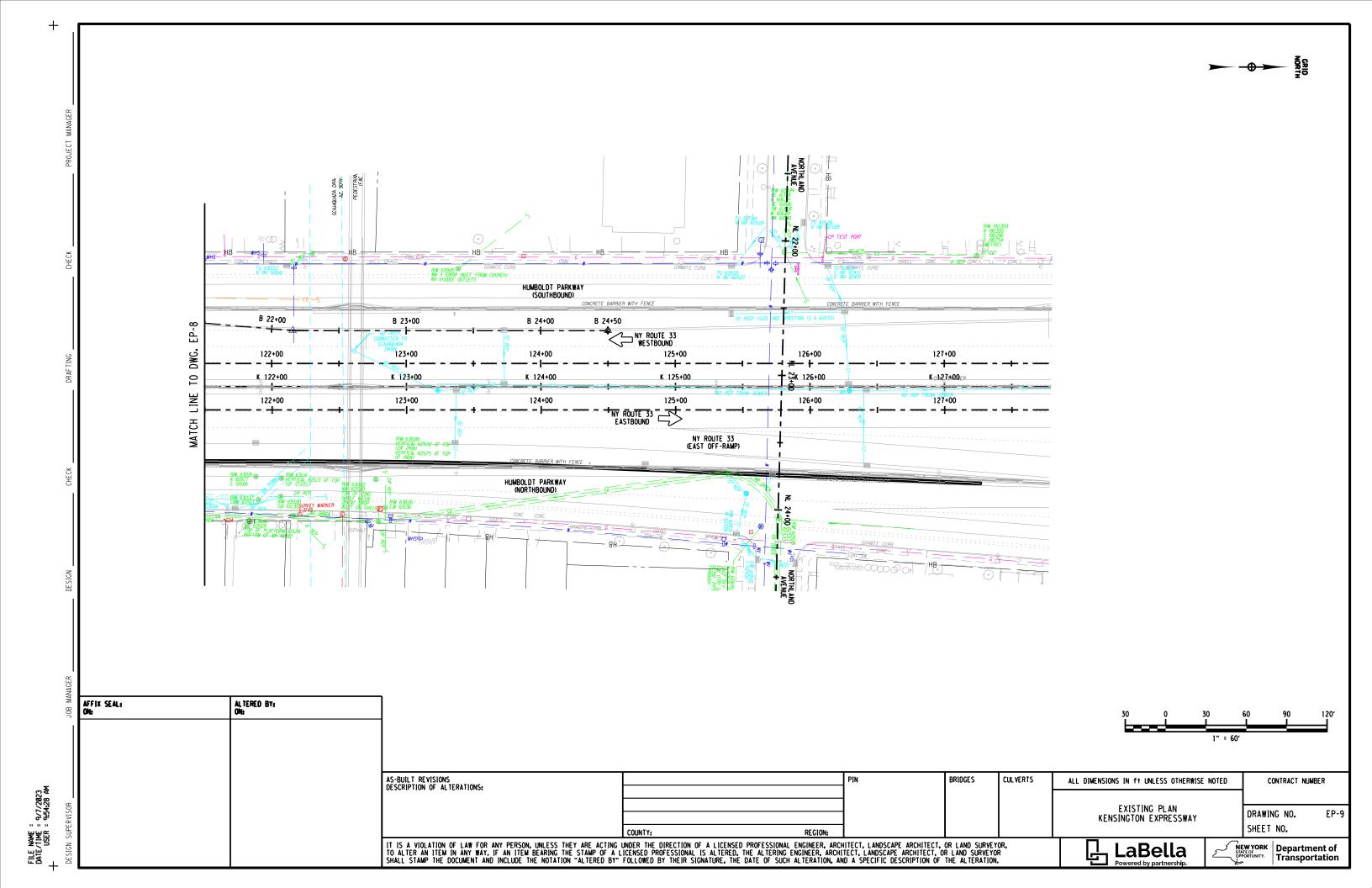


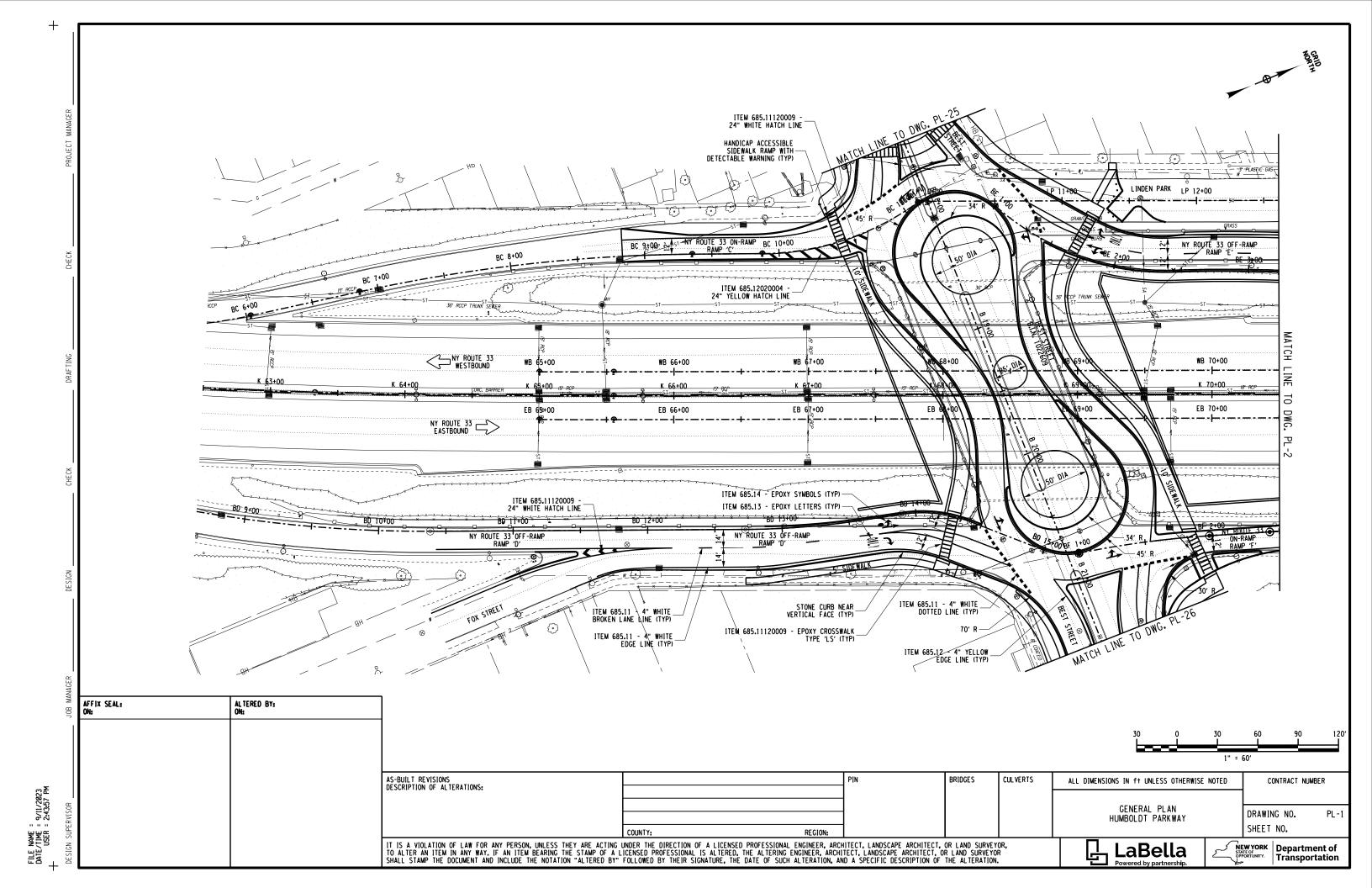


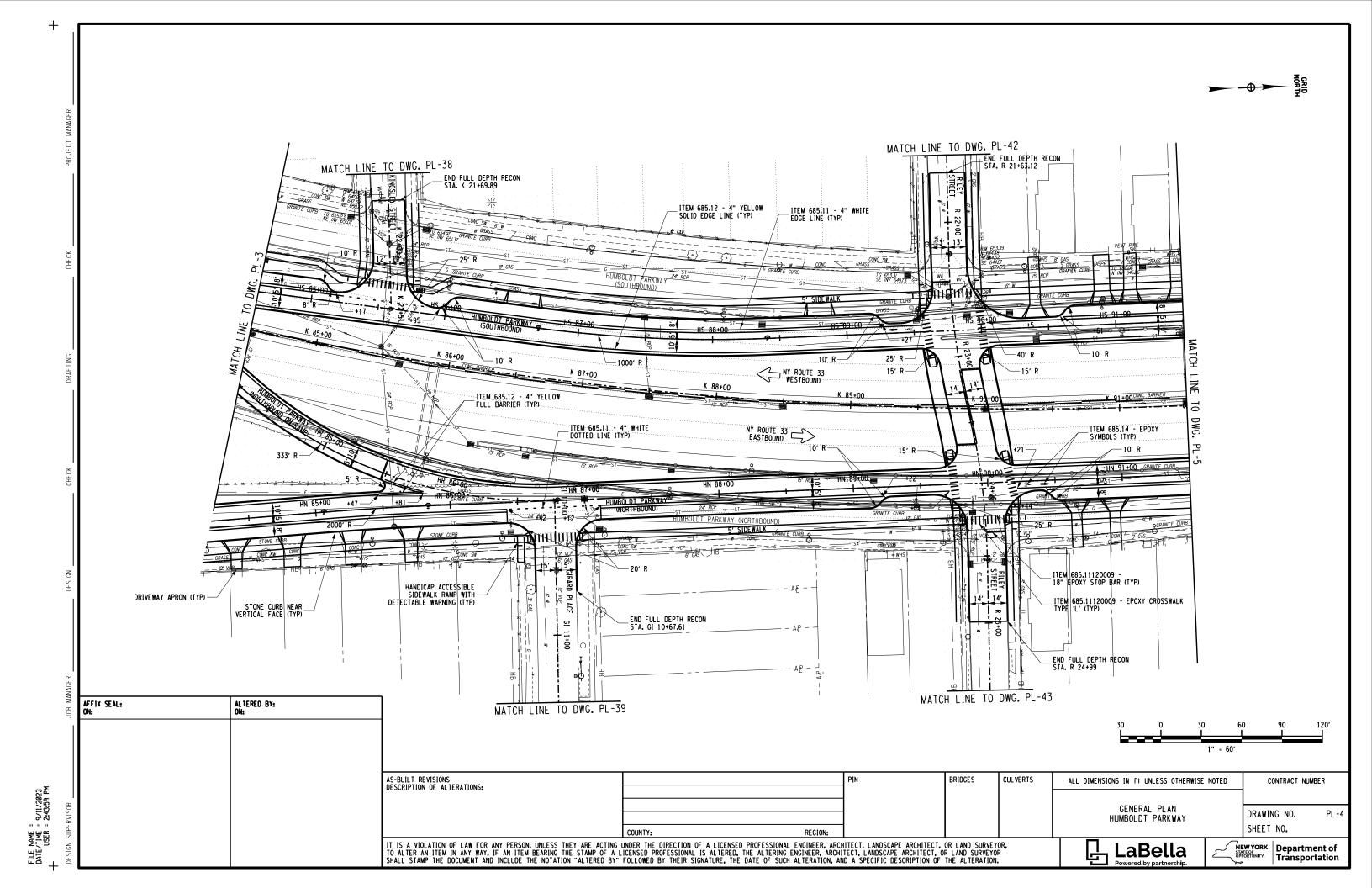


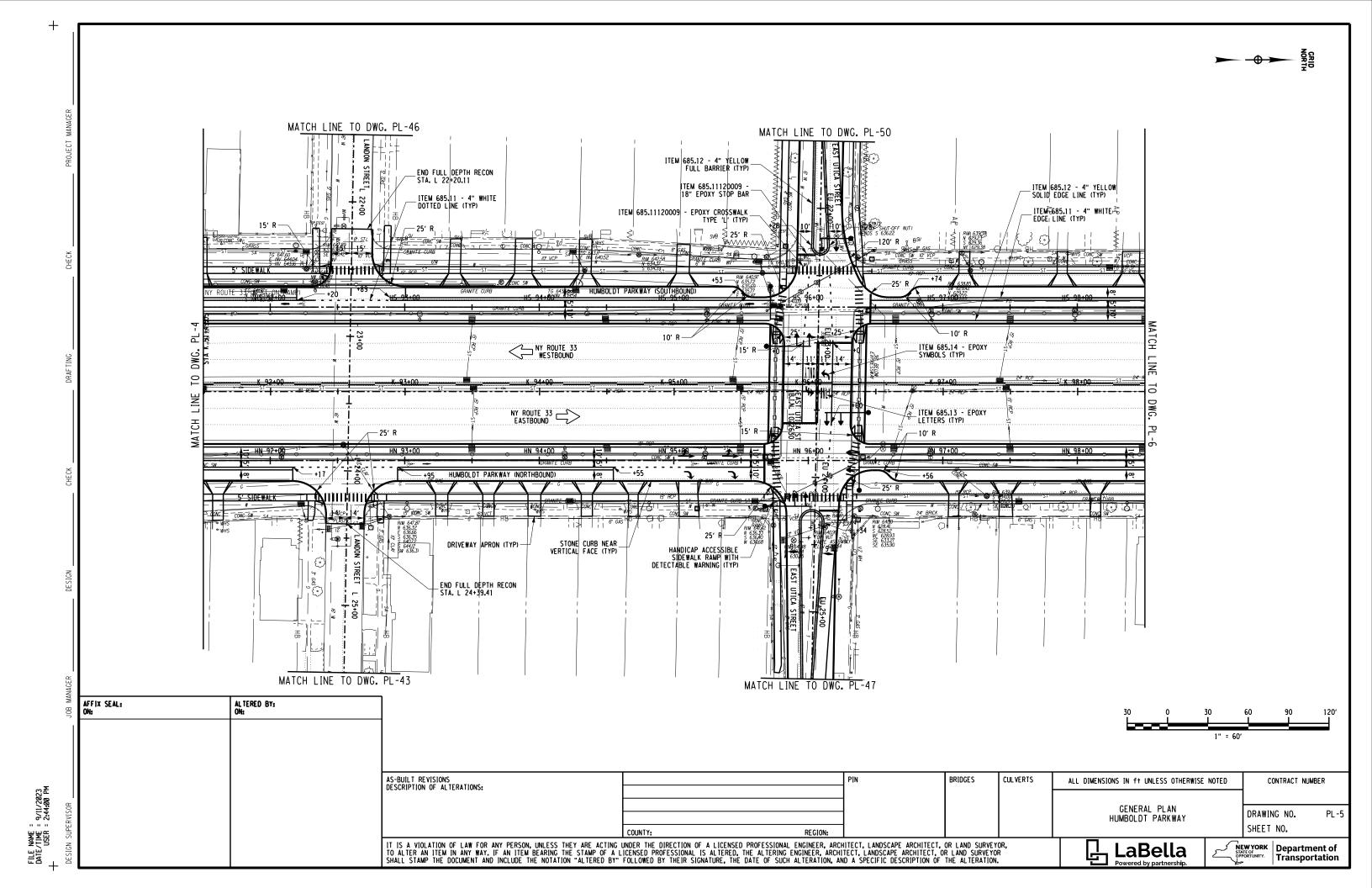


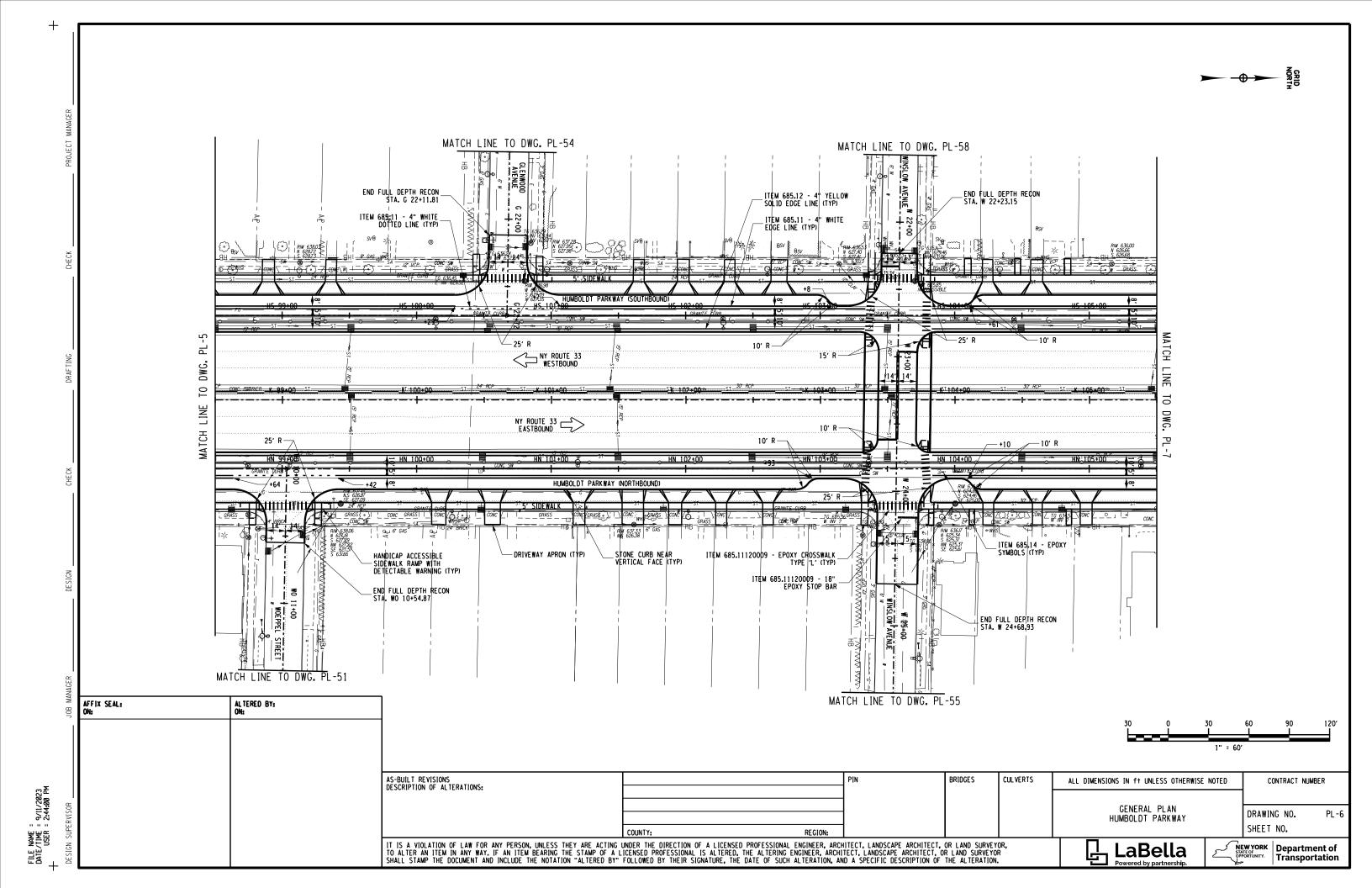


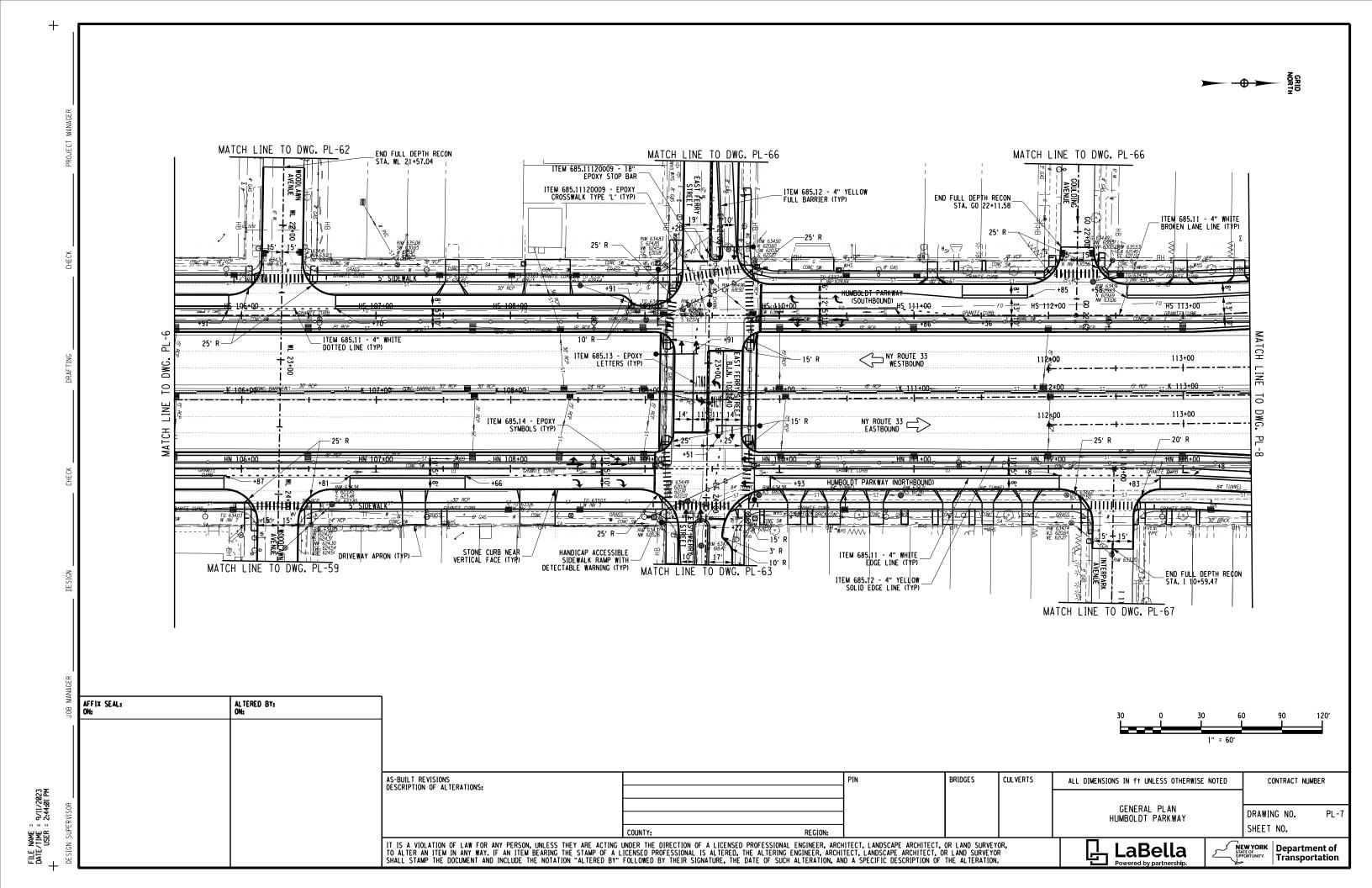


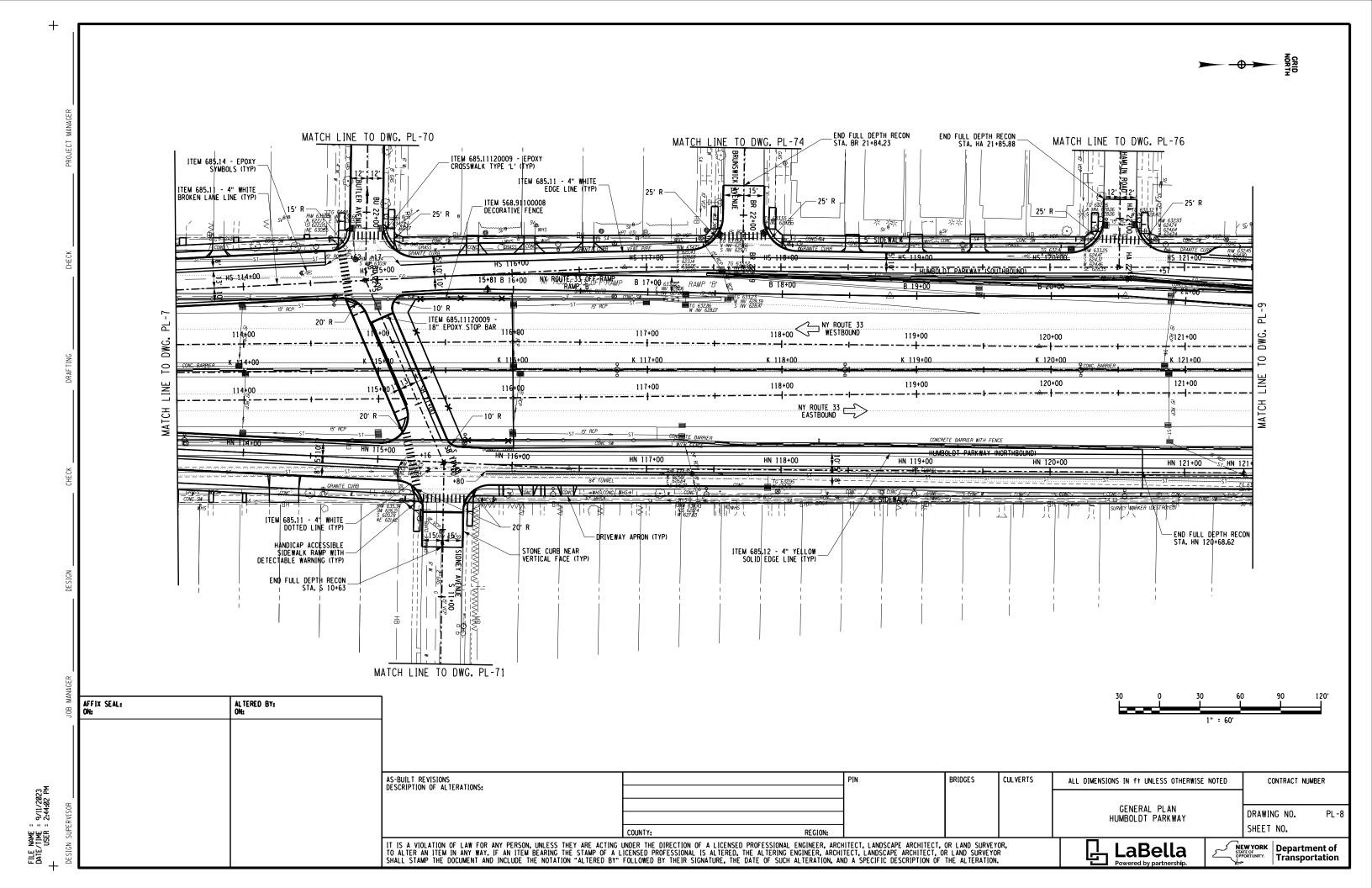


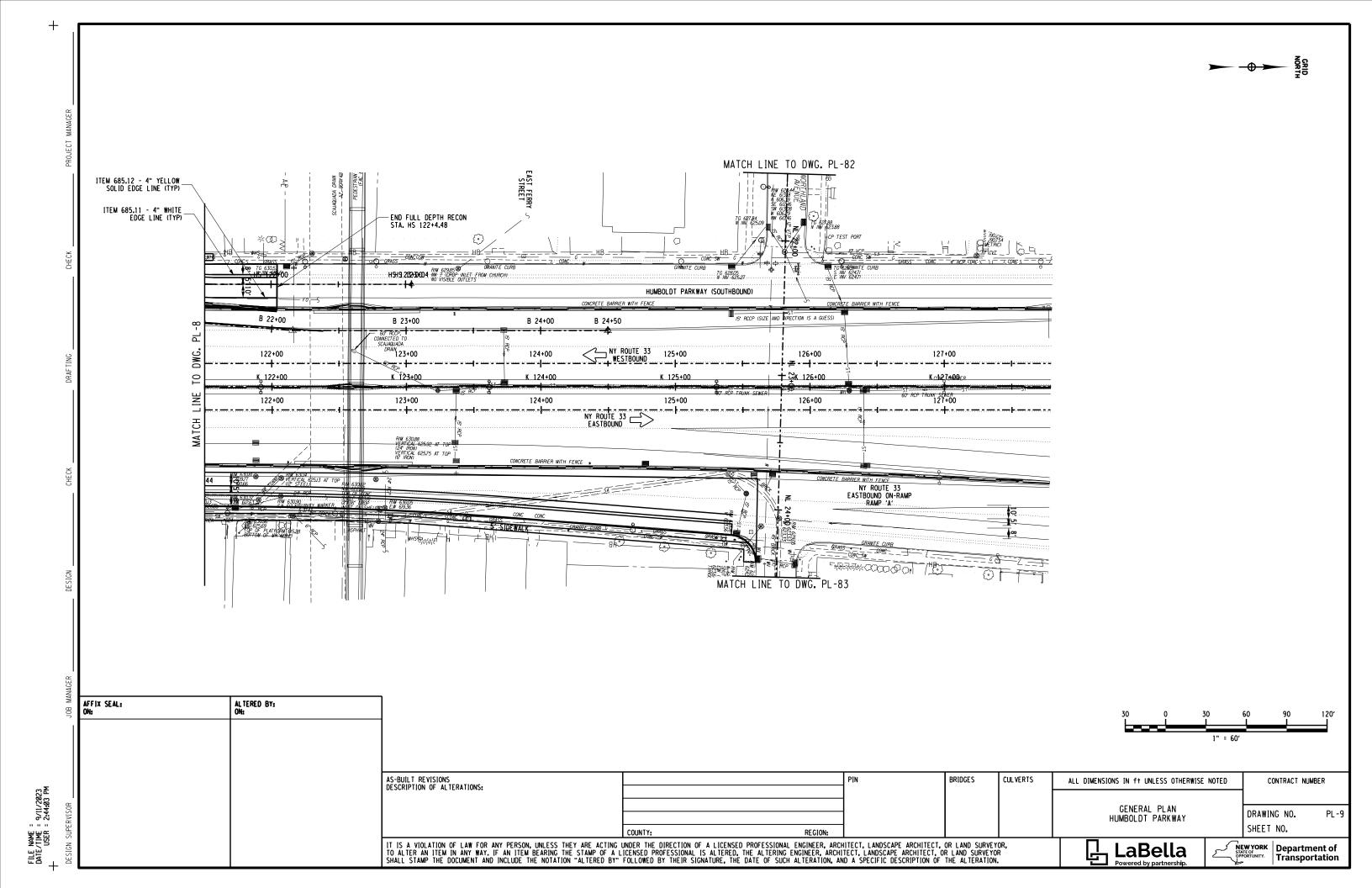


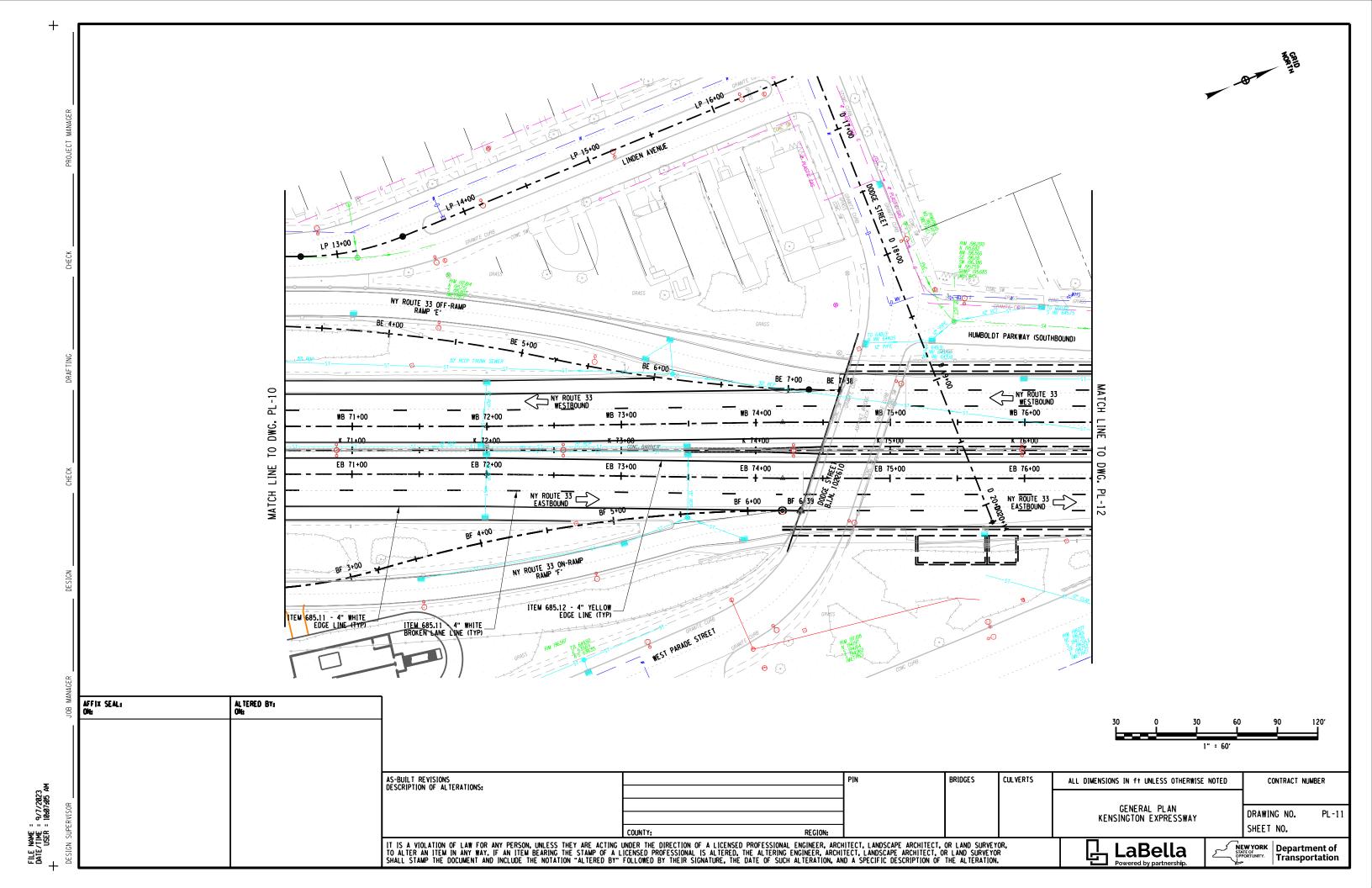






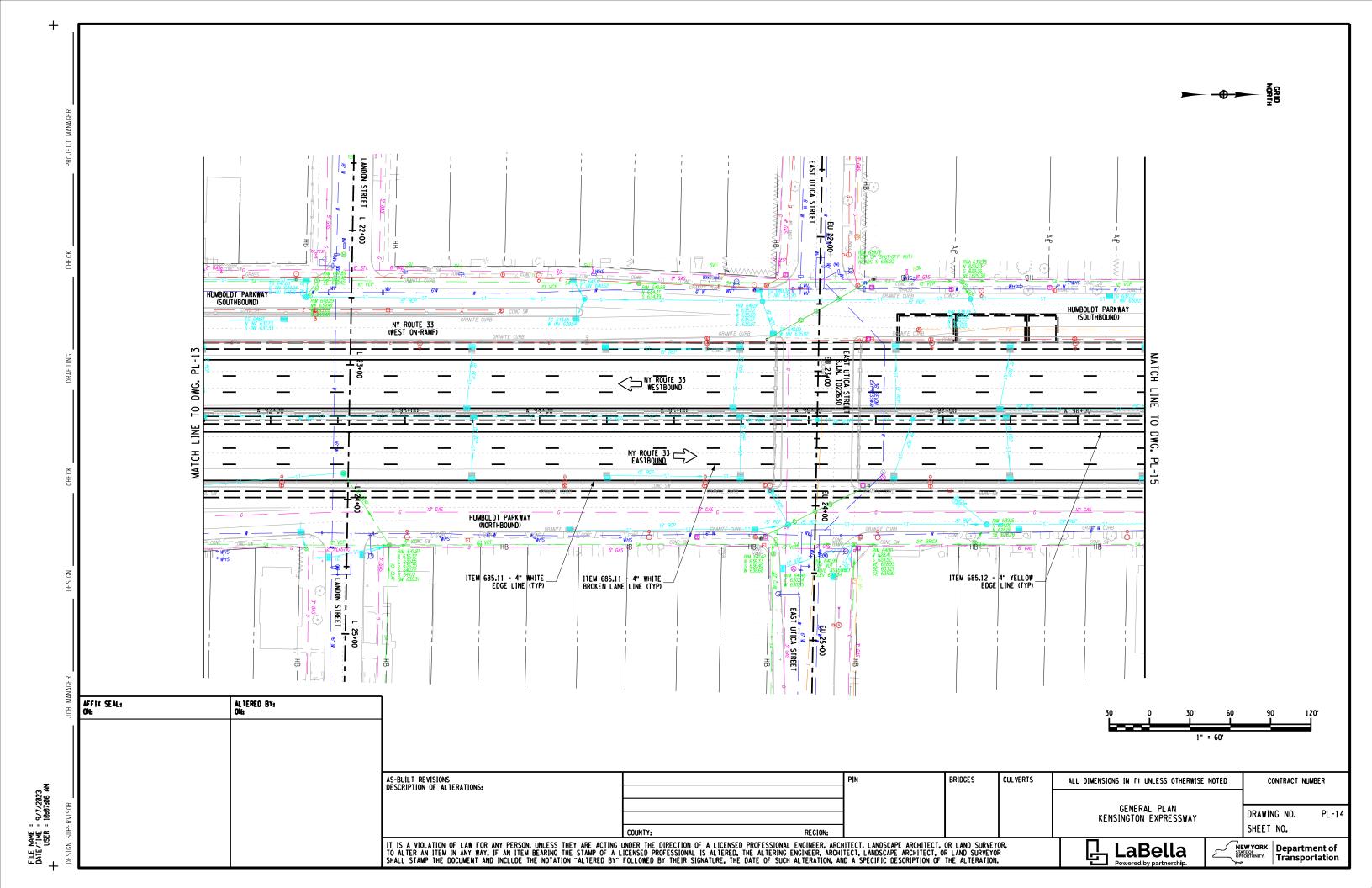


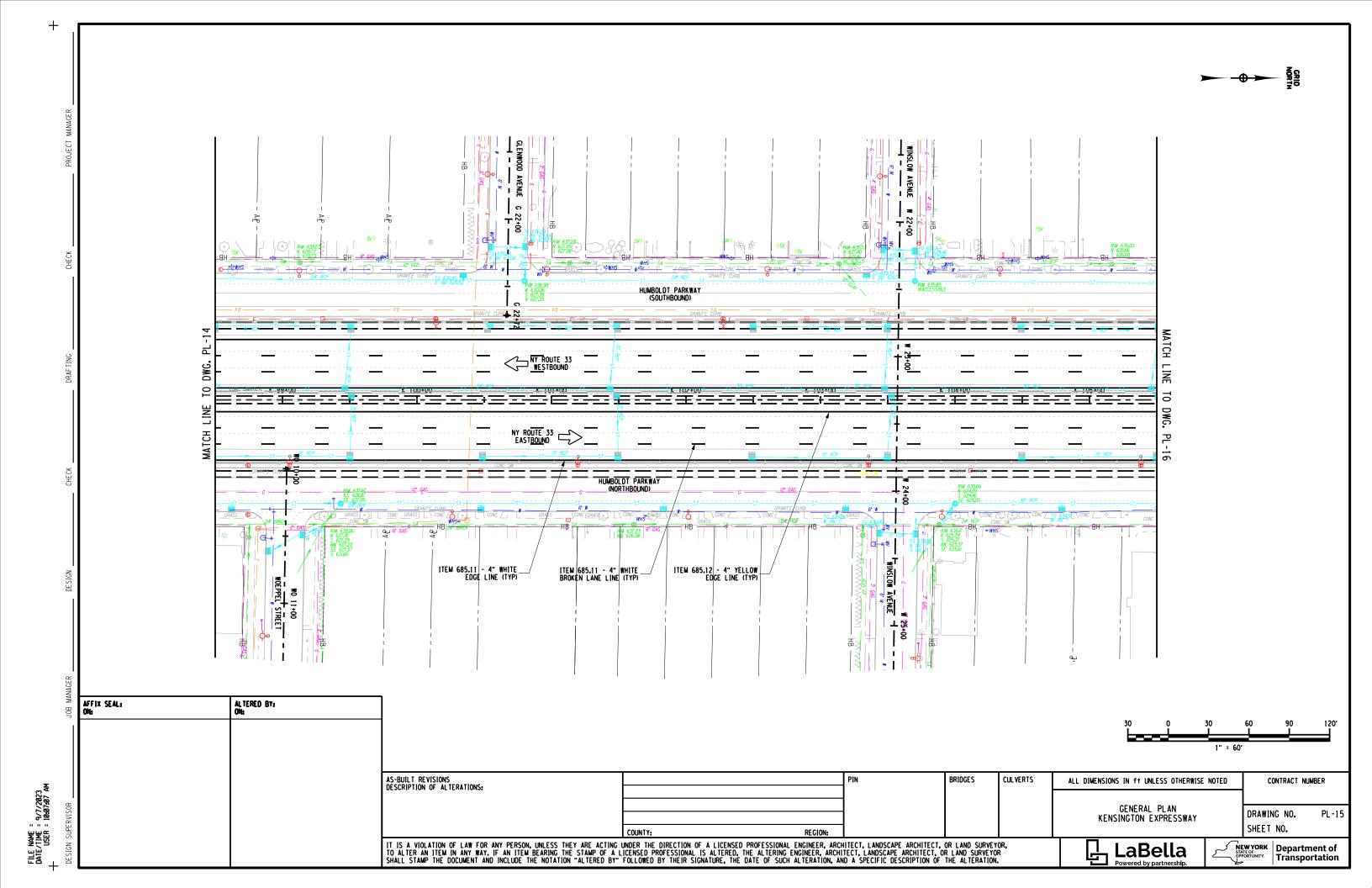




HUMBOLDT PARKWAY (SOUTHBOUND) NY ROUTE 33 WESTBOUND NY ROUTE 33 EB 77+08B 77+20 ITEM 685.11 - 4" WHITE BROKEN LANE LINE (TYP) ITEM 685.11 - 4" WHITE ITEM 685.11 - 4" WHITE EDGE LINE (TYP) ITEM 685.12 - 4" YELLOW _ EDGE LINE (TYP) ALTERED BY: AFFIX SEAL: ON: AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: BRIDGES CULVERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER GENERAL PLAN KENSINGTON EXPRESSWAY DRAWING NO. PL-12 SHEET NO. FILE NAME DATE/TIME USER IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. NEW YORK STATE OF OPPORTUNITY. Department of Transportation LaBella
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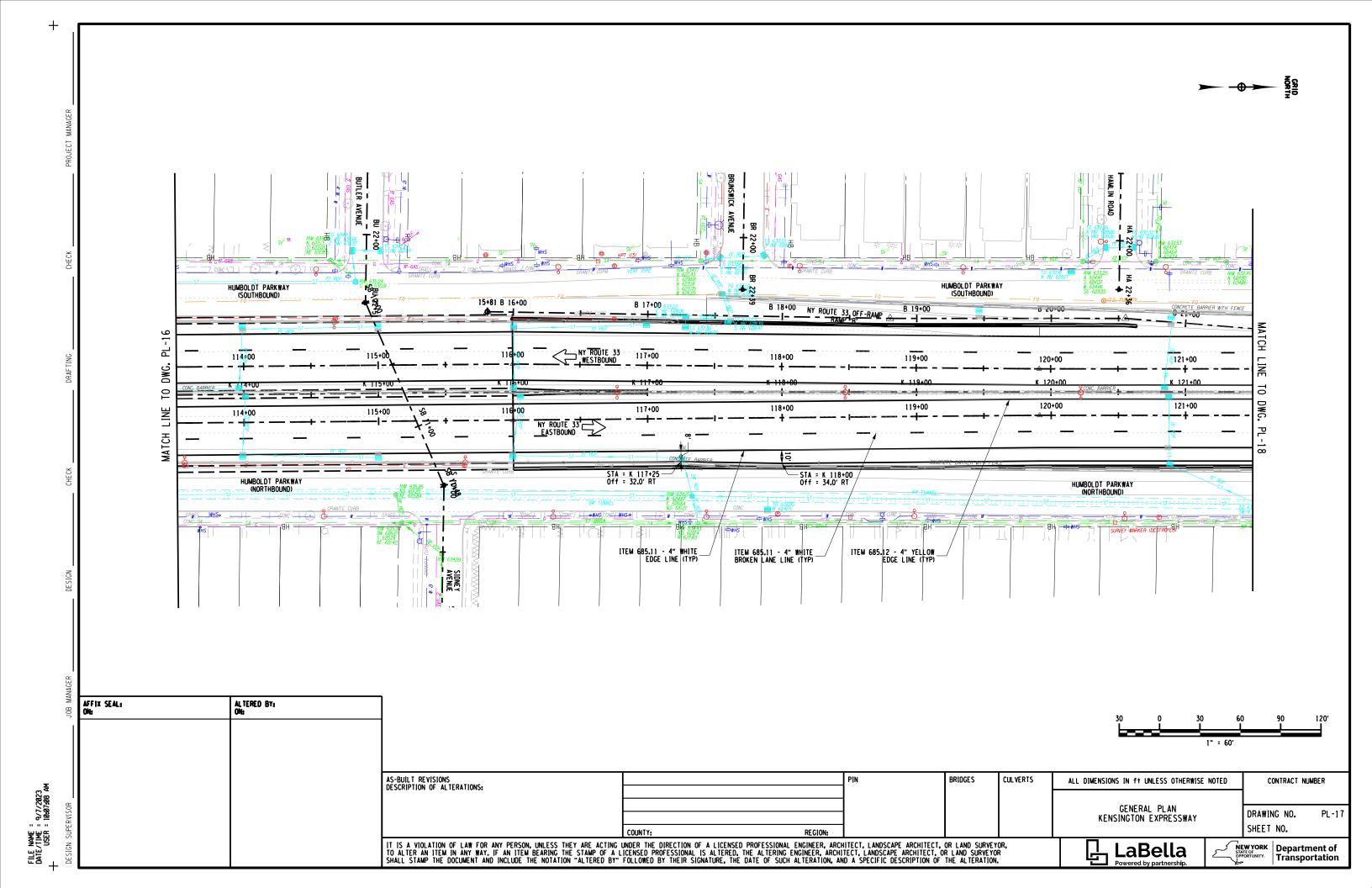
ITEM 685.11:- 4" WHITE _ EDGE LINE (TYP) ITEM 685.11 - 4" WHITE _ BROKEN LANE LINE (TYP) ITEM 685.12 - 4" YELLOW EDGE LINE (TYP) HUMBOLDT PARKWAY (SOUTHBOUND) 6 W HUMBOLDT PARKWAY (SOUTHBOUND) NY ROUTE 33 WESTBOUND OFF-RAMP 'A' HUMBOLDT PARKWAY (NORTHBOUND) RILEY STREET 11±80 ALTERED BY: ON: AFFIX SEAL: AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: BRIDGES CULVERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER FILE NAME = 077/2023 DATE/TIME = 9/7/2023 + USER = 10:07:06 AM GENERAL PLAN KENSINGTON EXPRESSWAY DRAWING NO. PL-13 SHEET NO. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. NEW YORK STATE OF OPPORTUNITY. Department of Transportation LaBella

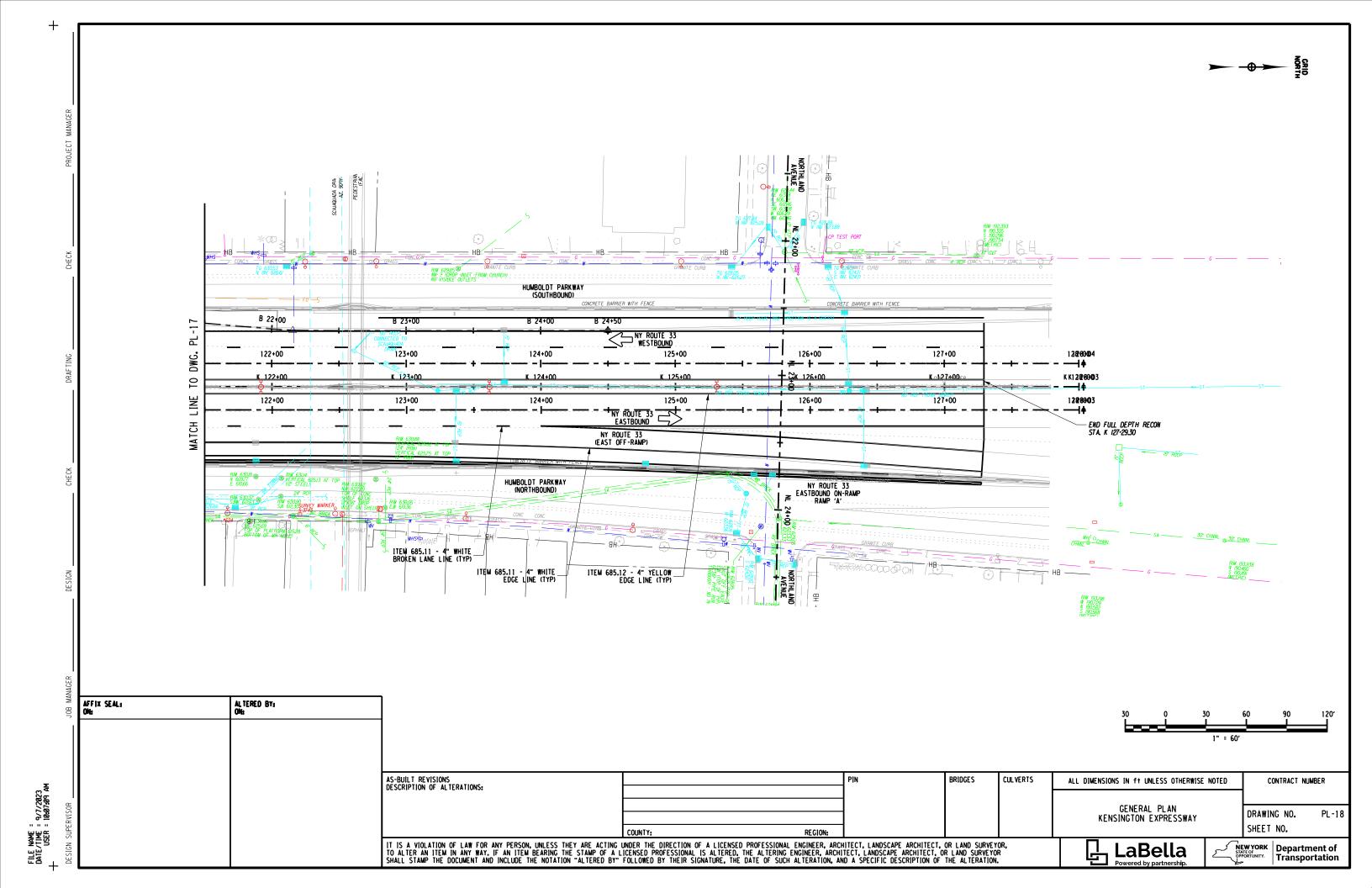


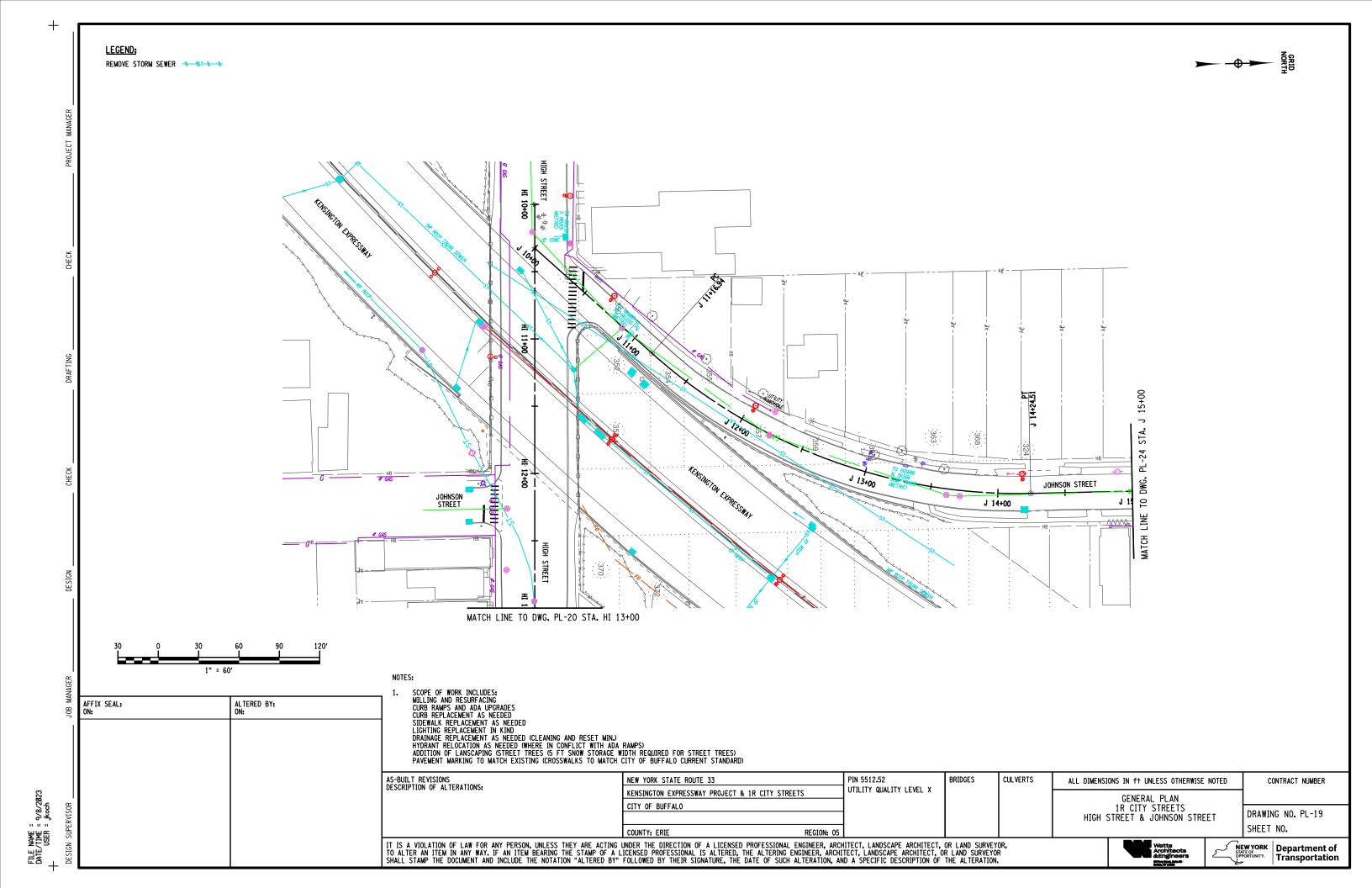


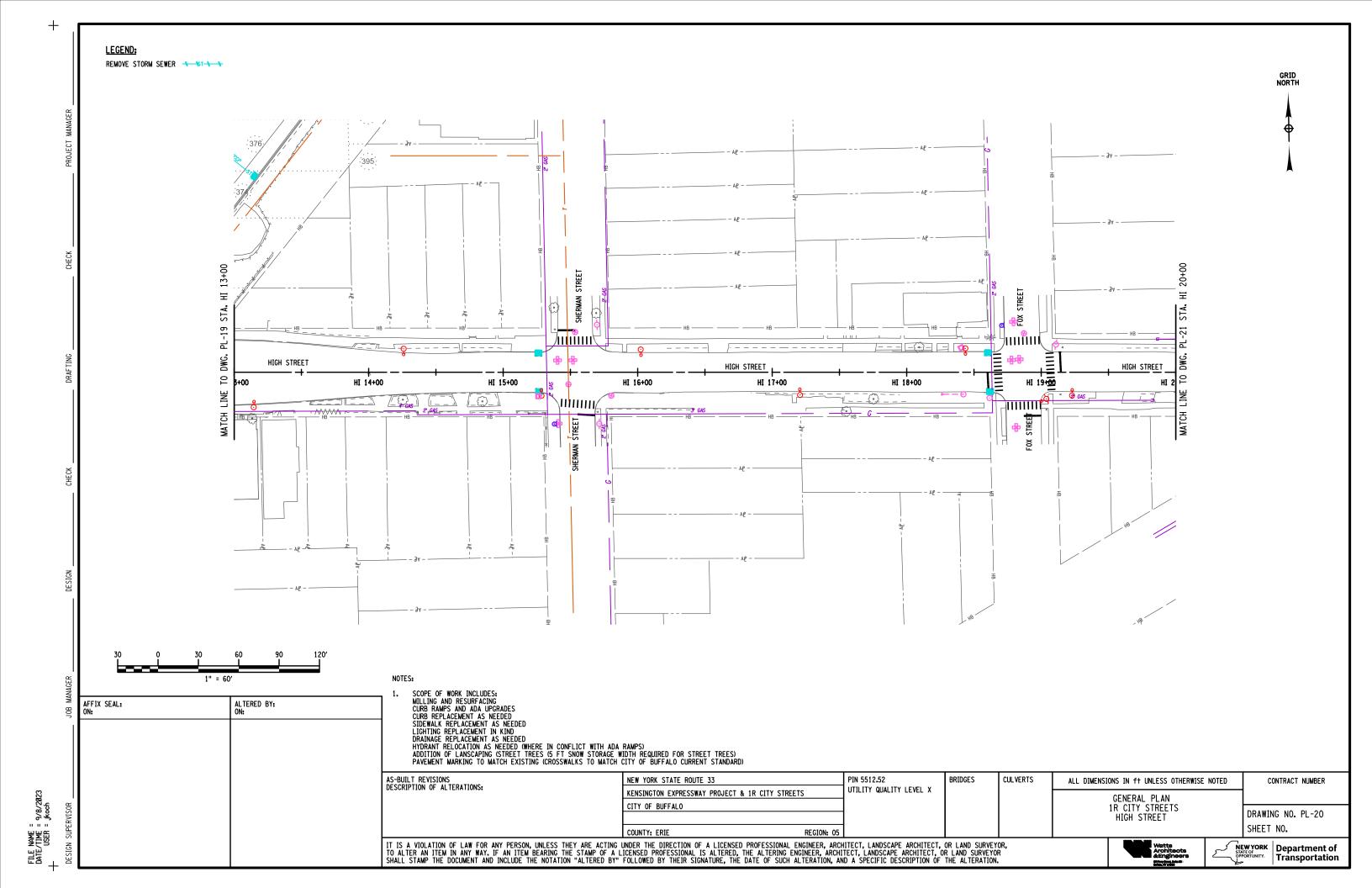
NORTH NORTH HUMBOLDT (PAR) (SOUTHBOUN MATCH EF 23+00 EF 23+00 I FERRY STRE 3.LN. 1022640 NY ROUTE 33 WESTBOUND NY ROUTE 33 WESTBOUND 113+00 112*00 DWG. 0 112+00 NY ROUTE 33 EASTBOUND NY ROUTE 33 HUMBOLDT PARKWAY (NORTHBOUND) ITEM 685.12 - 4" YELLOW _ EDGE LINE (TYP) ITEM 685.11 - 4" WHITE EDGE LINE (TYP) ITEM 685.11 - 4" WHITE BROKEN LANE LINE (TYP) T 25+00 AFFIX SEAL: ALTERED BY: ON: AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: BRIDGES CULVERTS PIN ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER FILE NAME = 077/2023 DATE/TIME = 9/7/2023 + USER = 10:07:08 AM GENERAL PLAN KENSINGTON EXPRESSWAY DRAWING NO. PL-16 SHEET NO. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. LaBella NEW YORK STATE OF OPPORTUNITY.

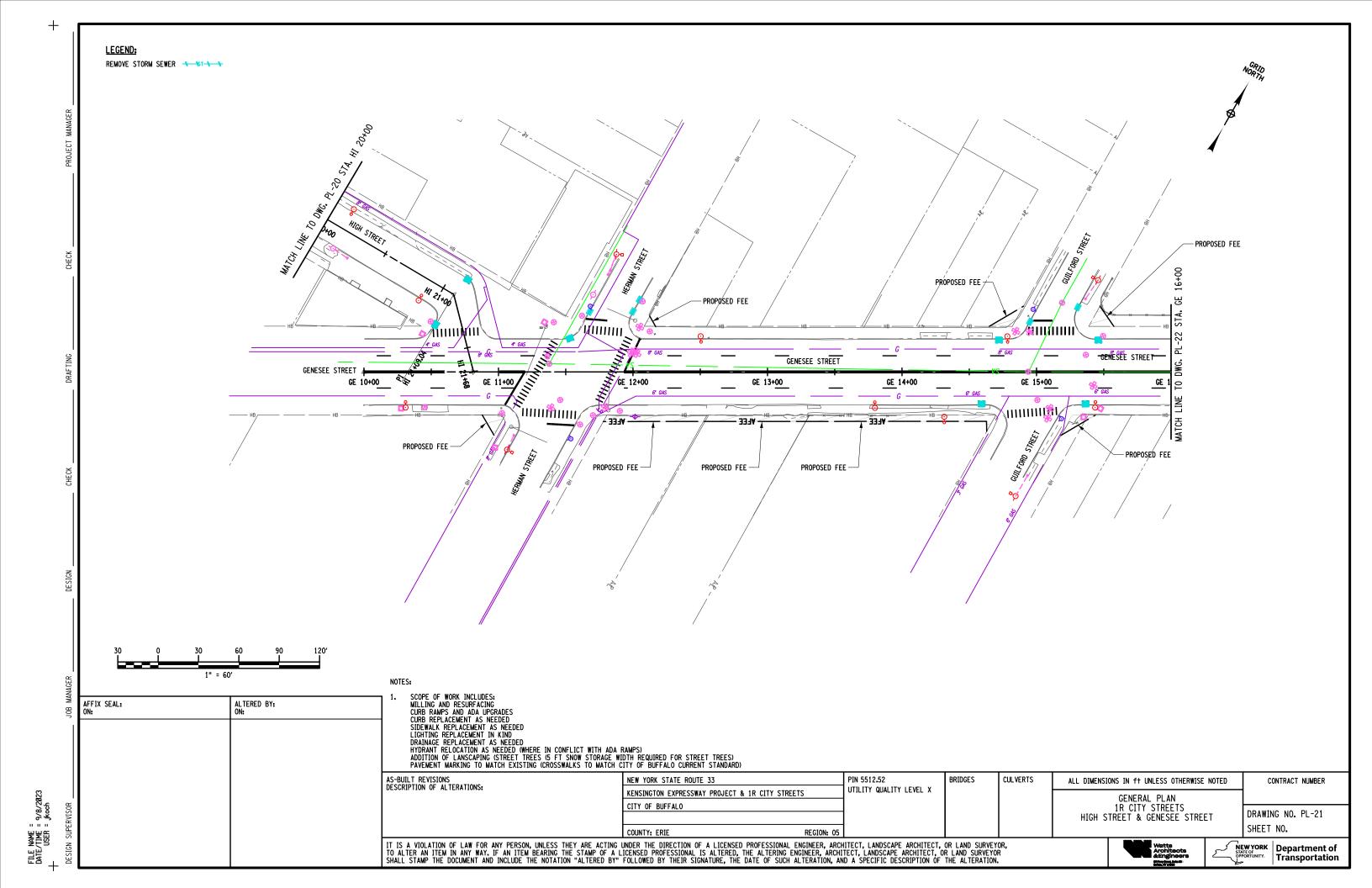
Department of Transportation

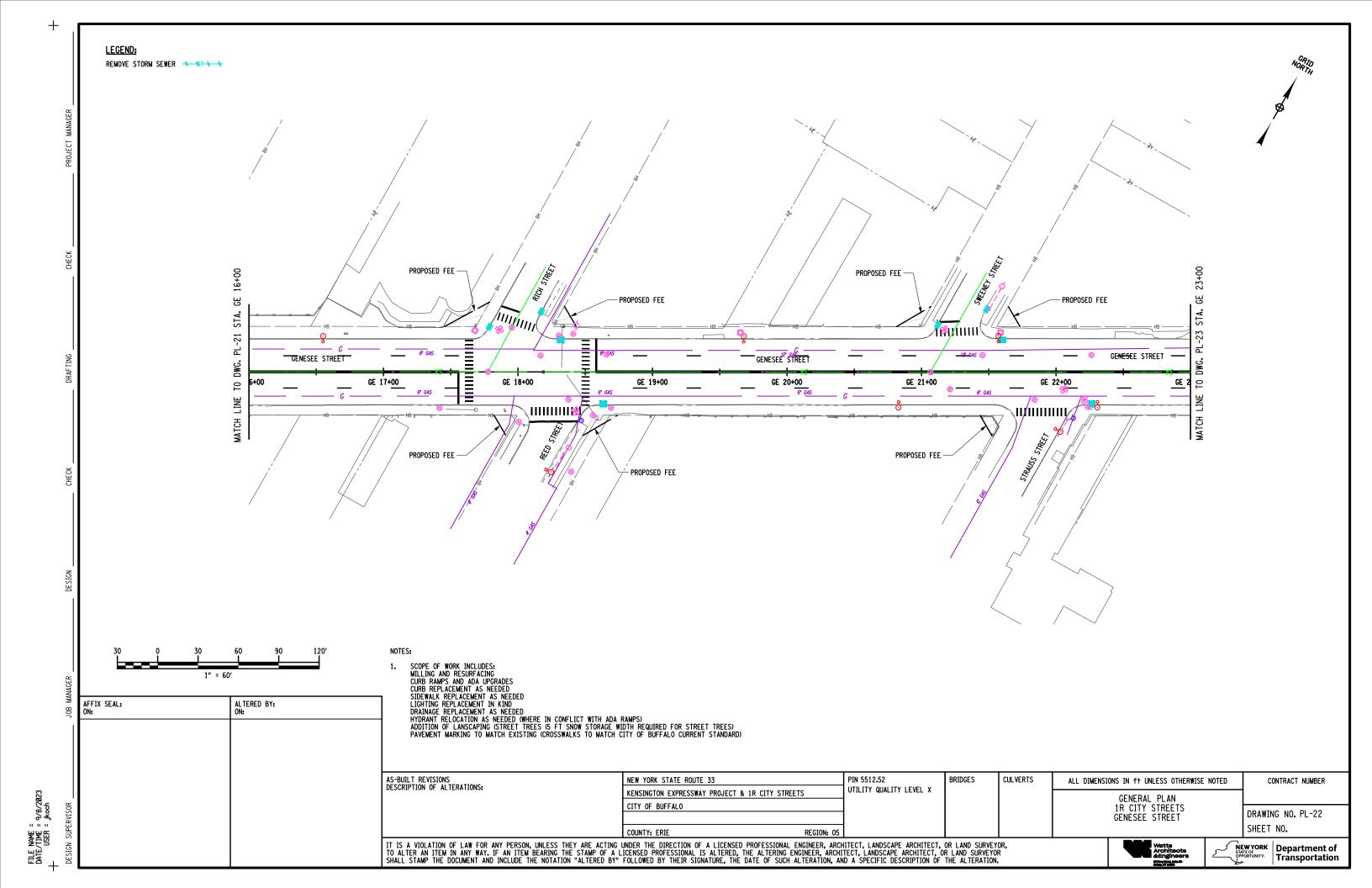


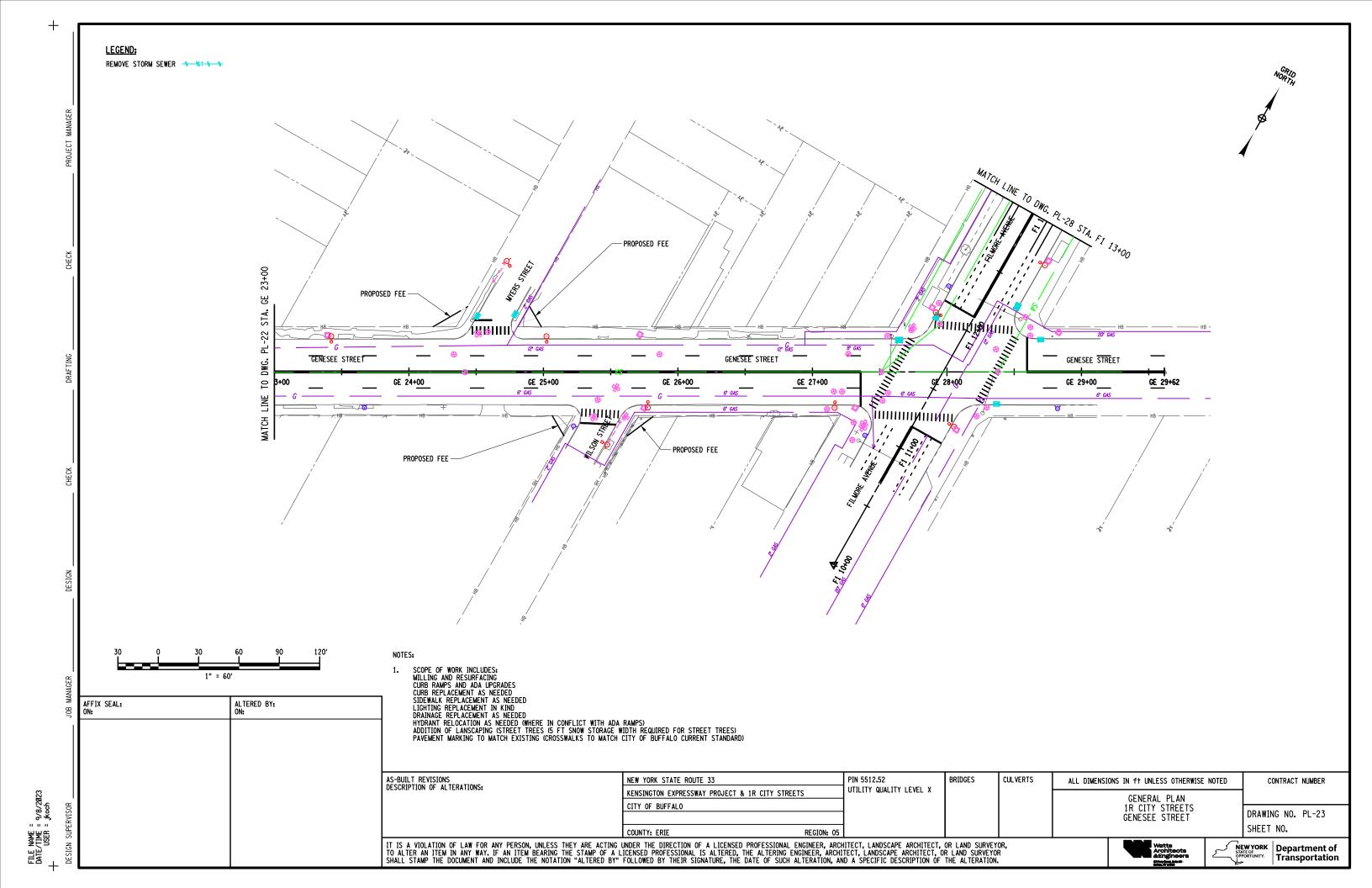


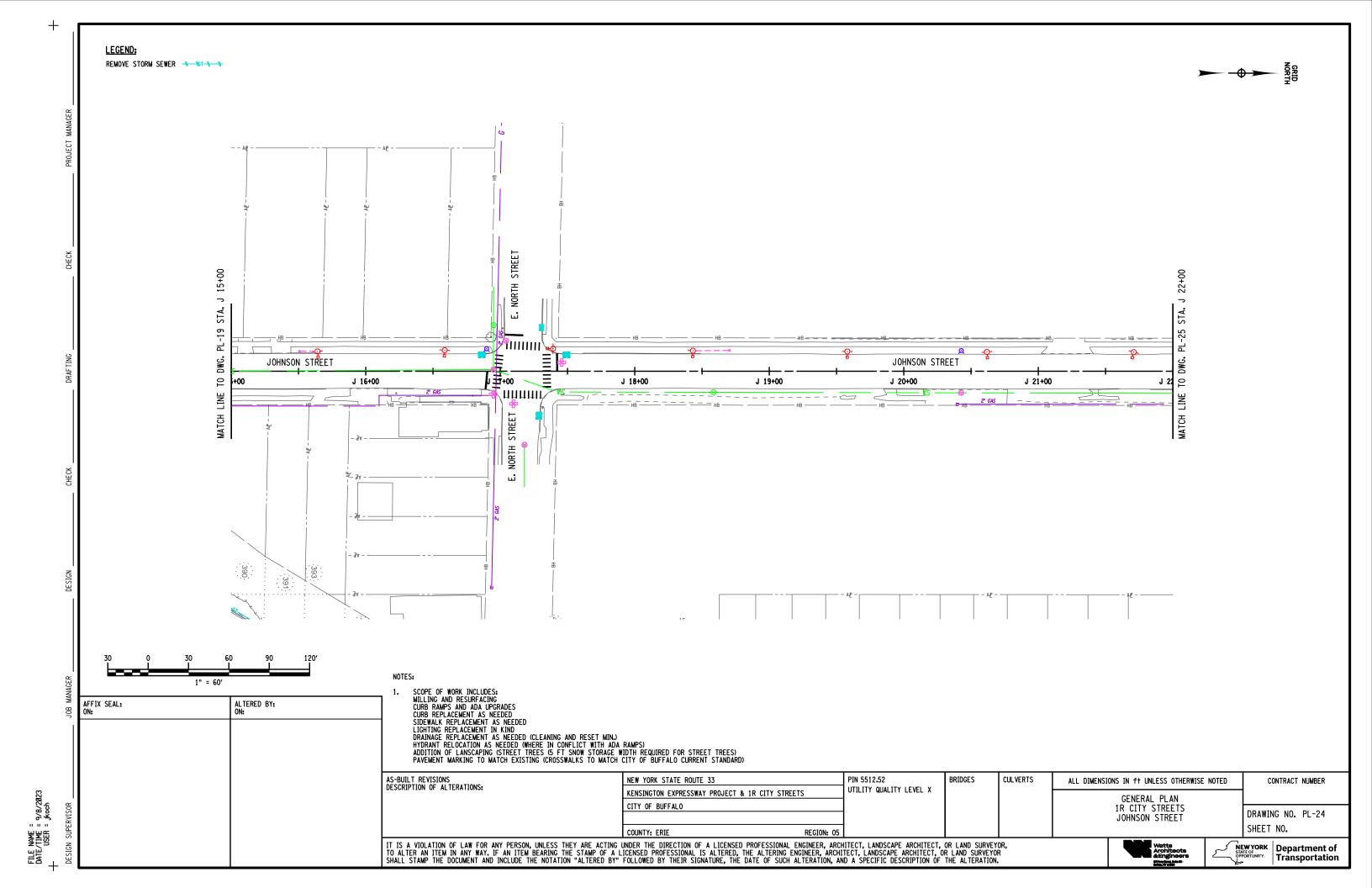


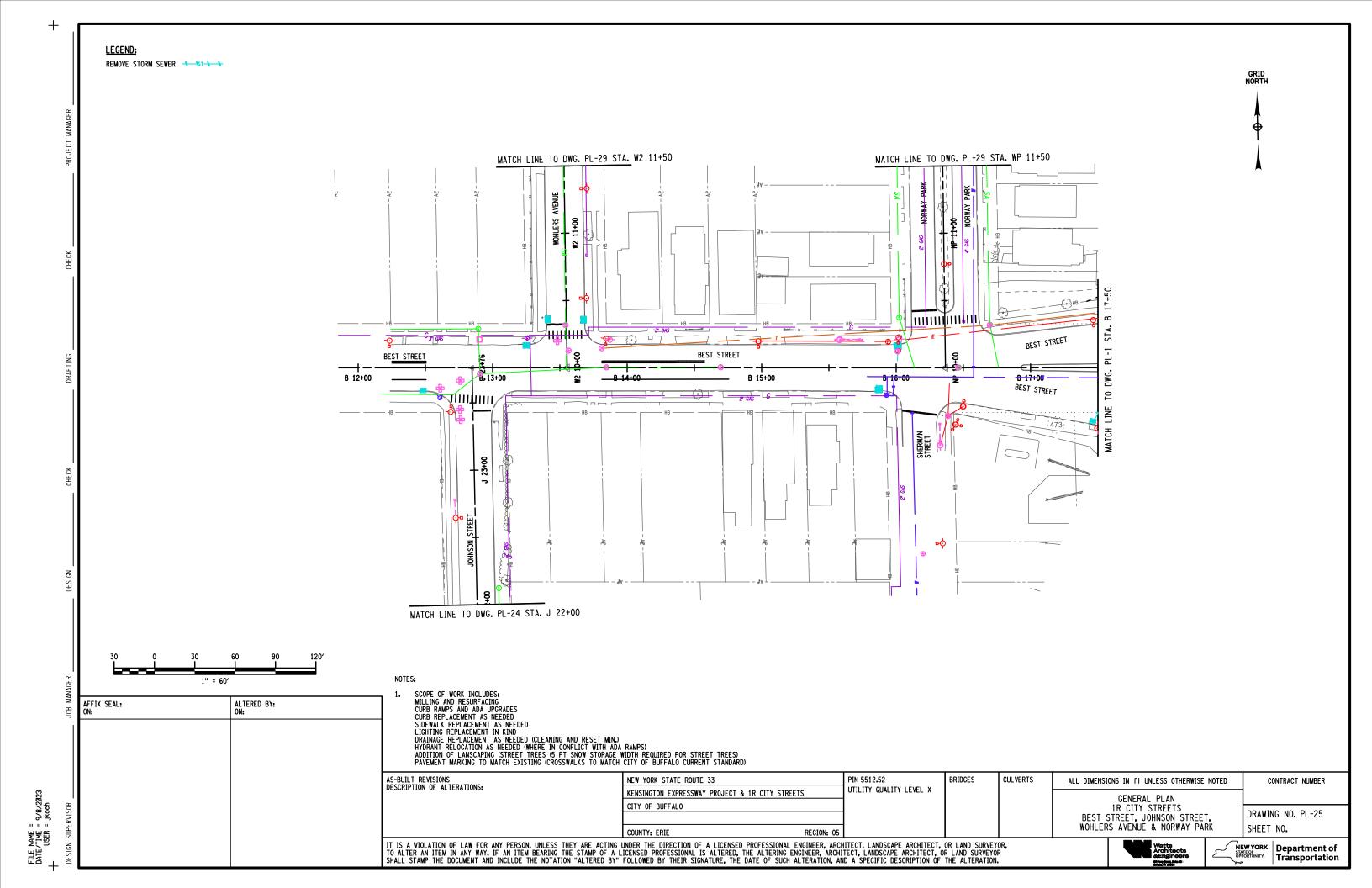


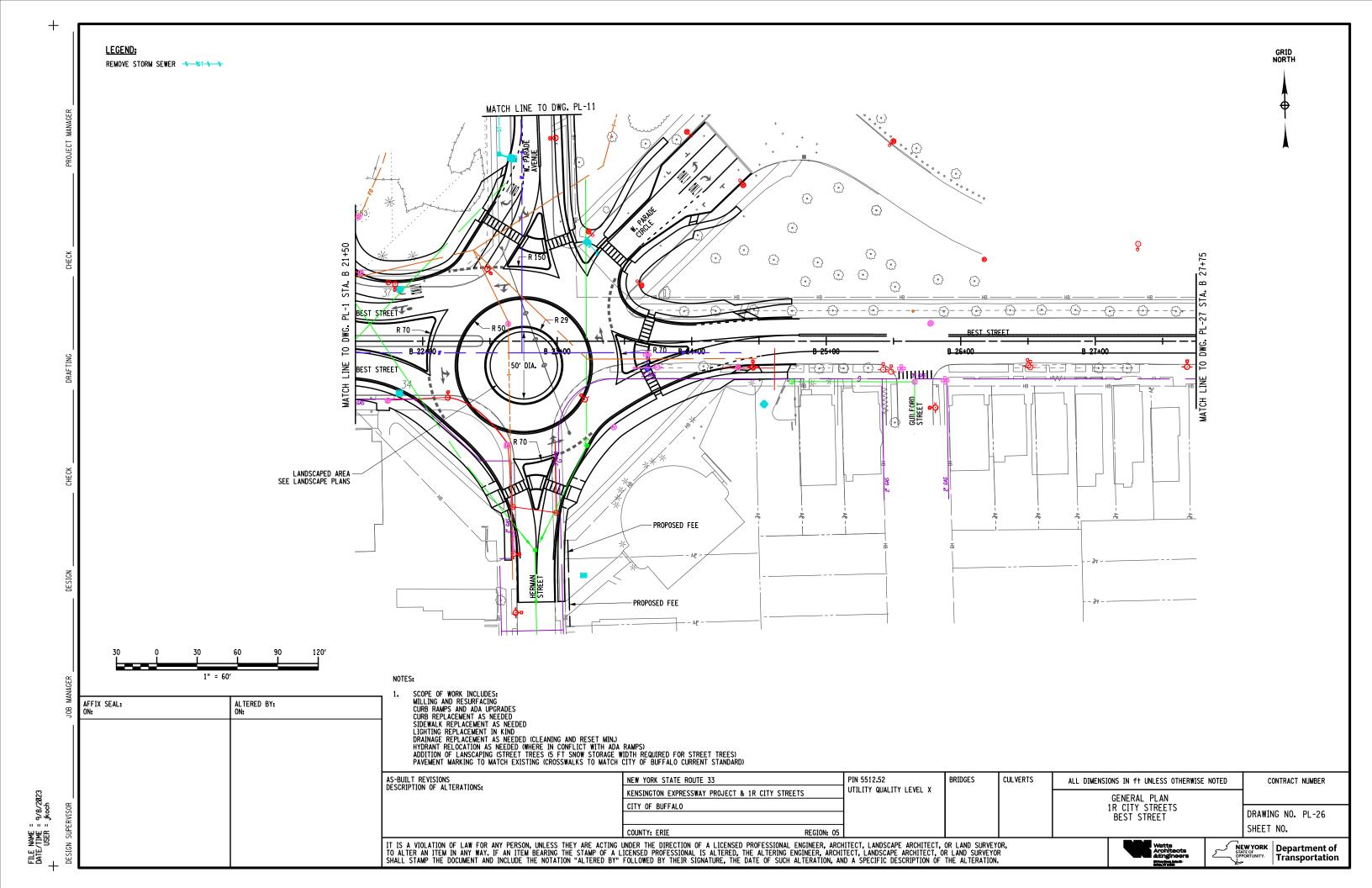


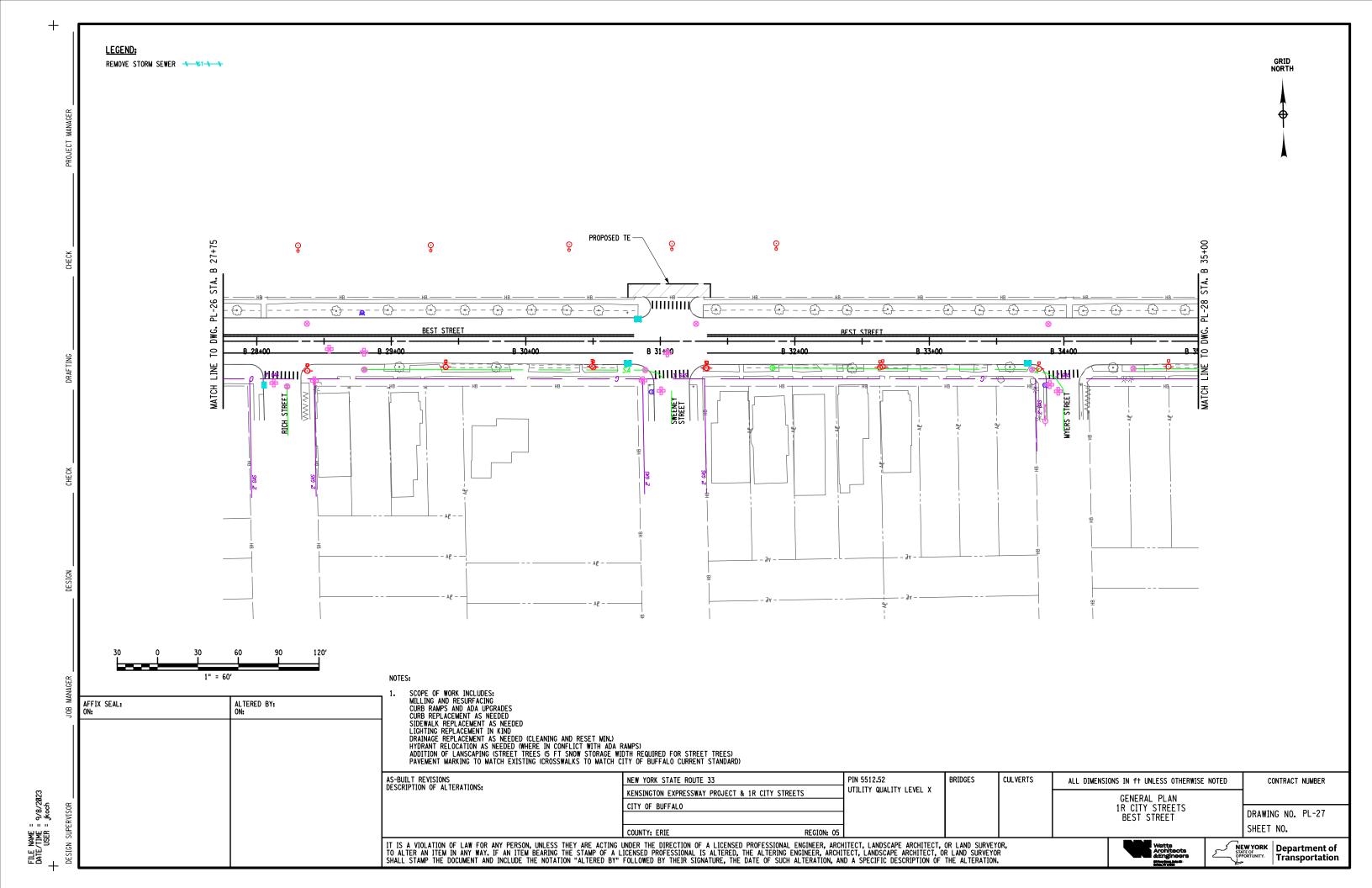


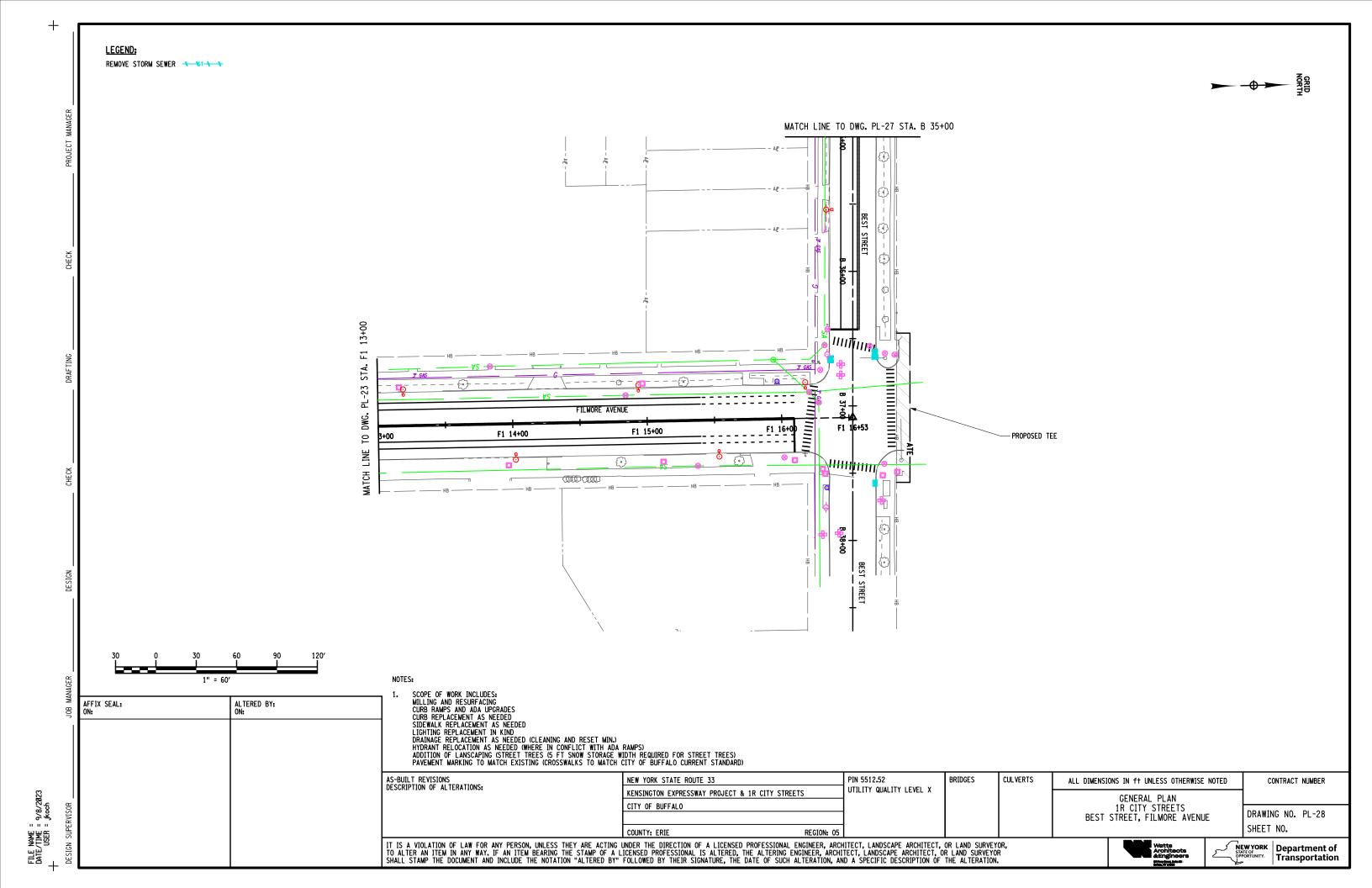


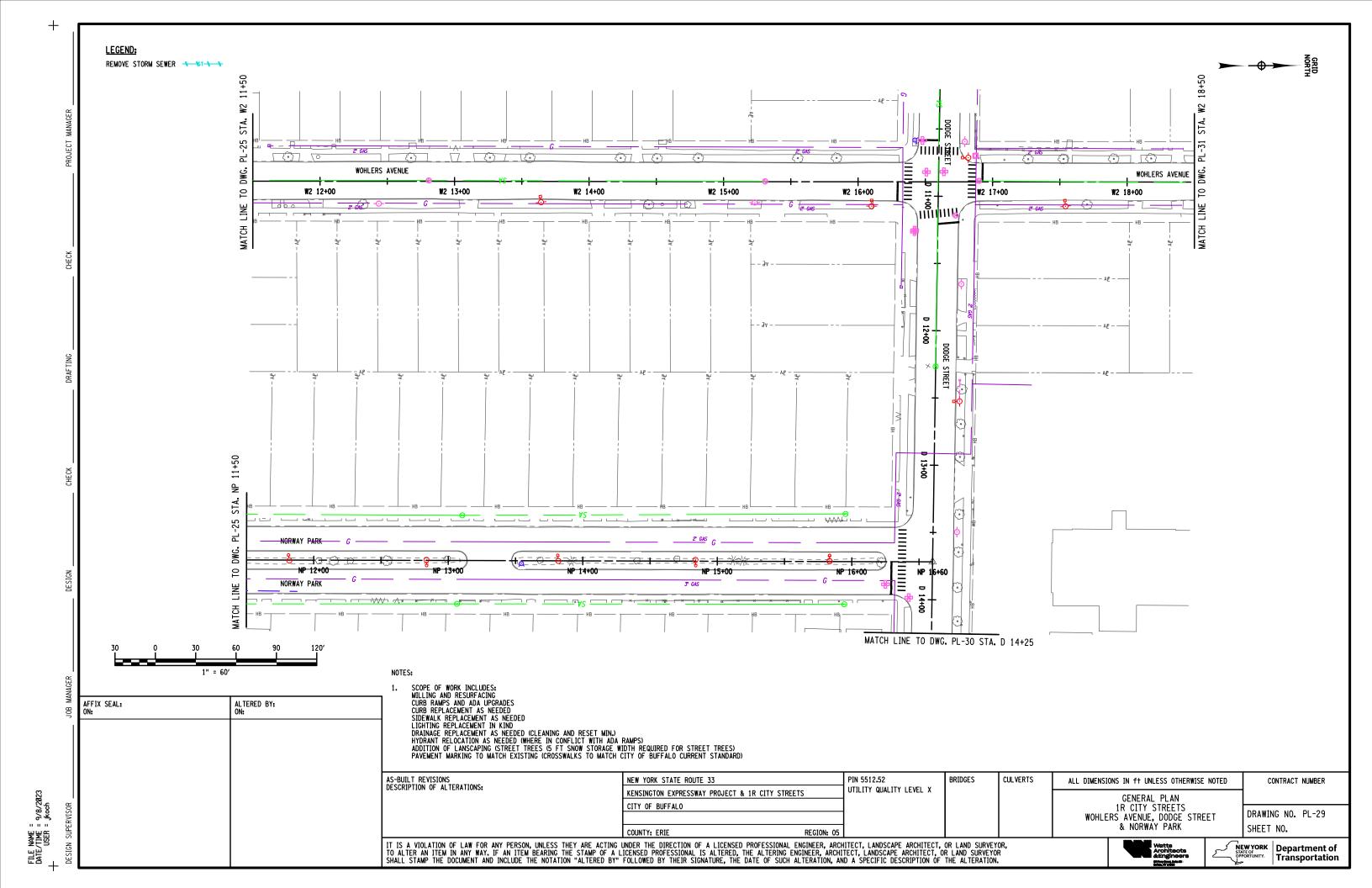


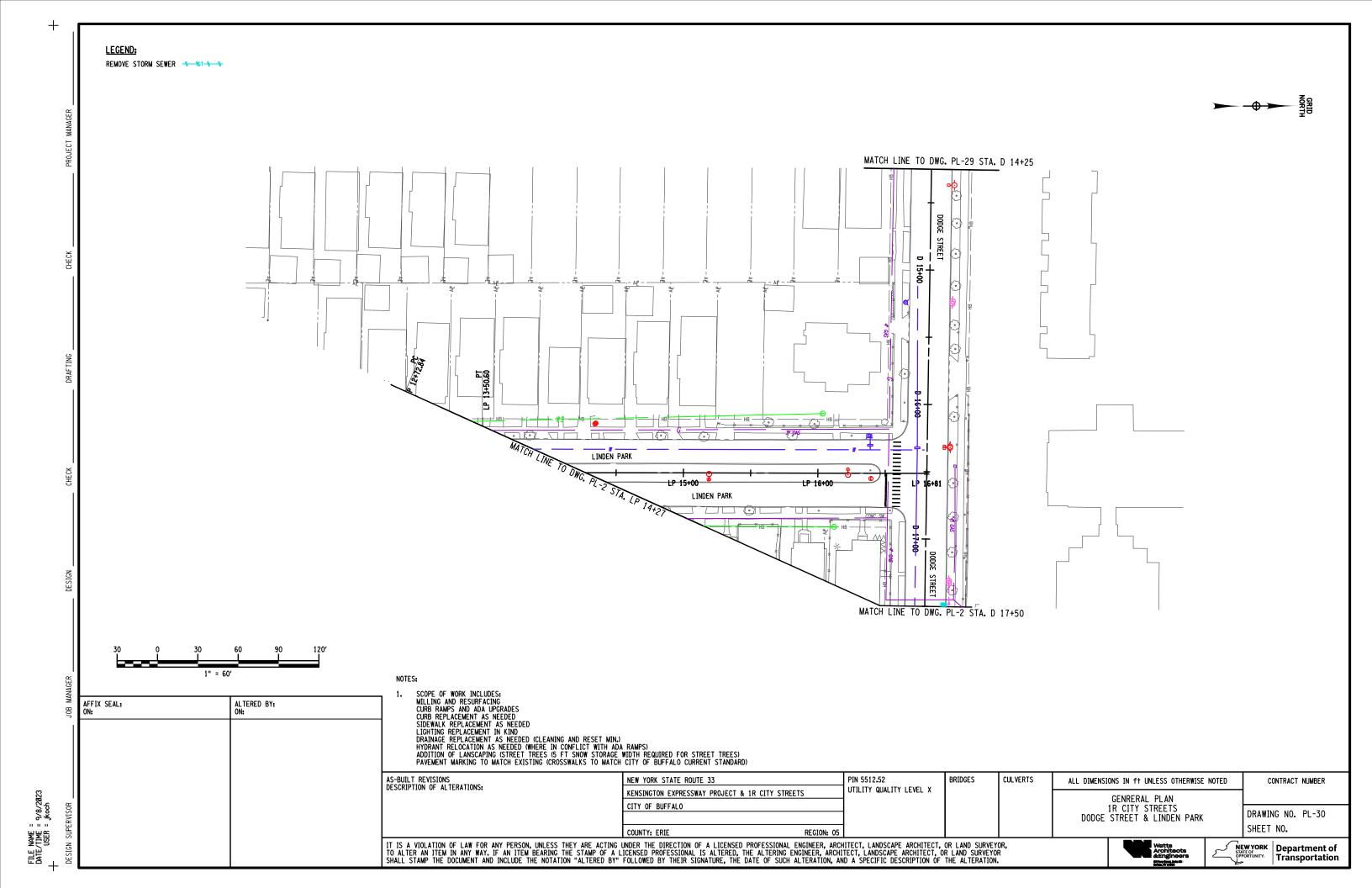


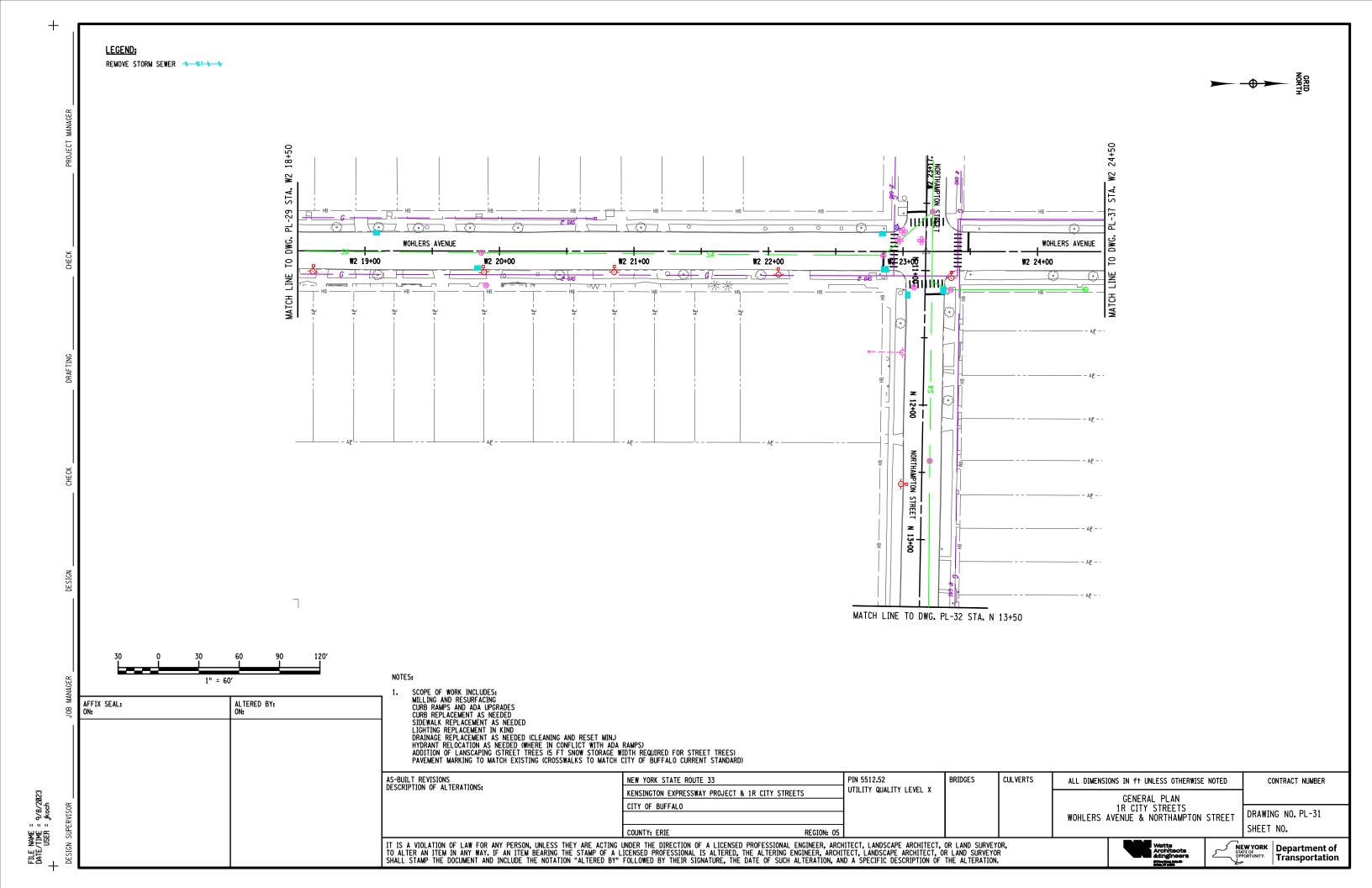


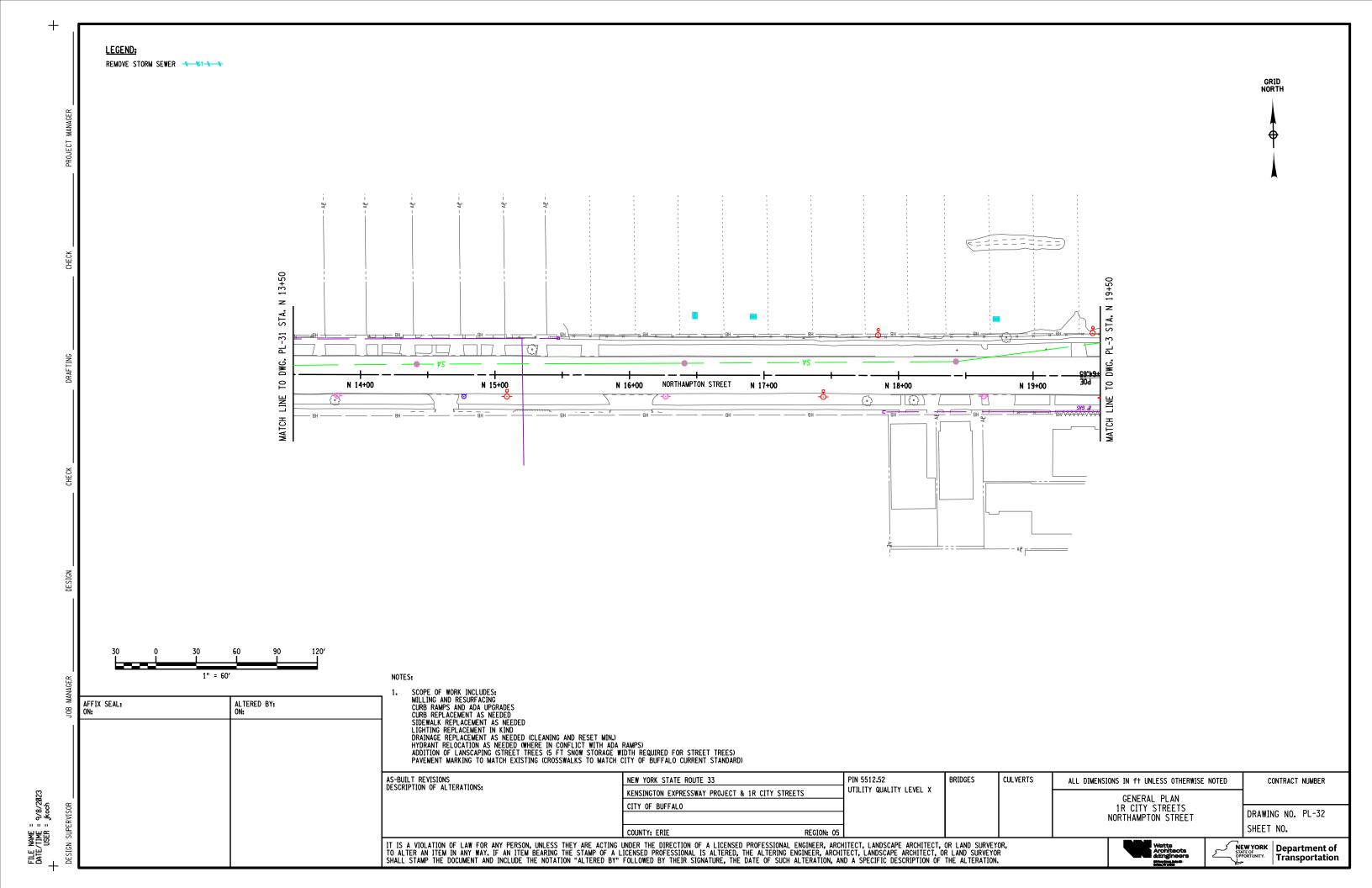


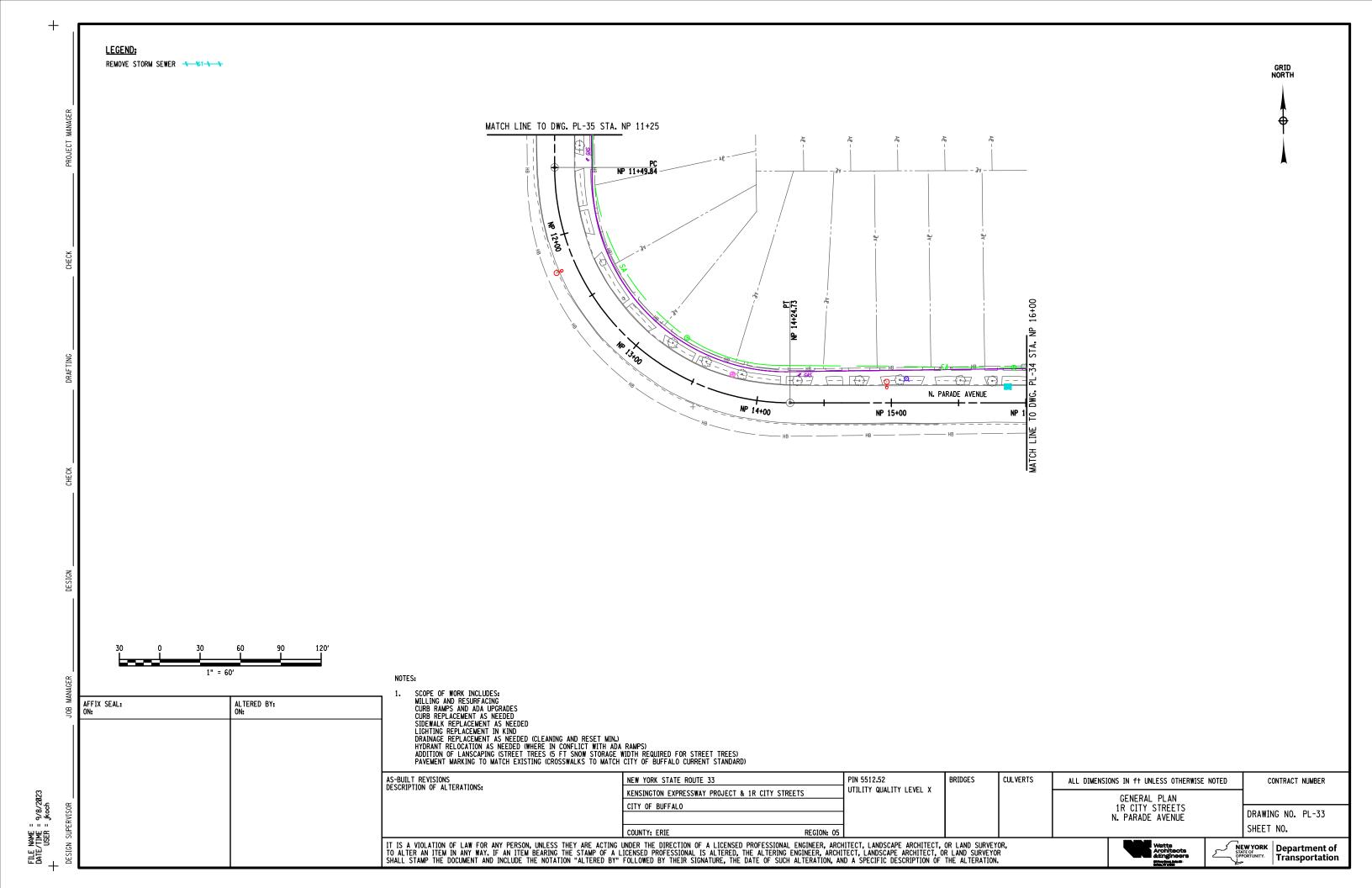


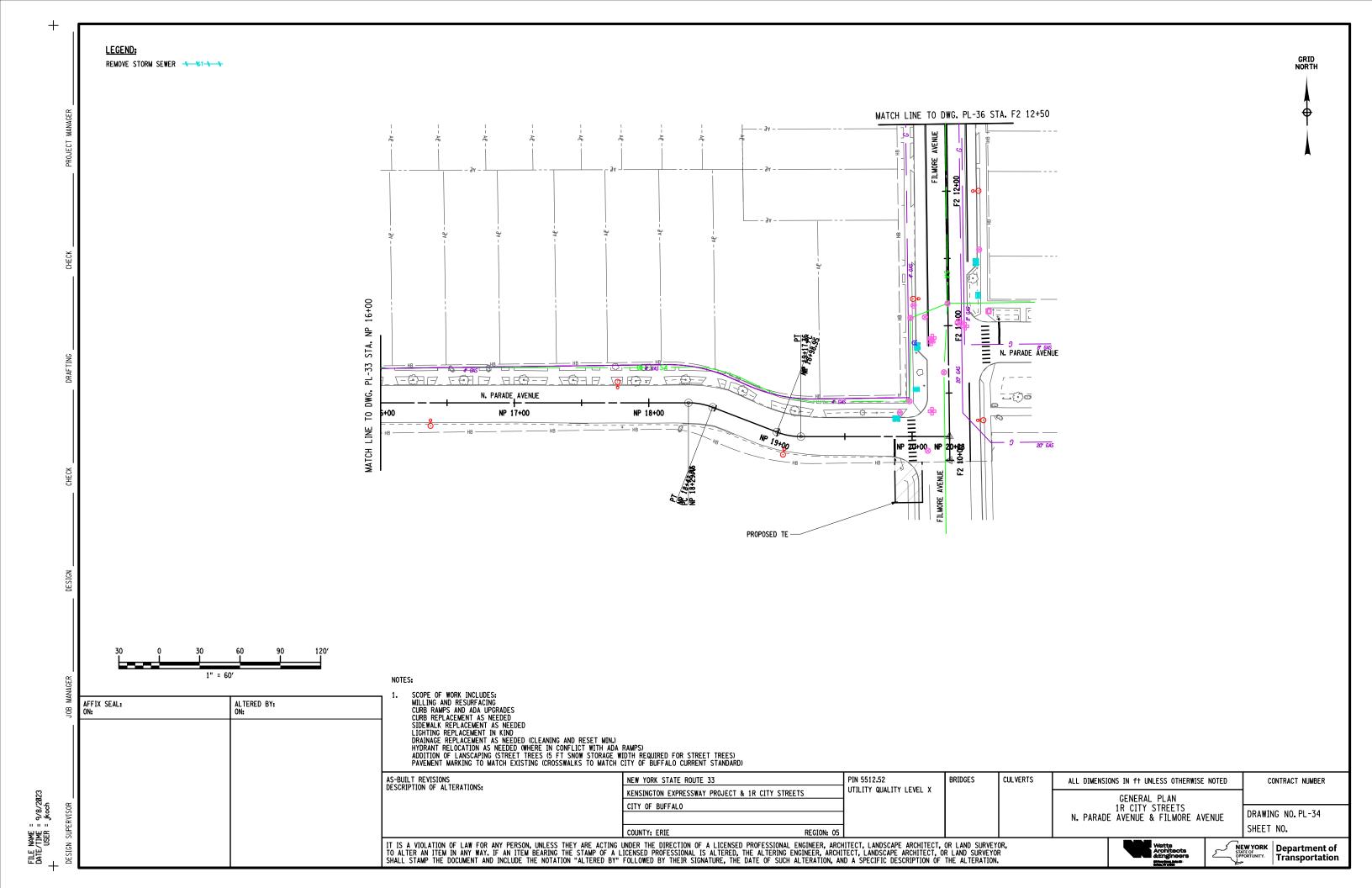


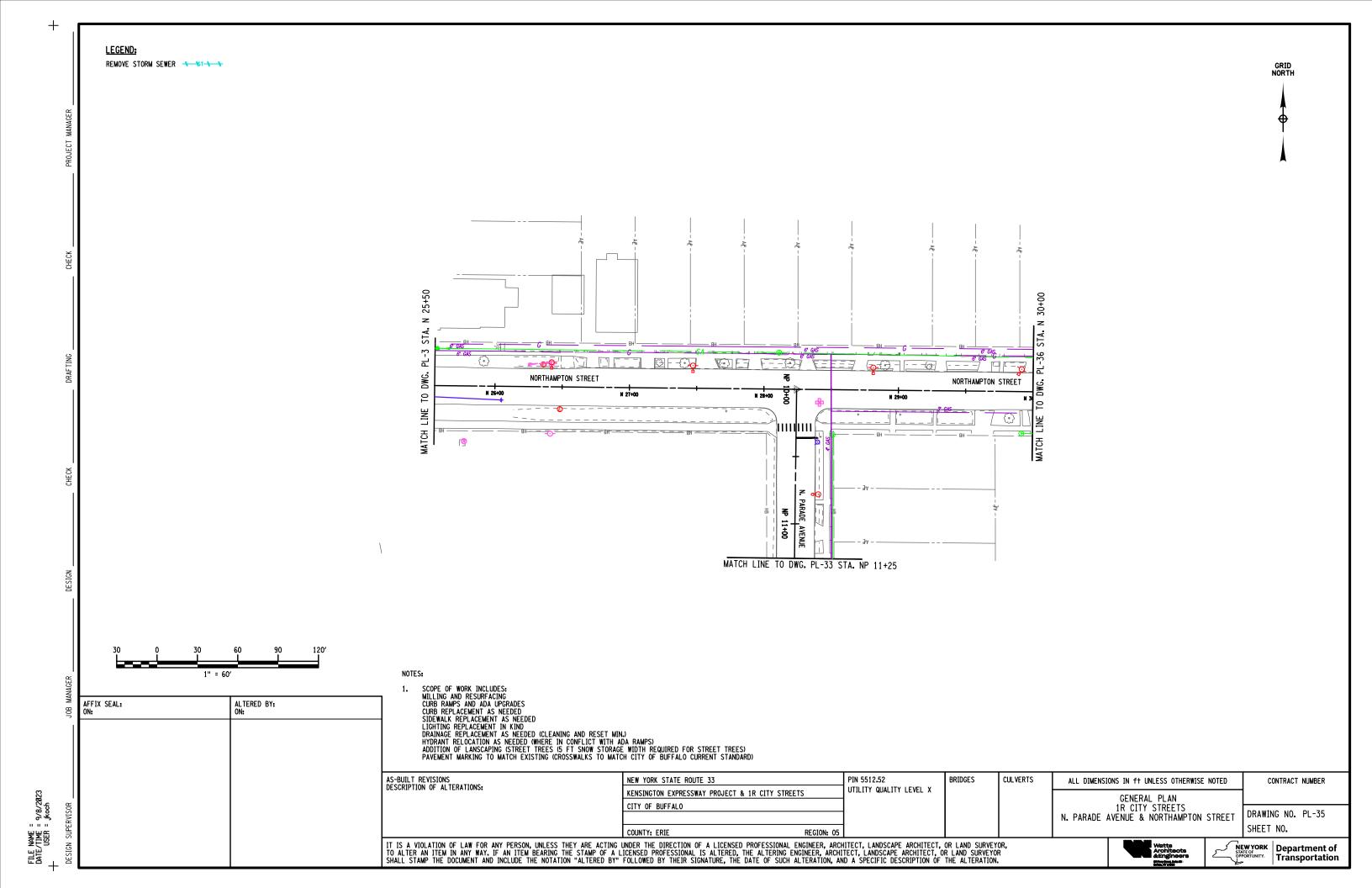


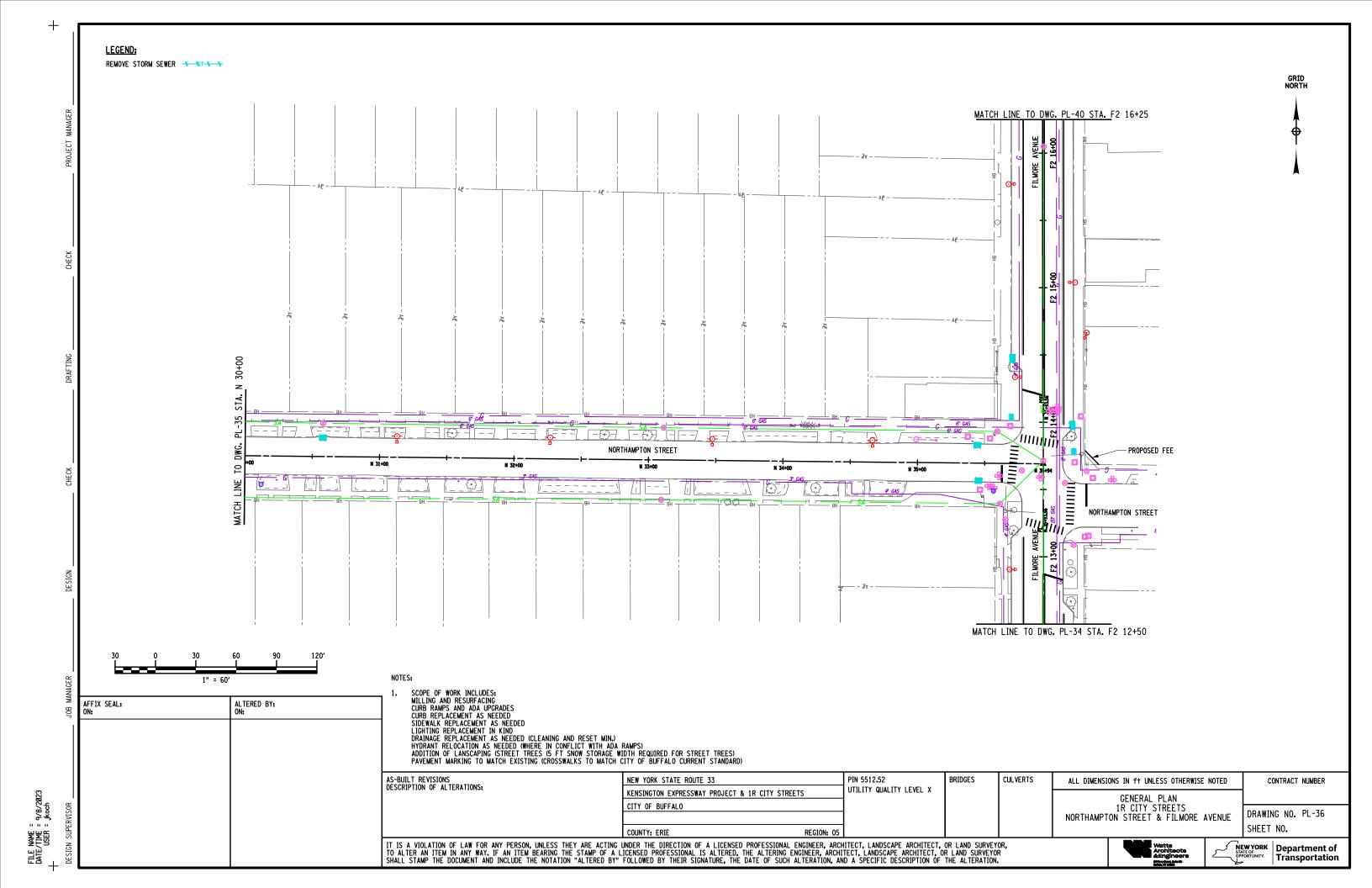


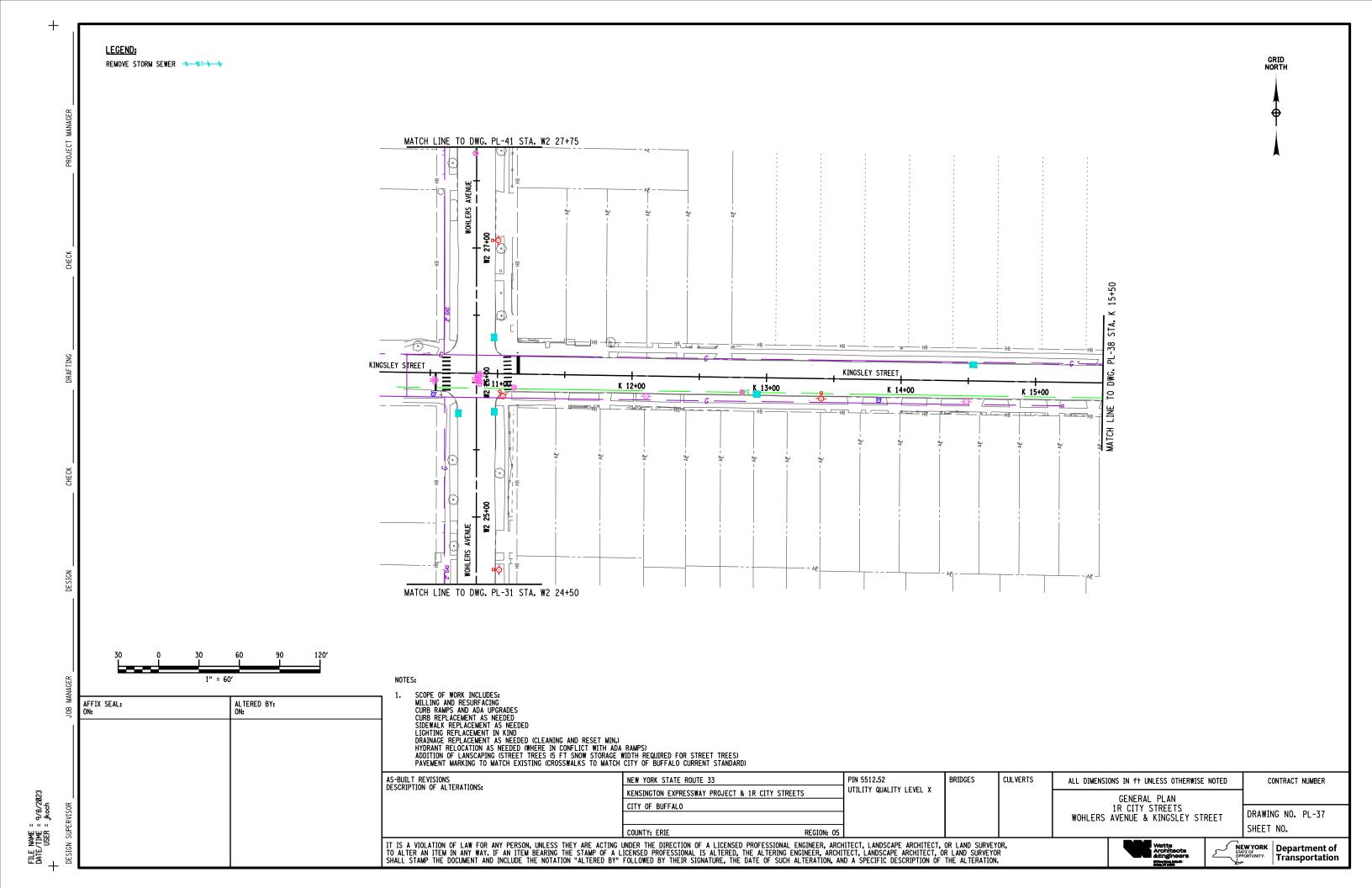


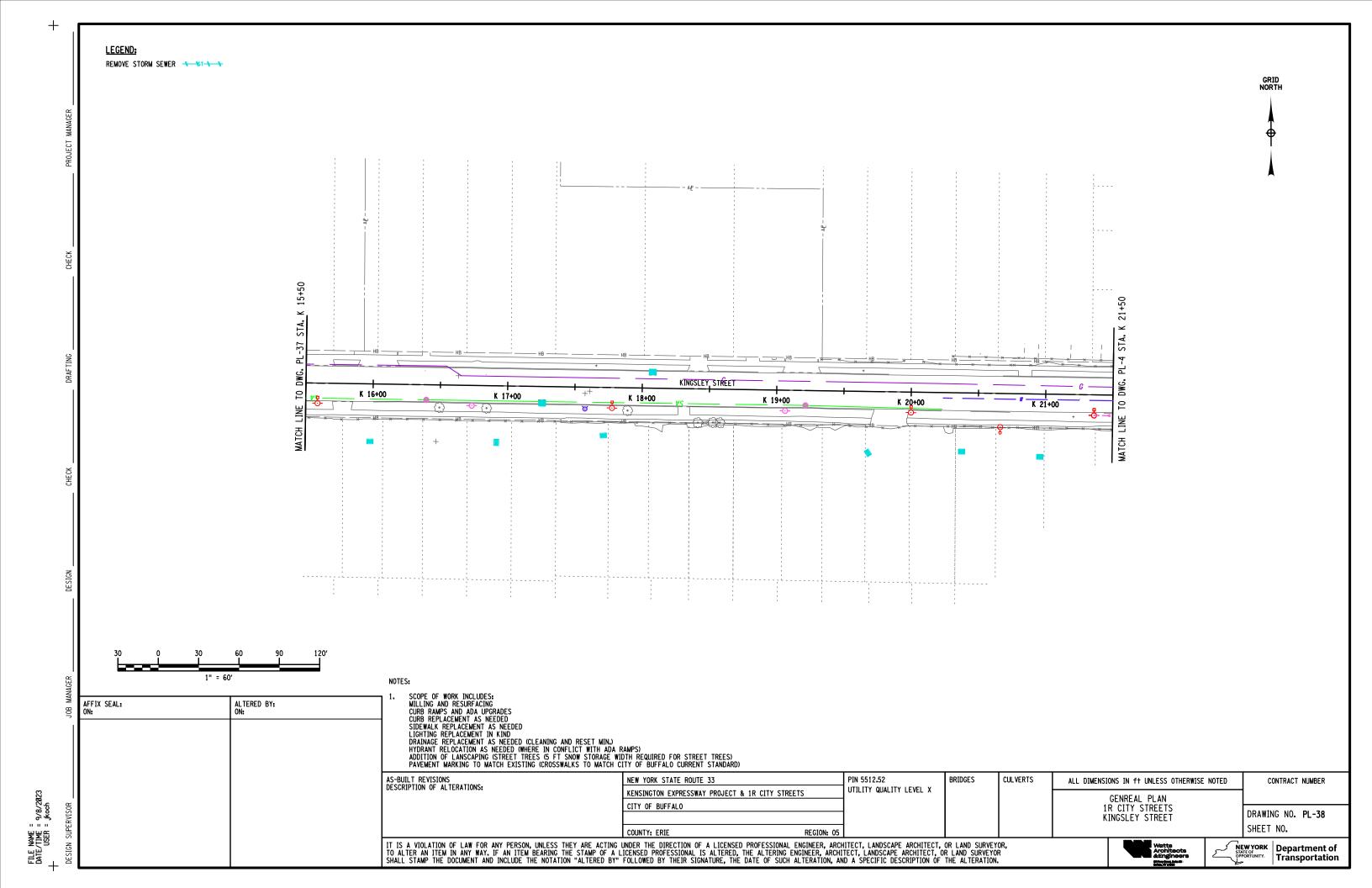


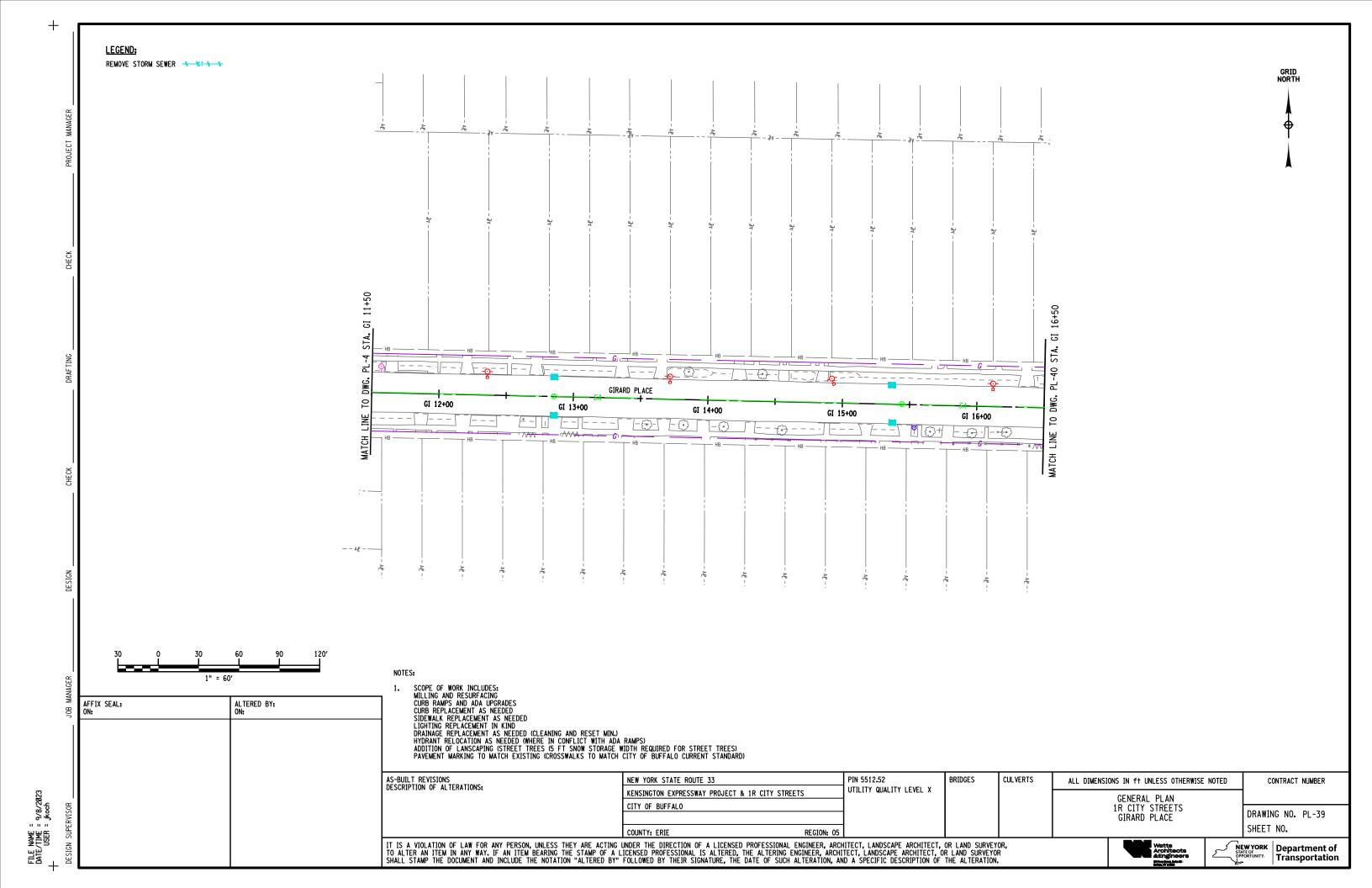


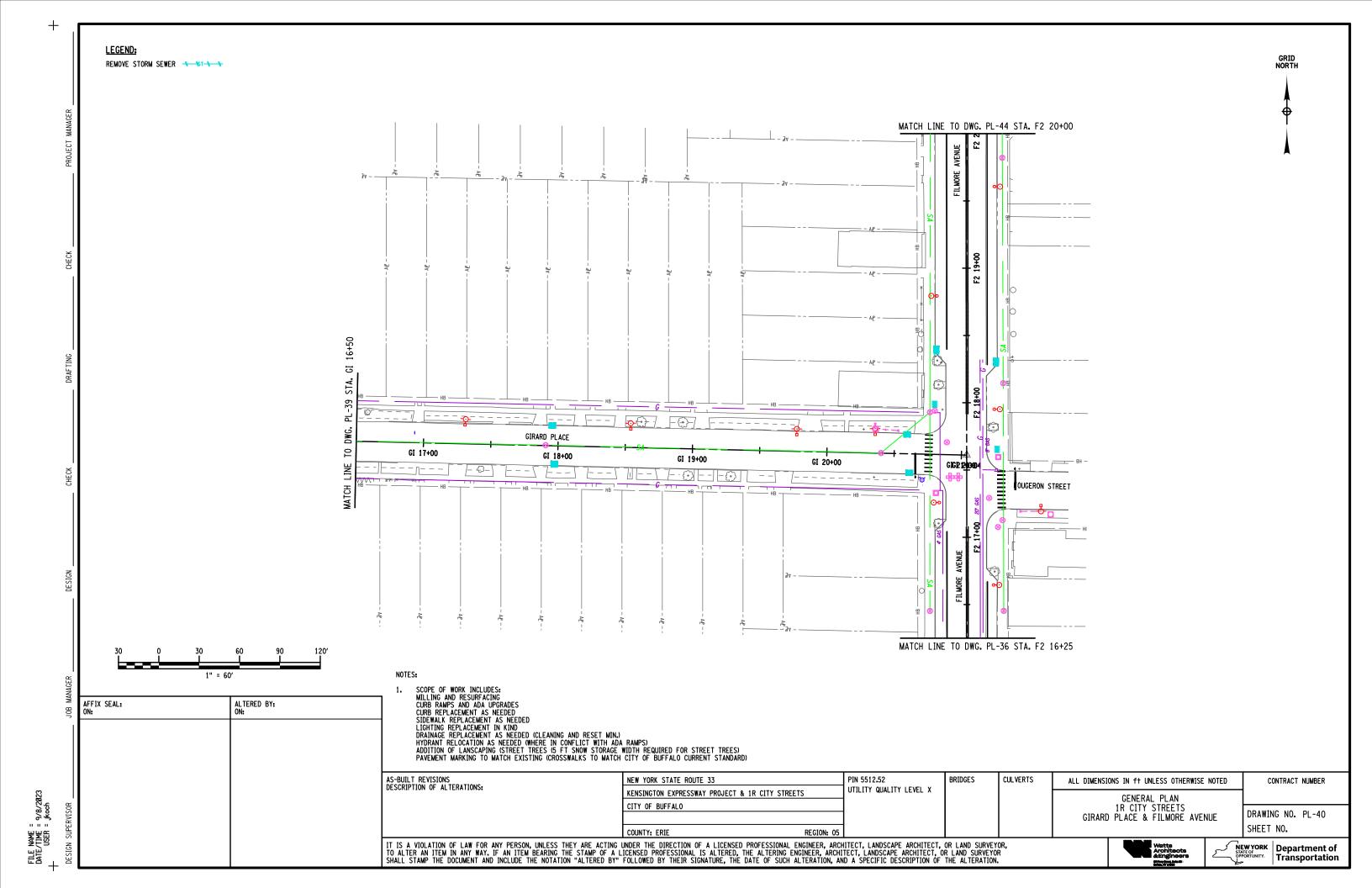


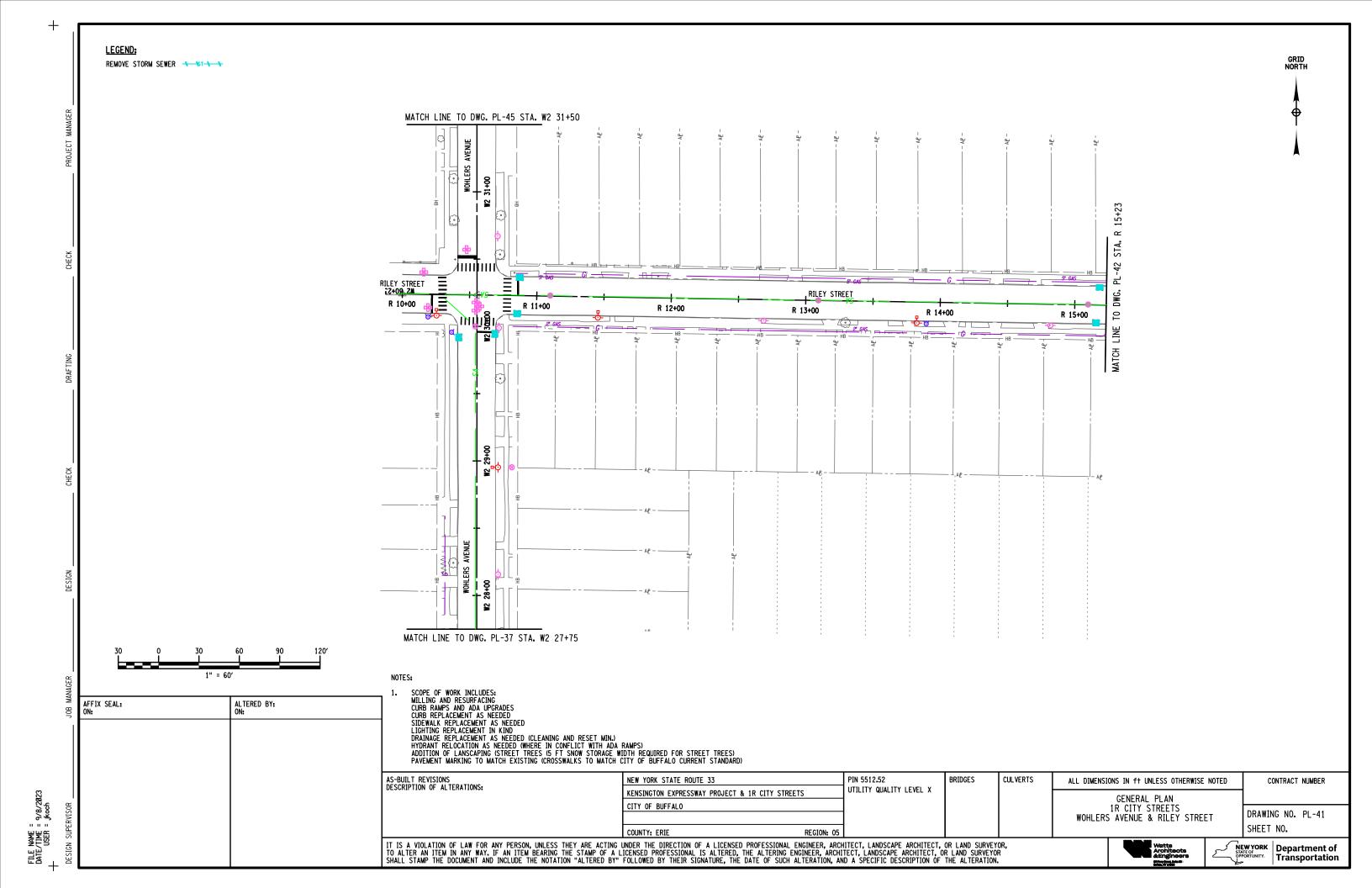


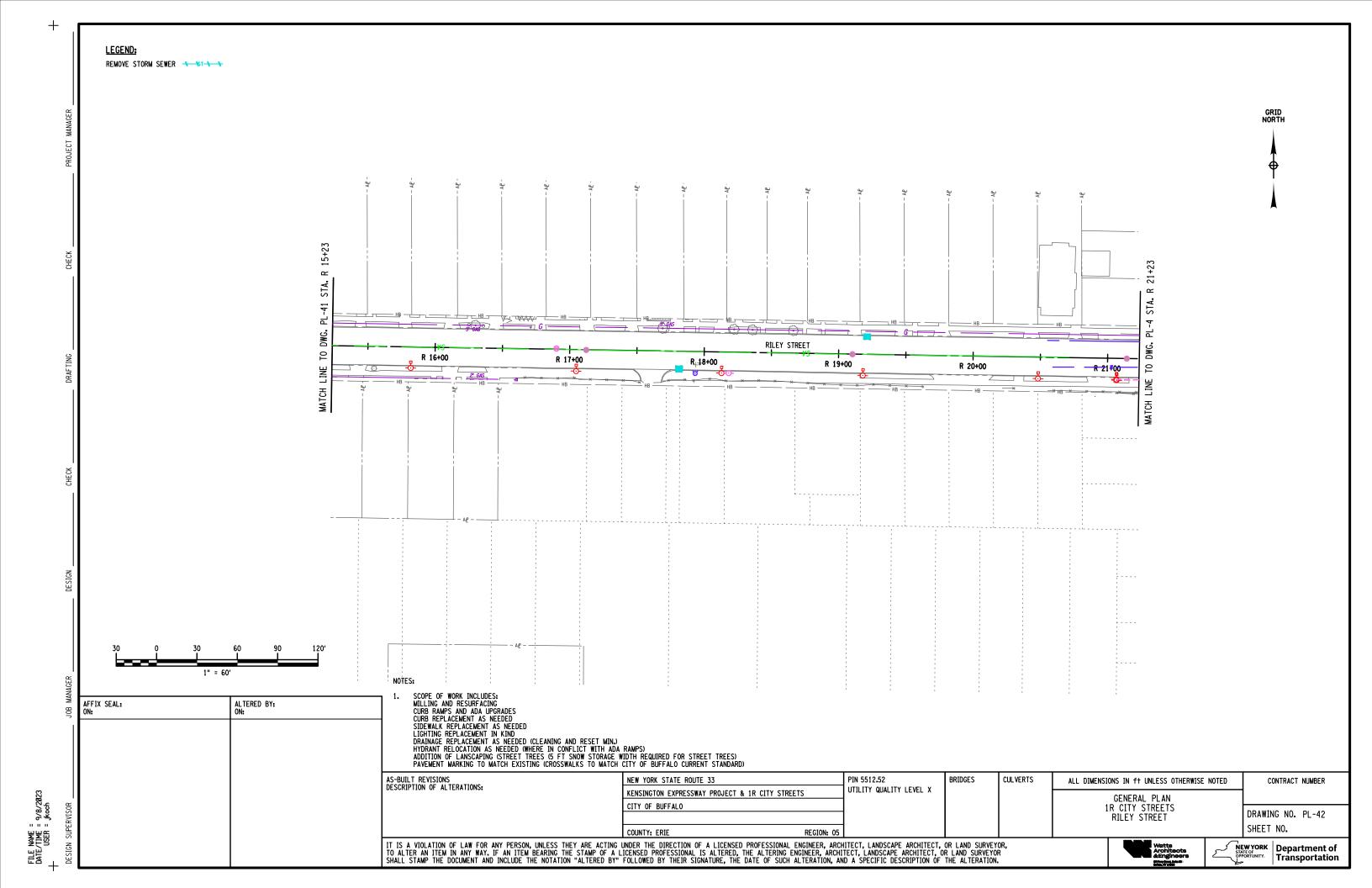


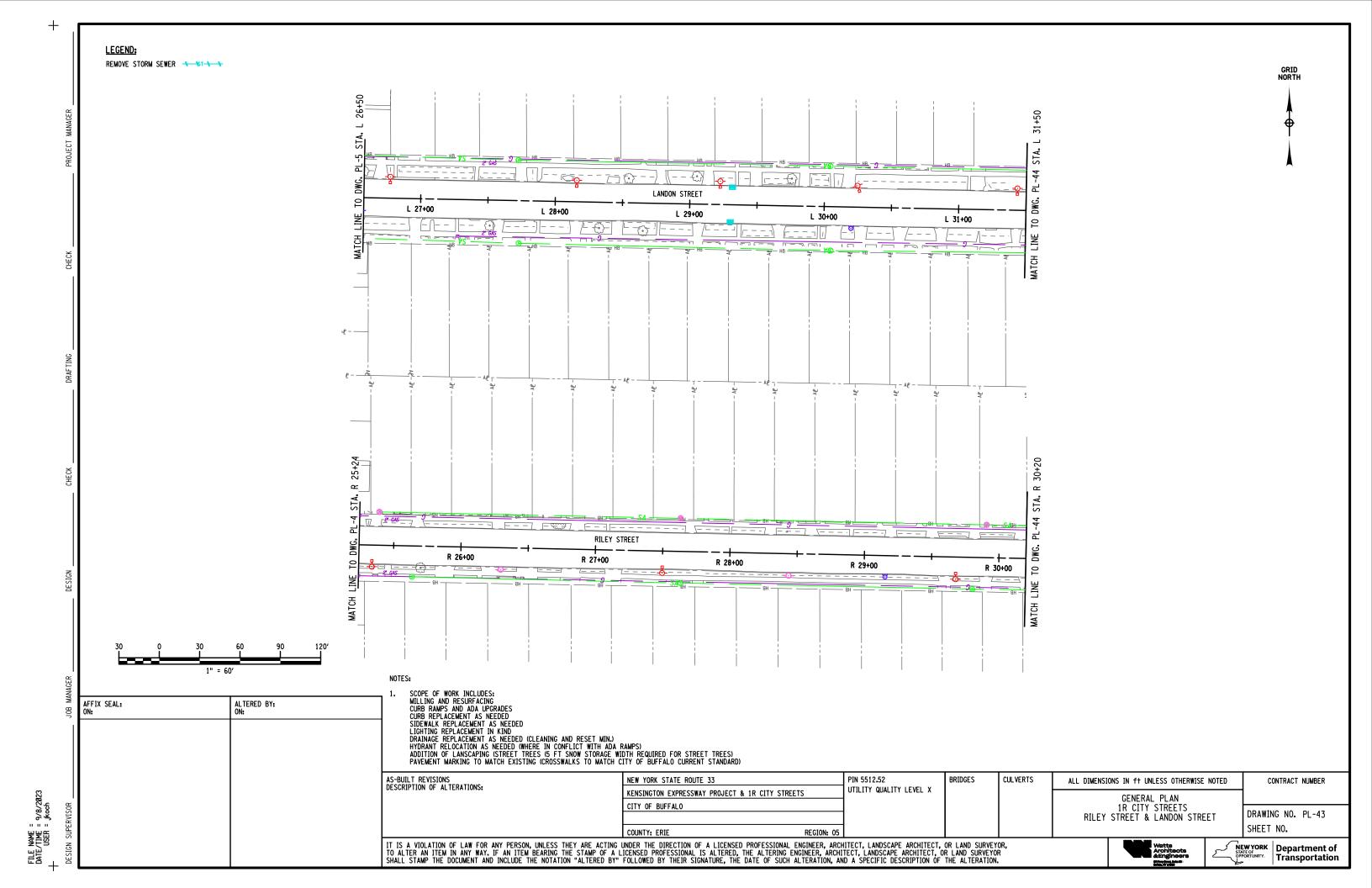


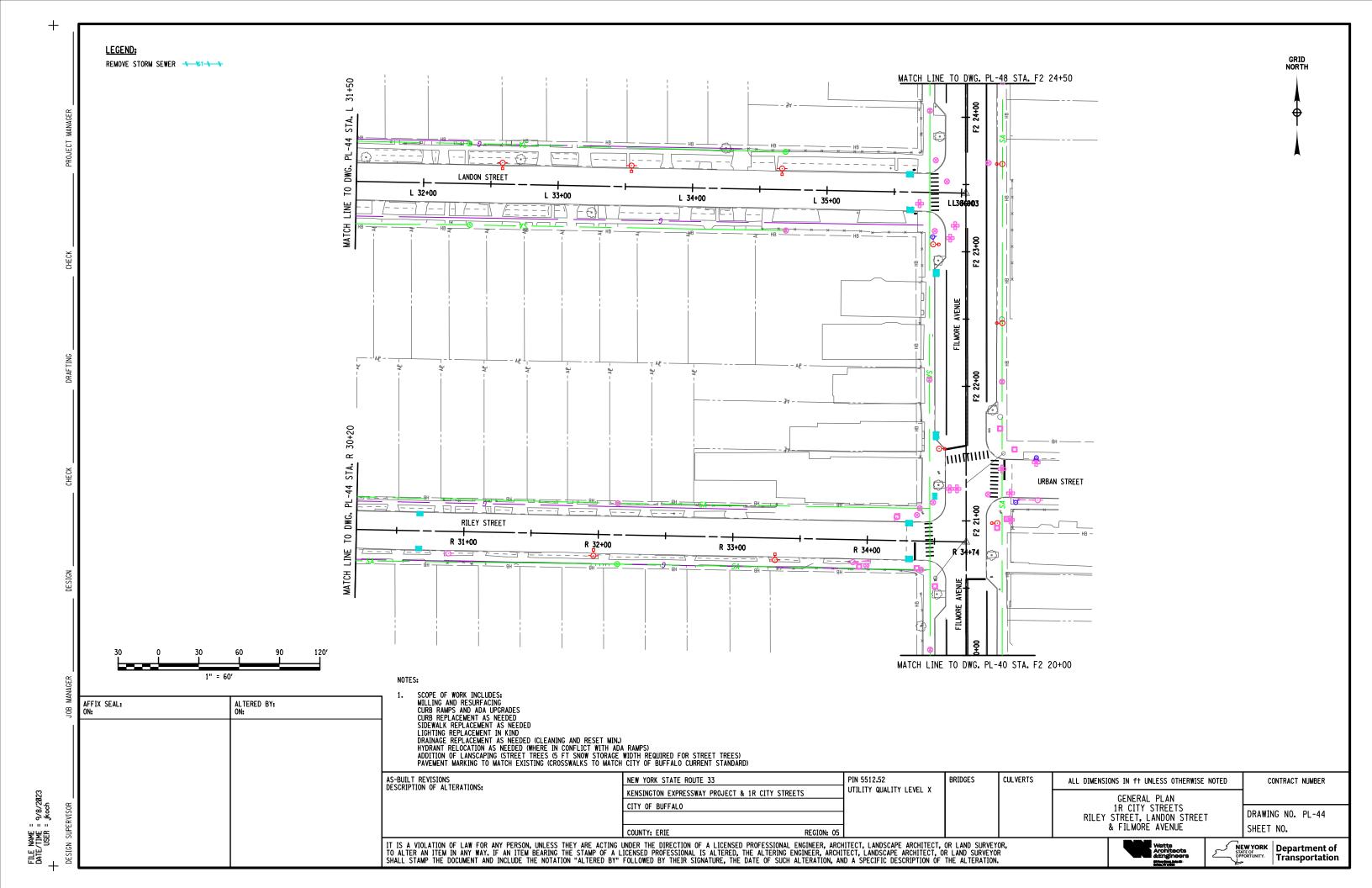


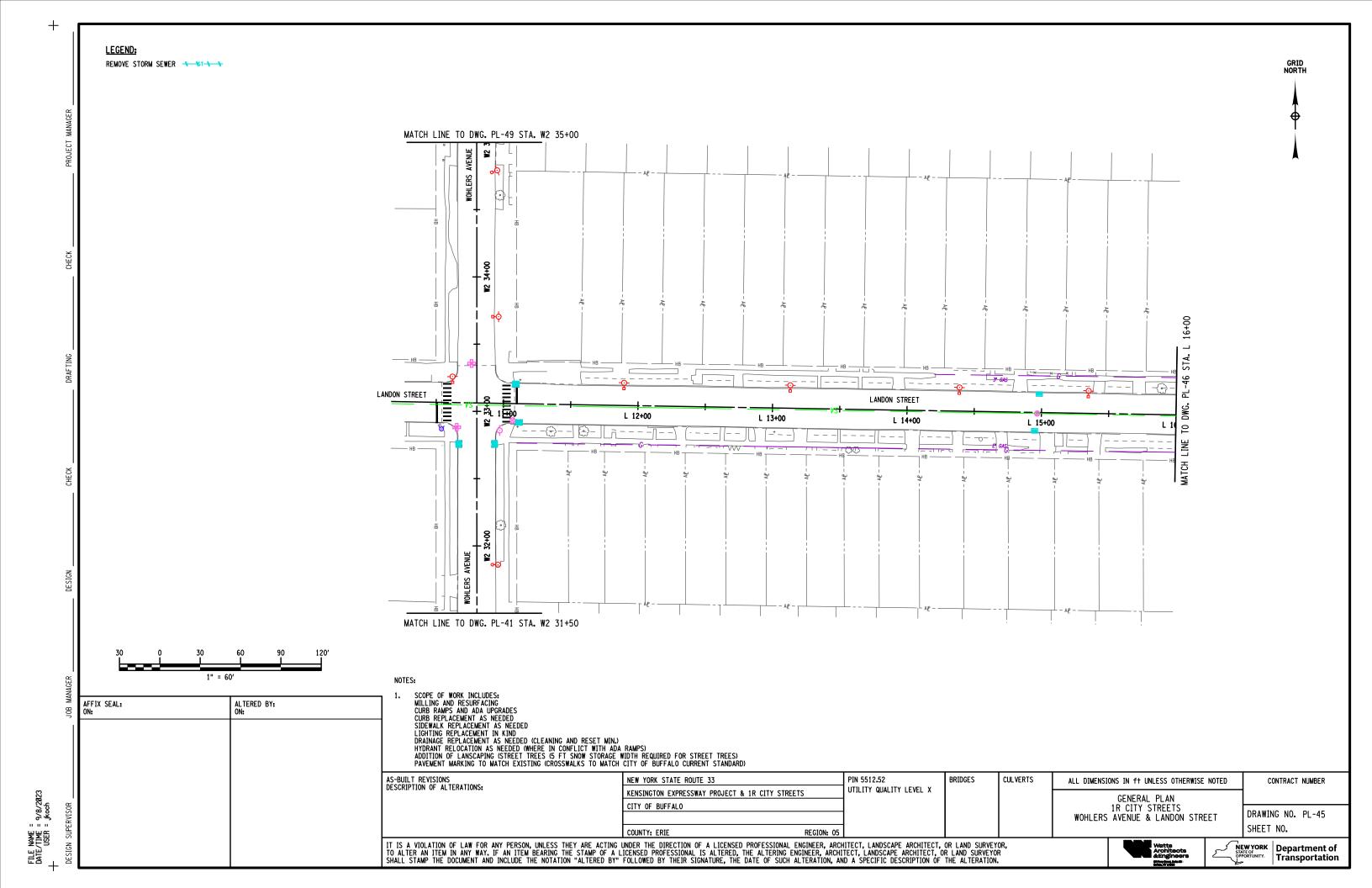


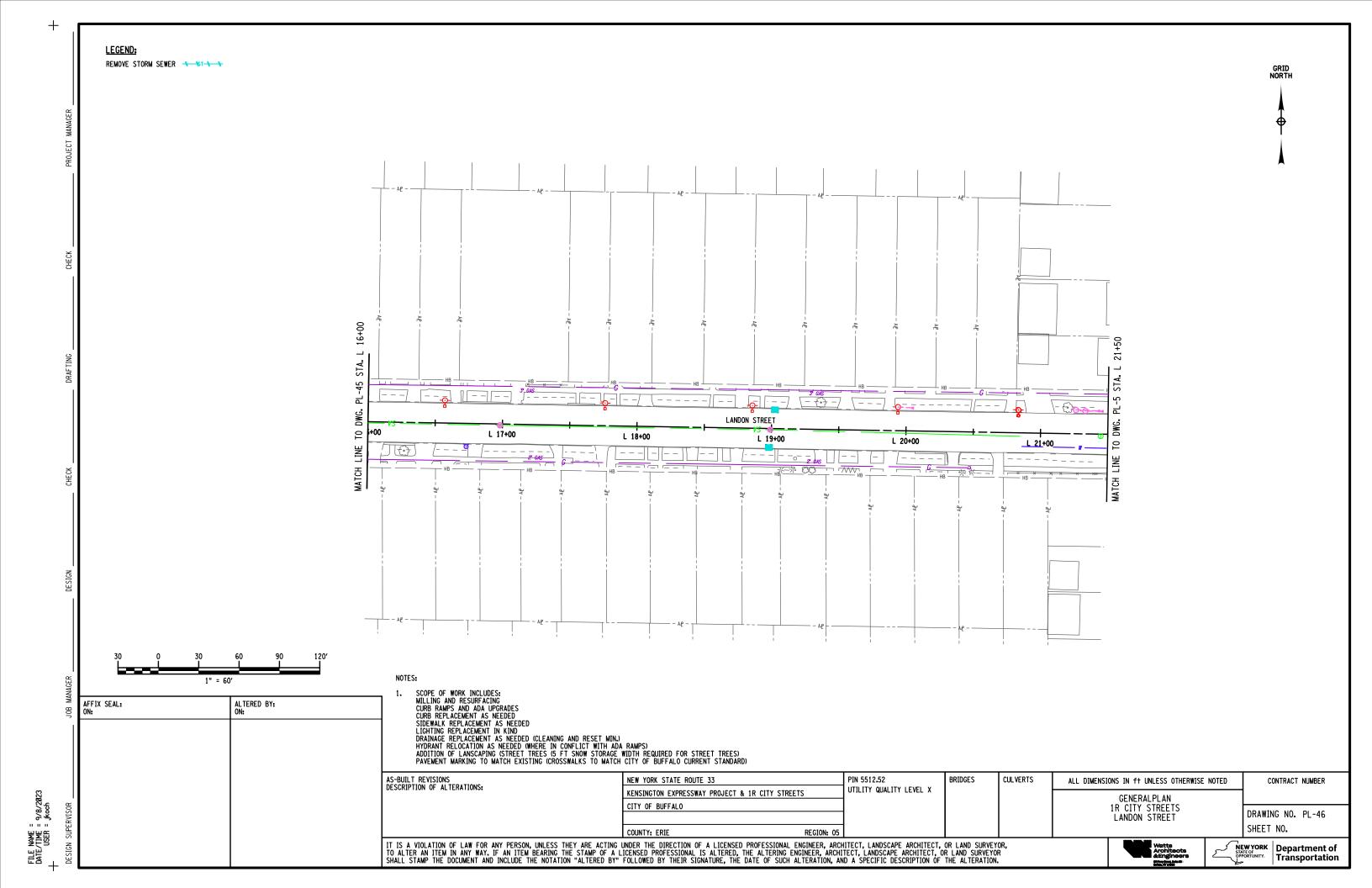


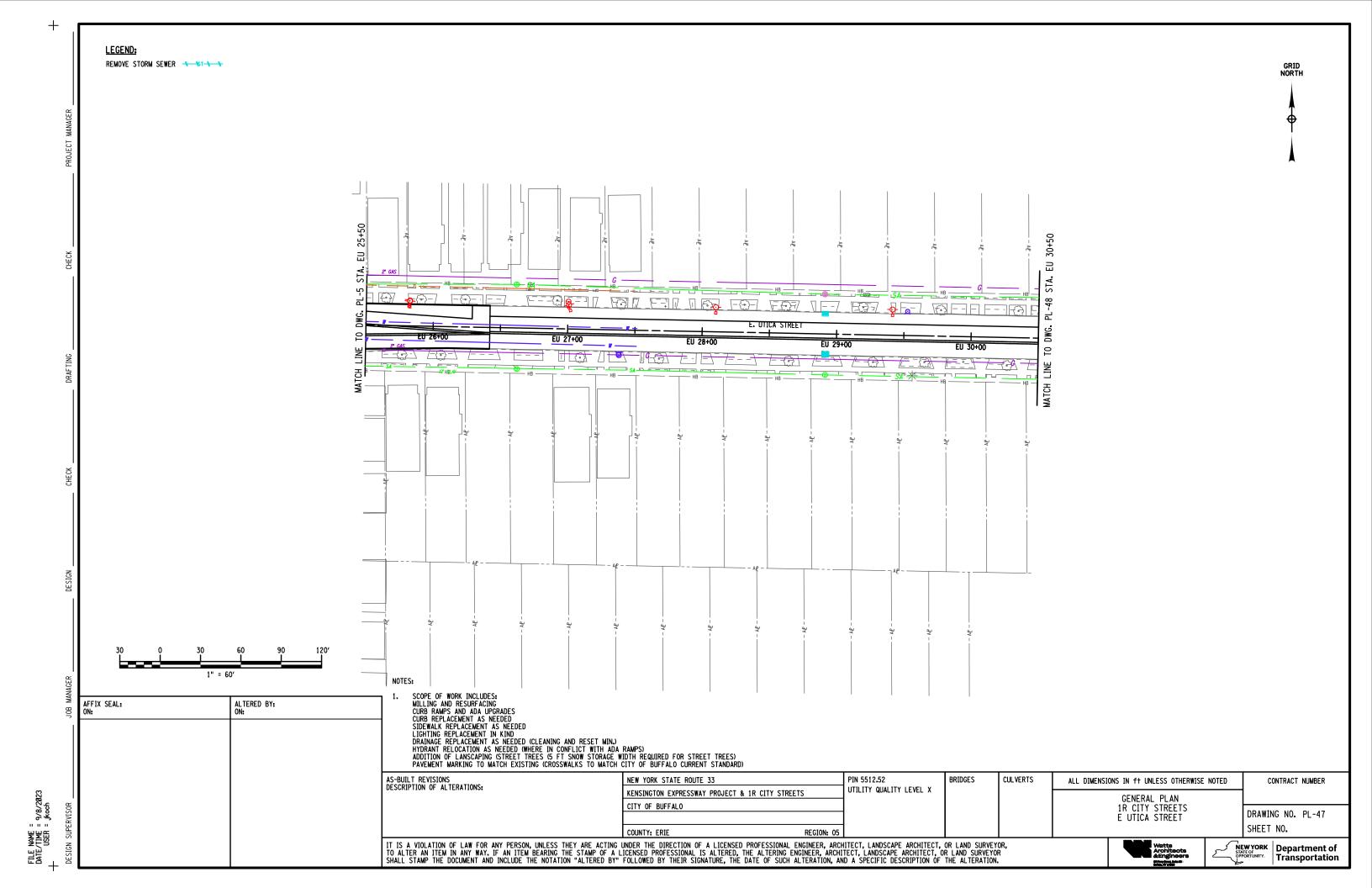


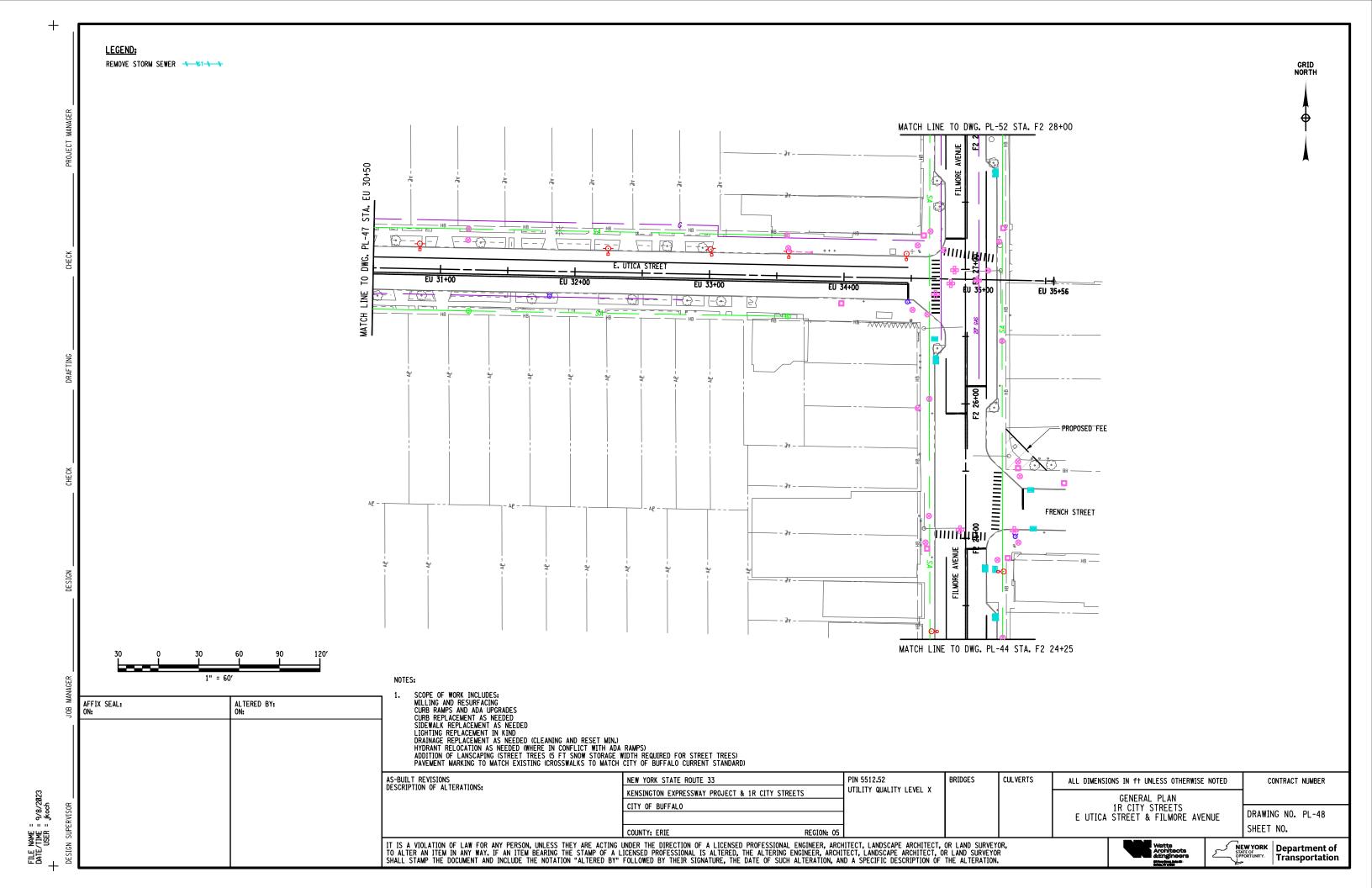


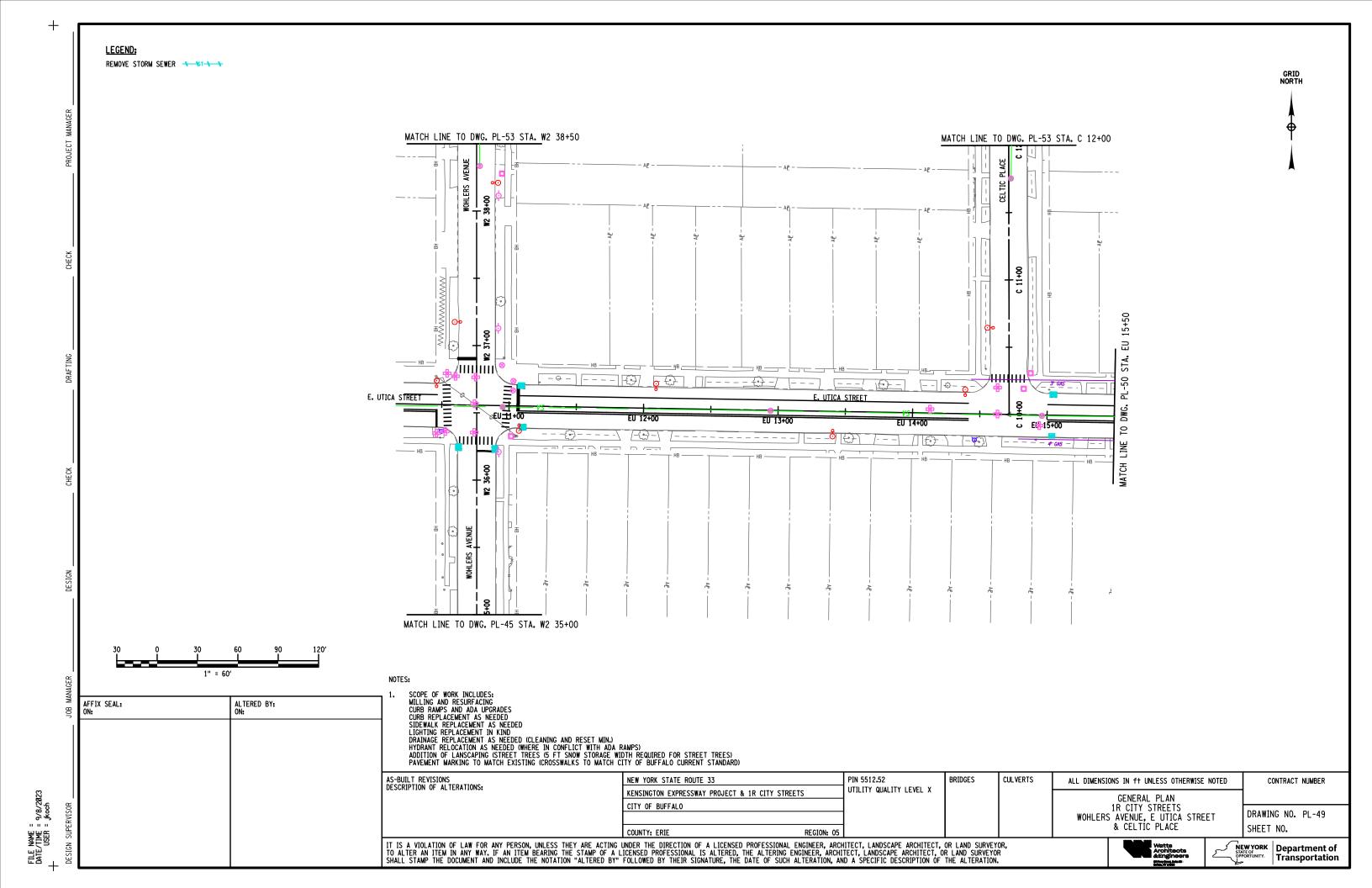


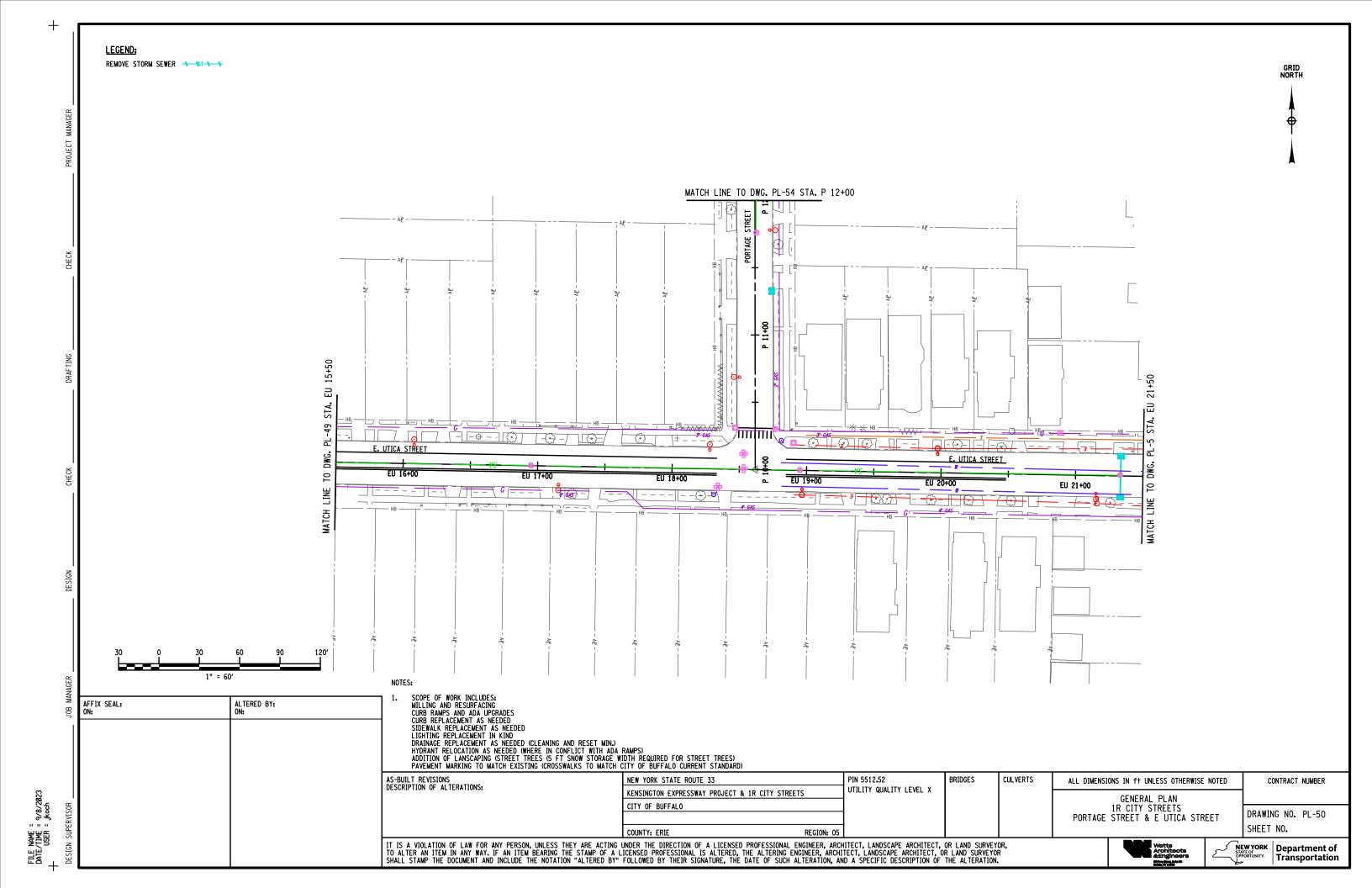


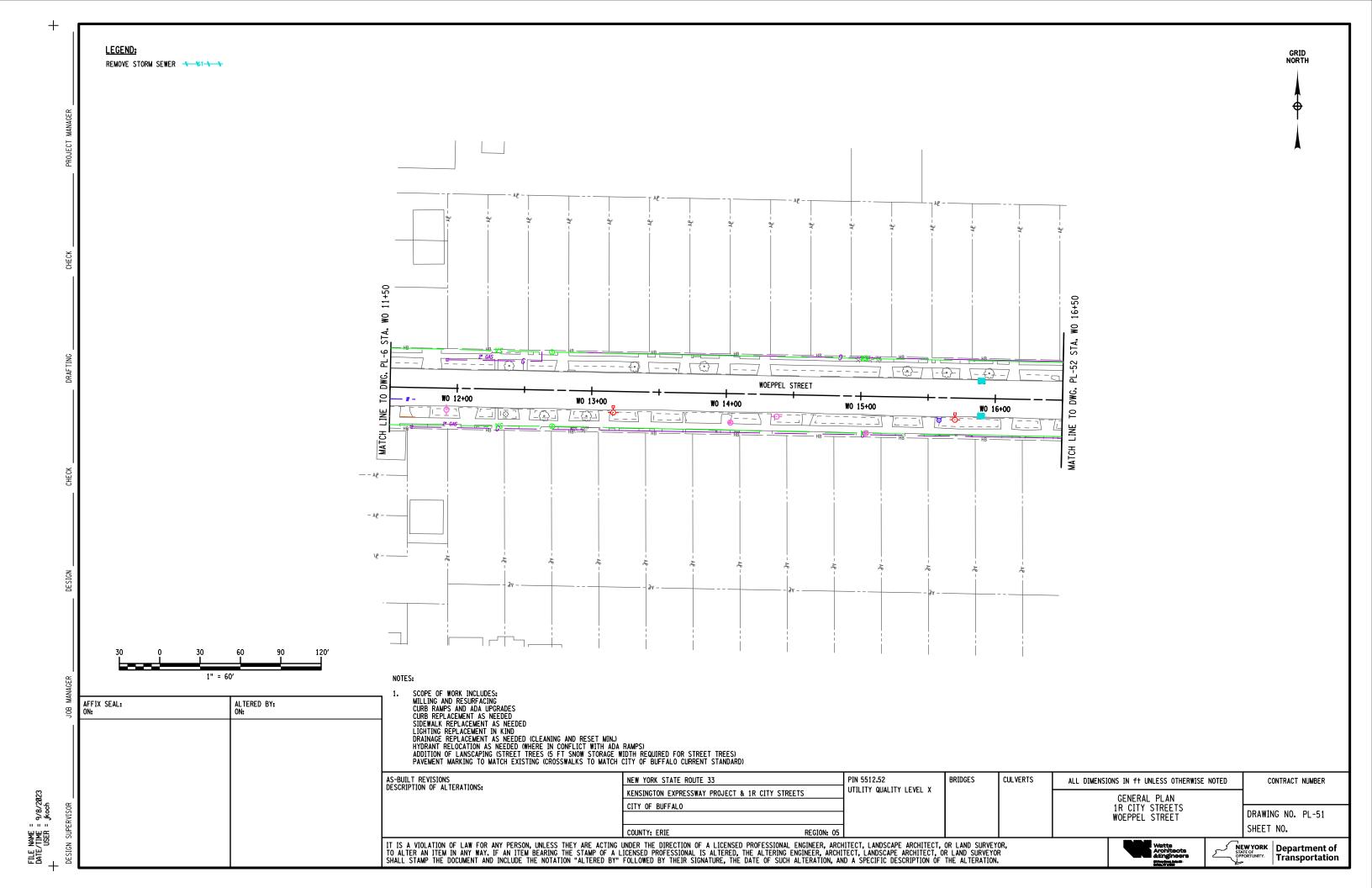


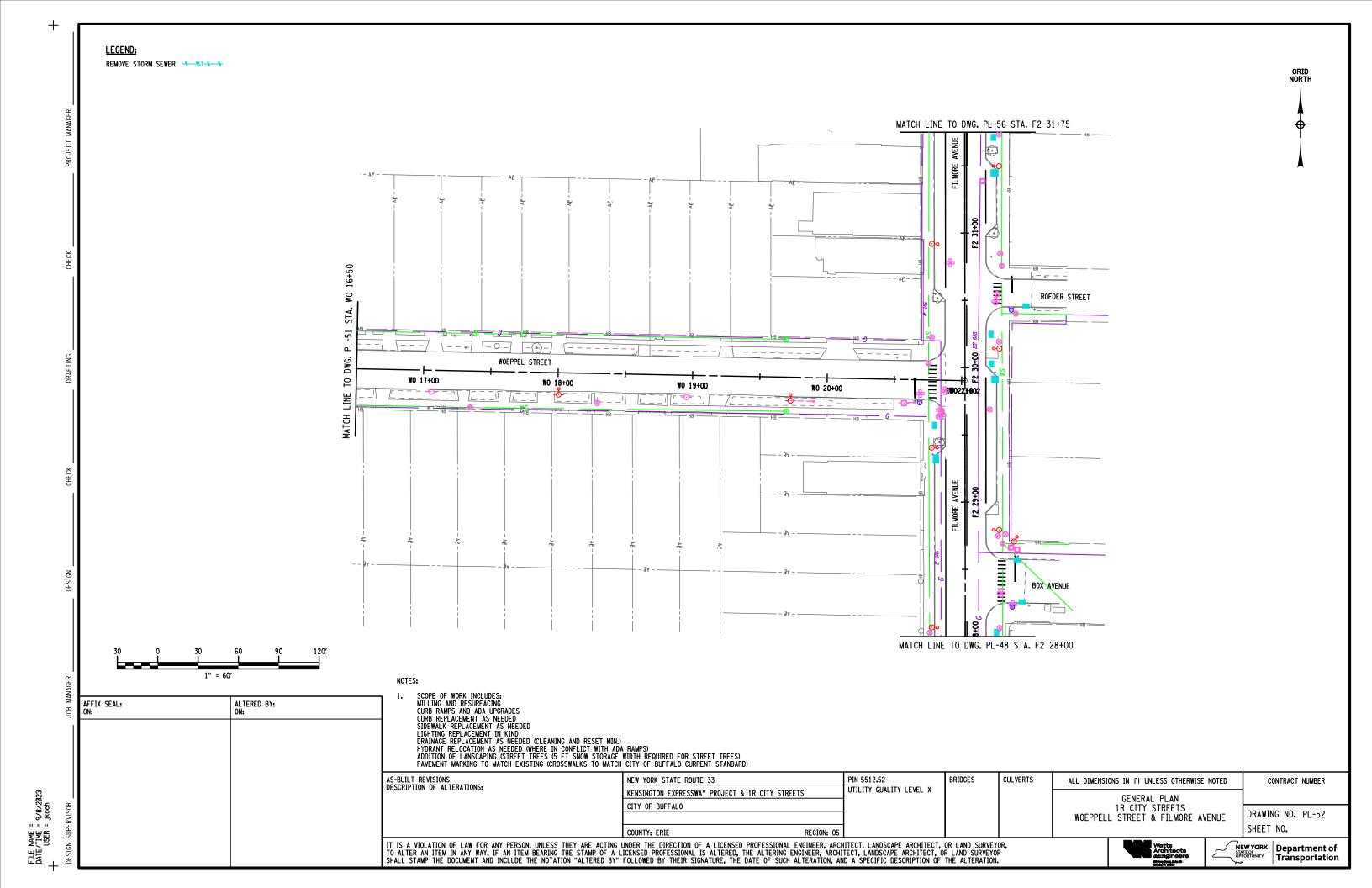


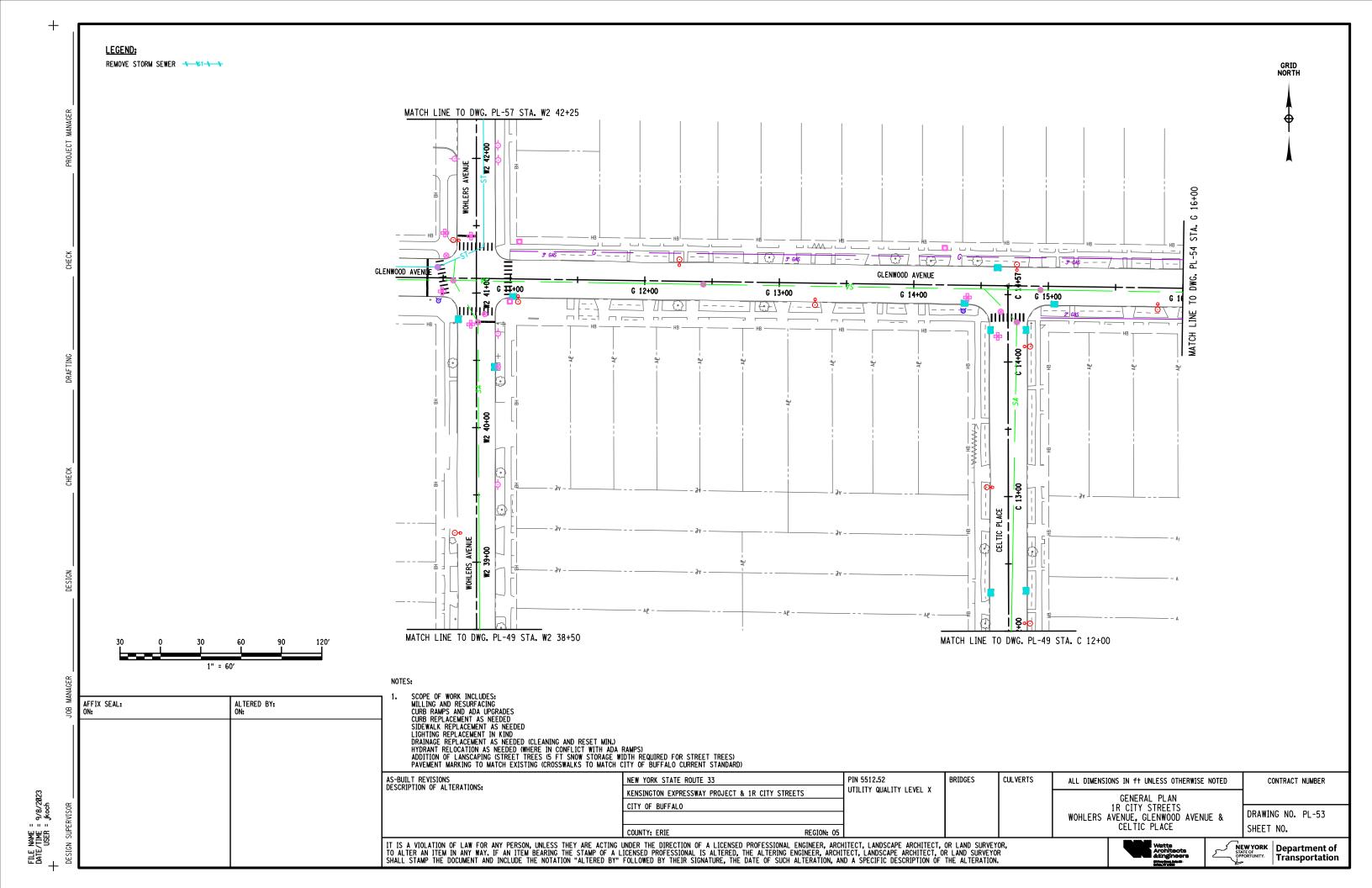


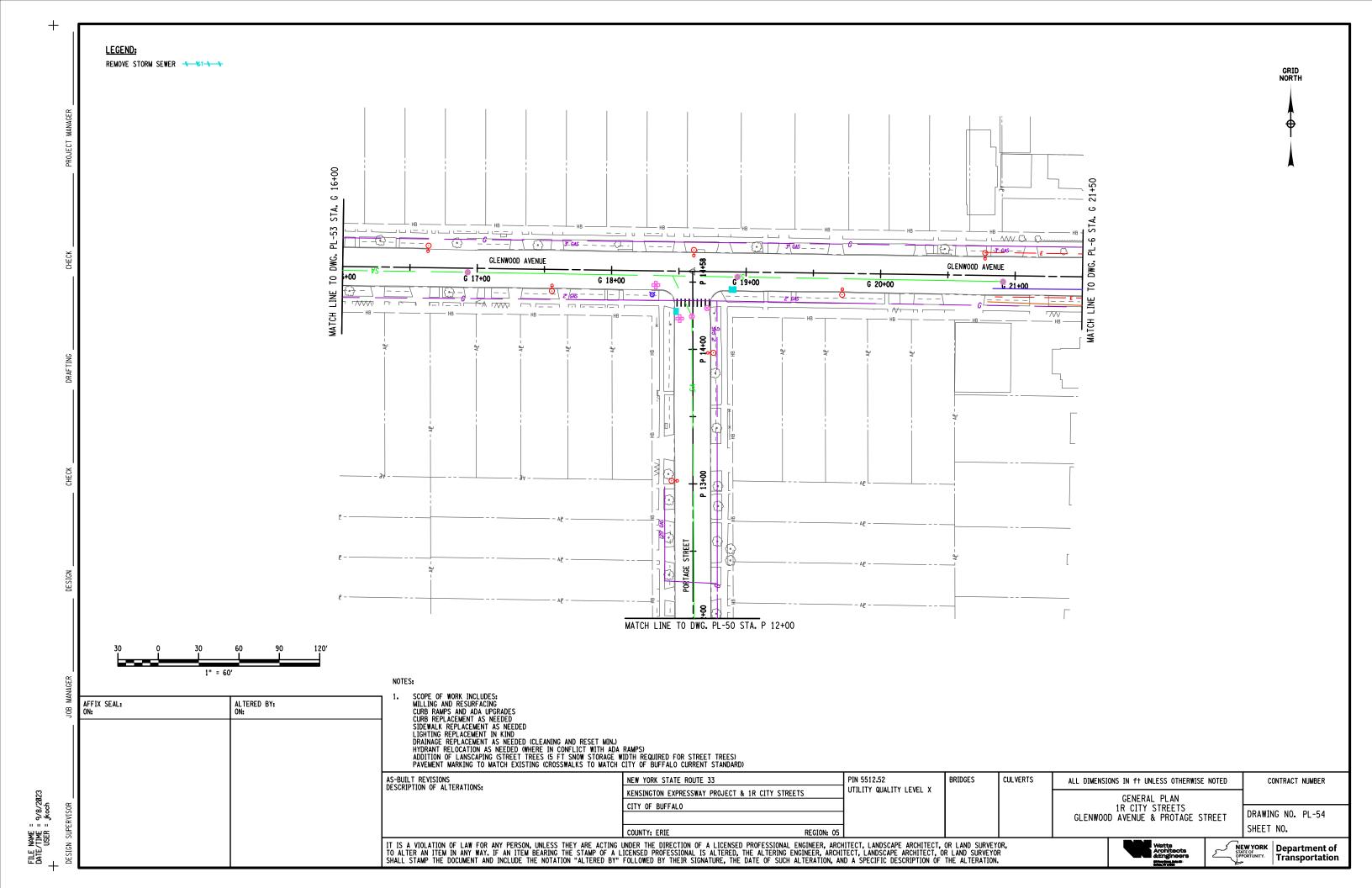


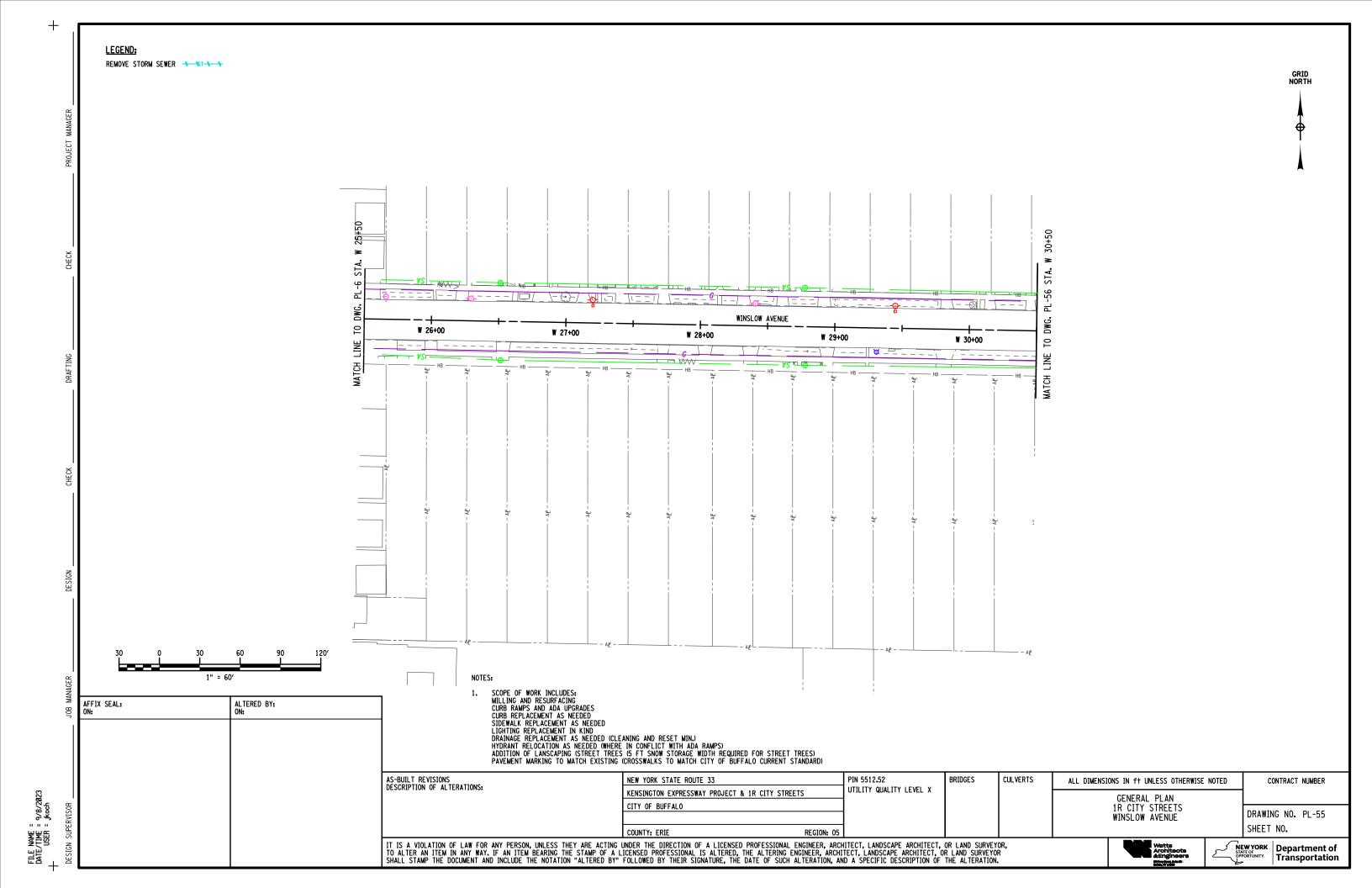


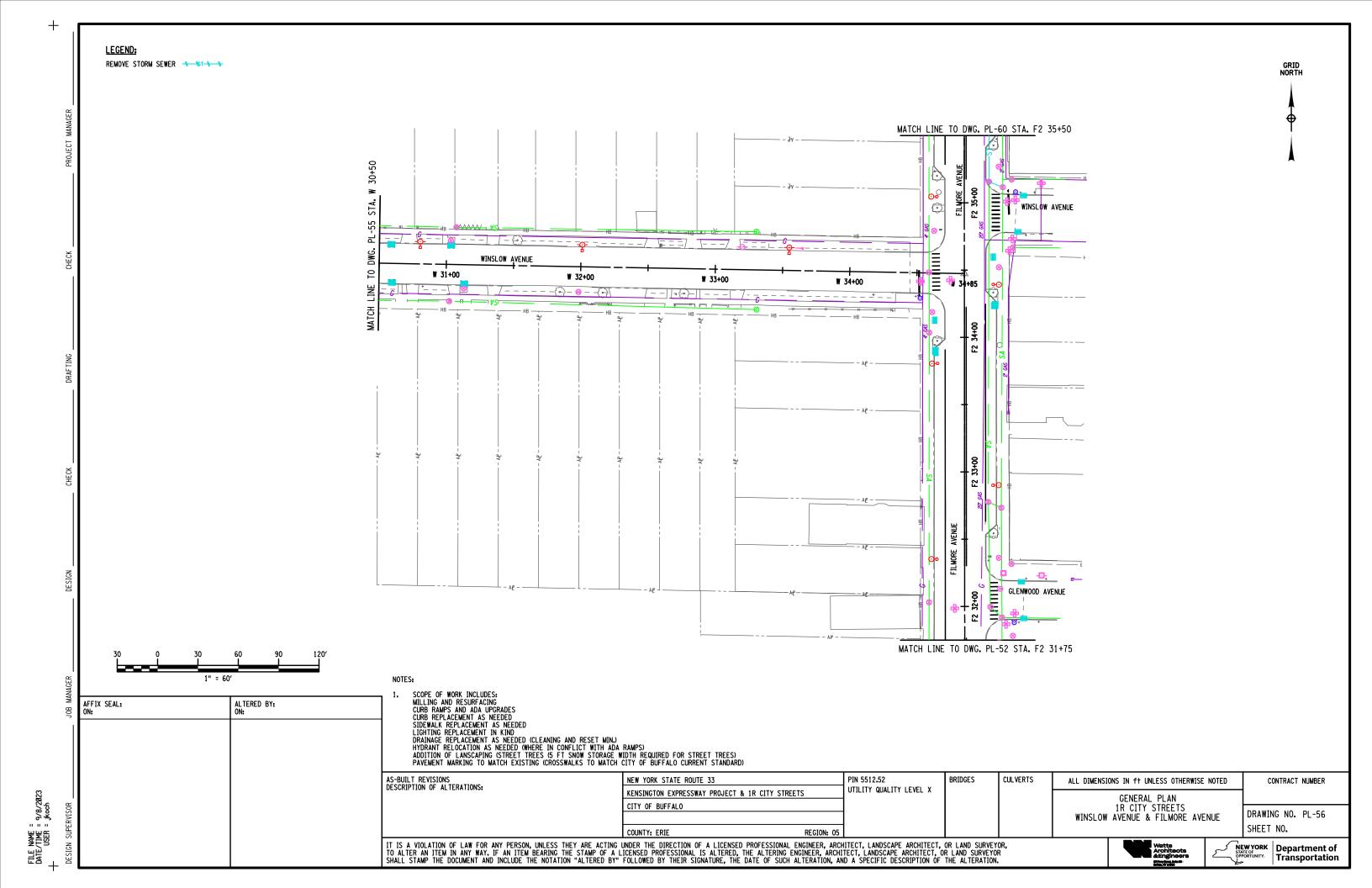


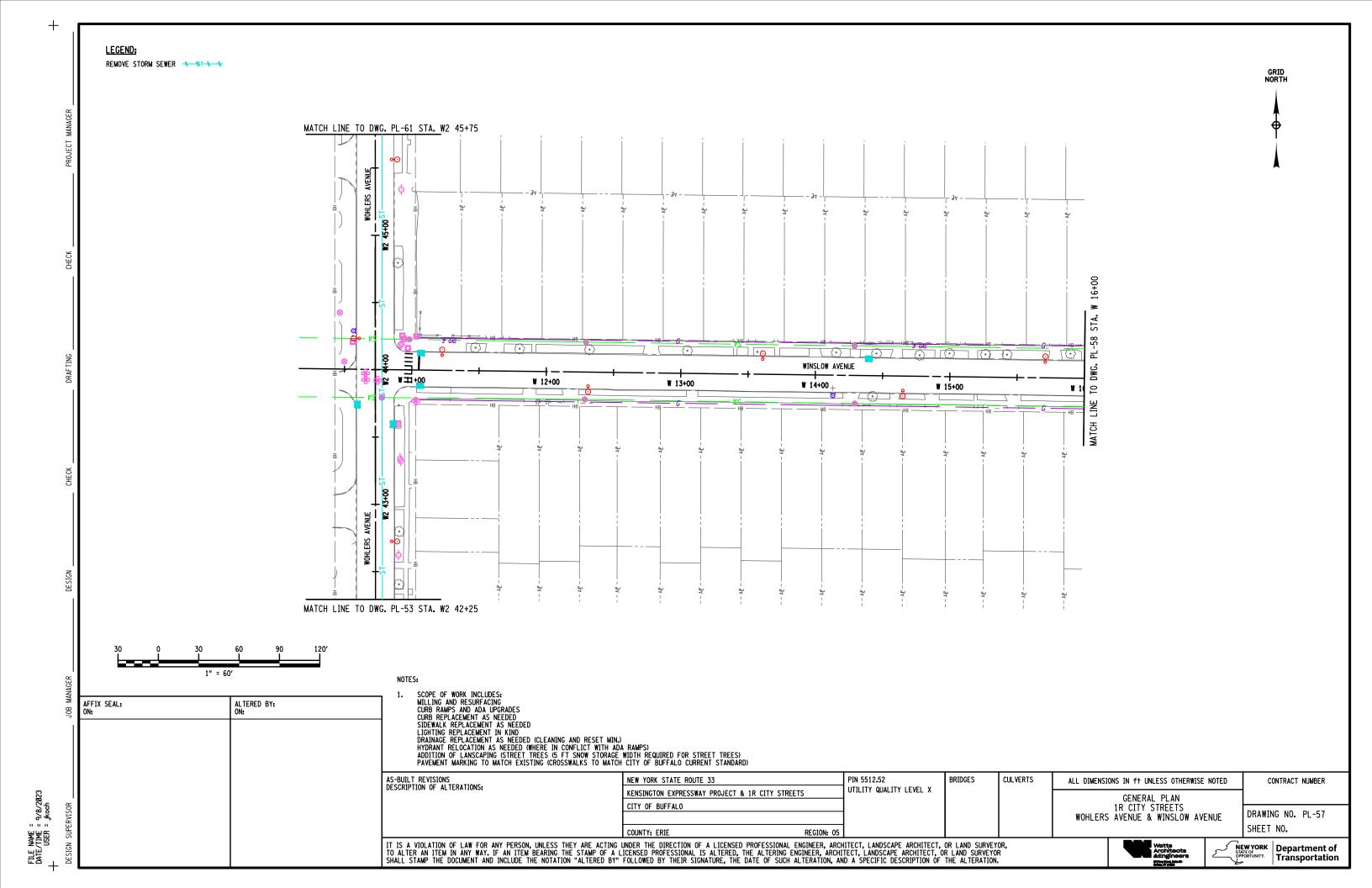




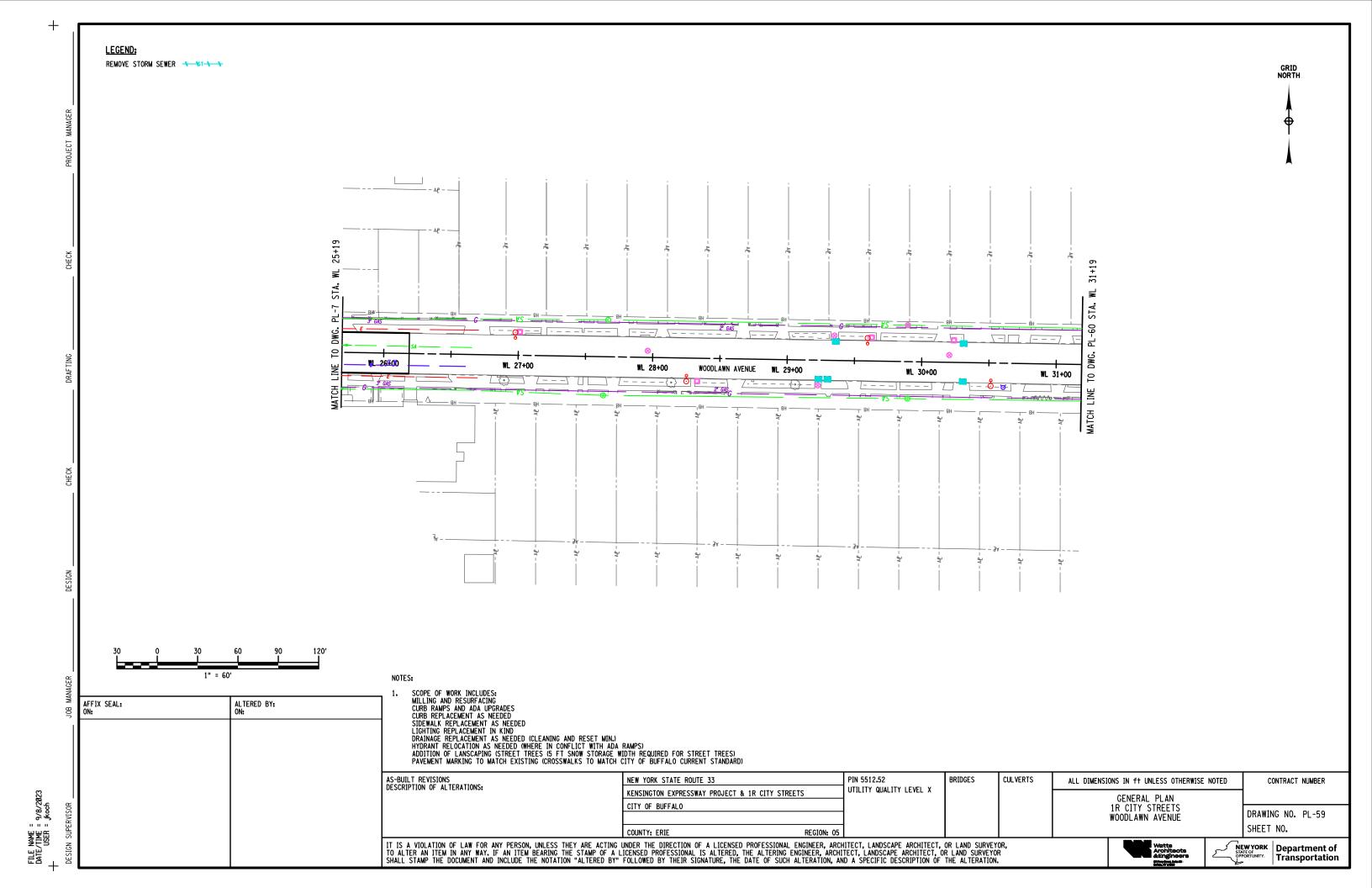


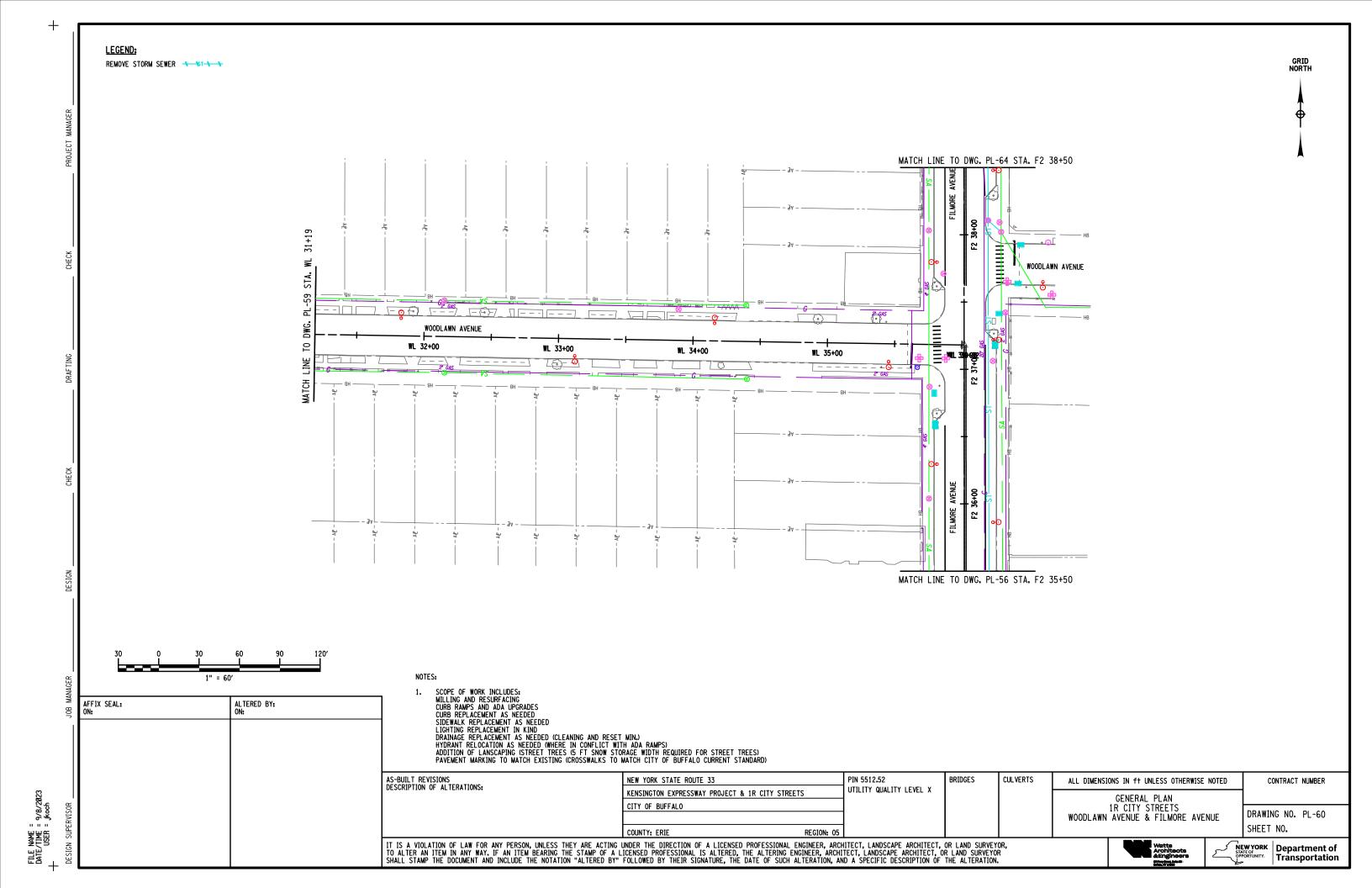


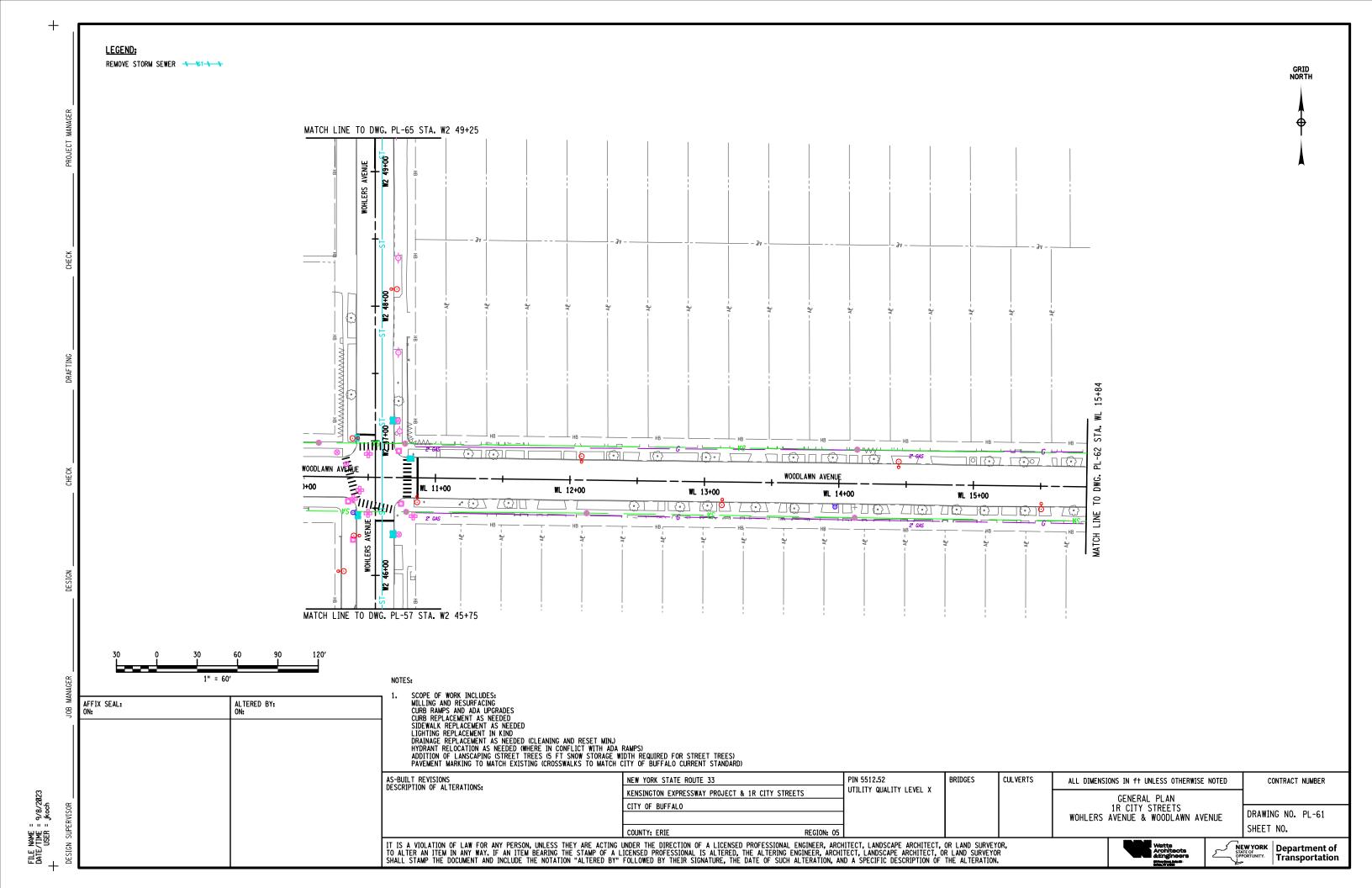


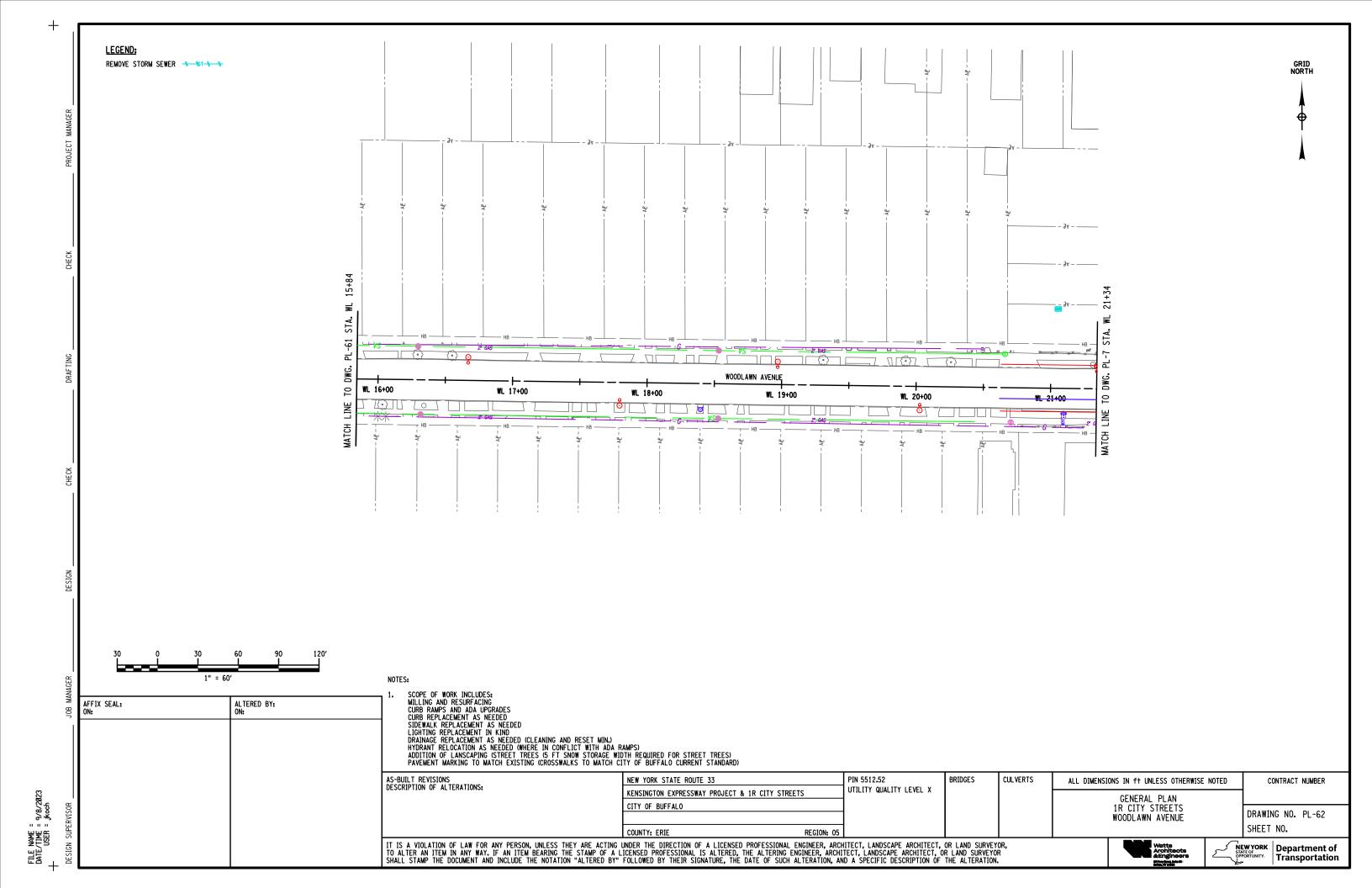


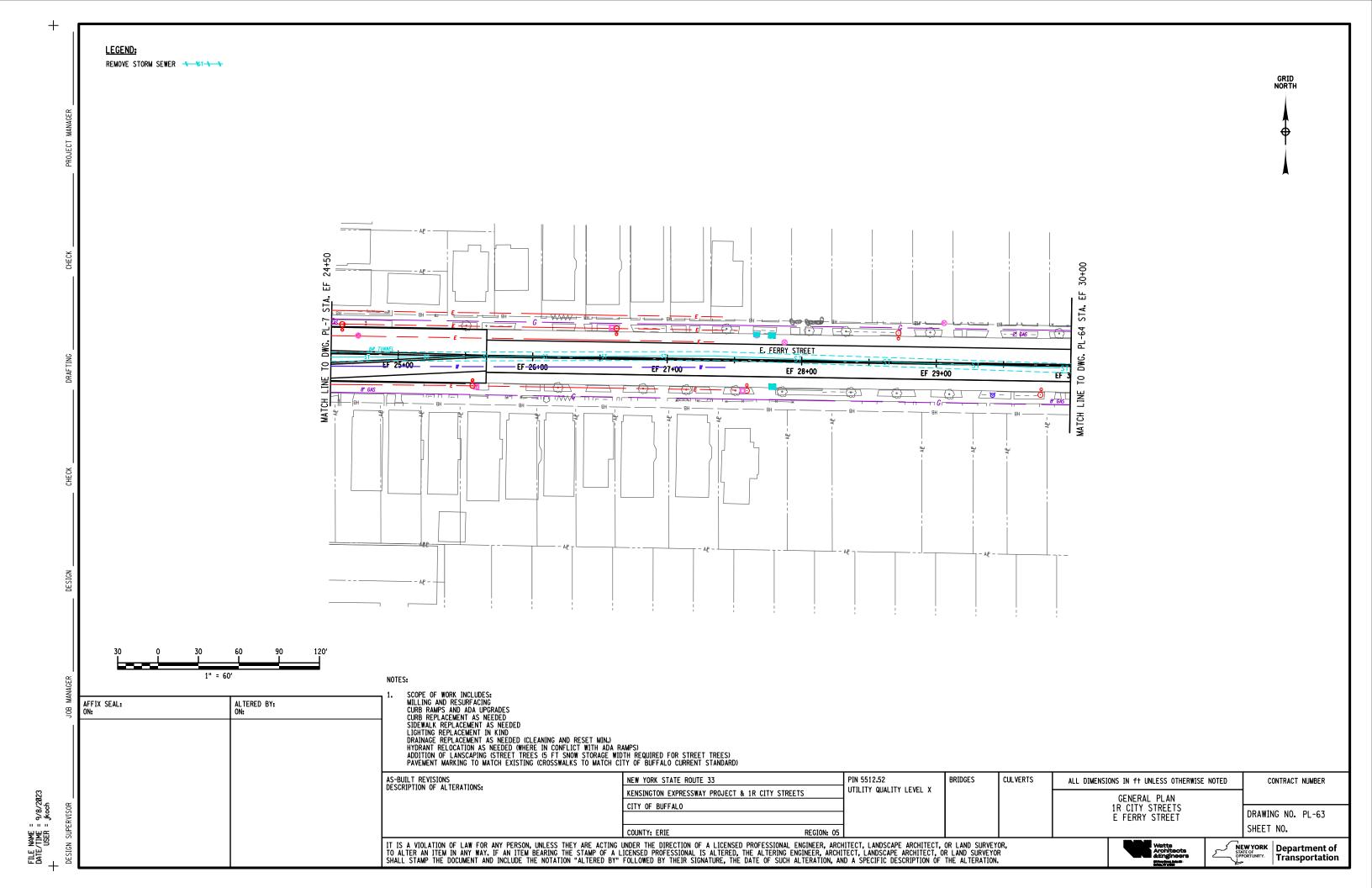


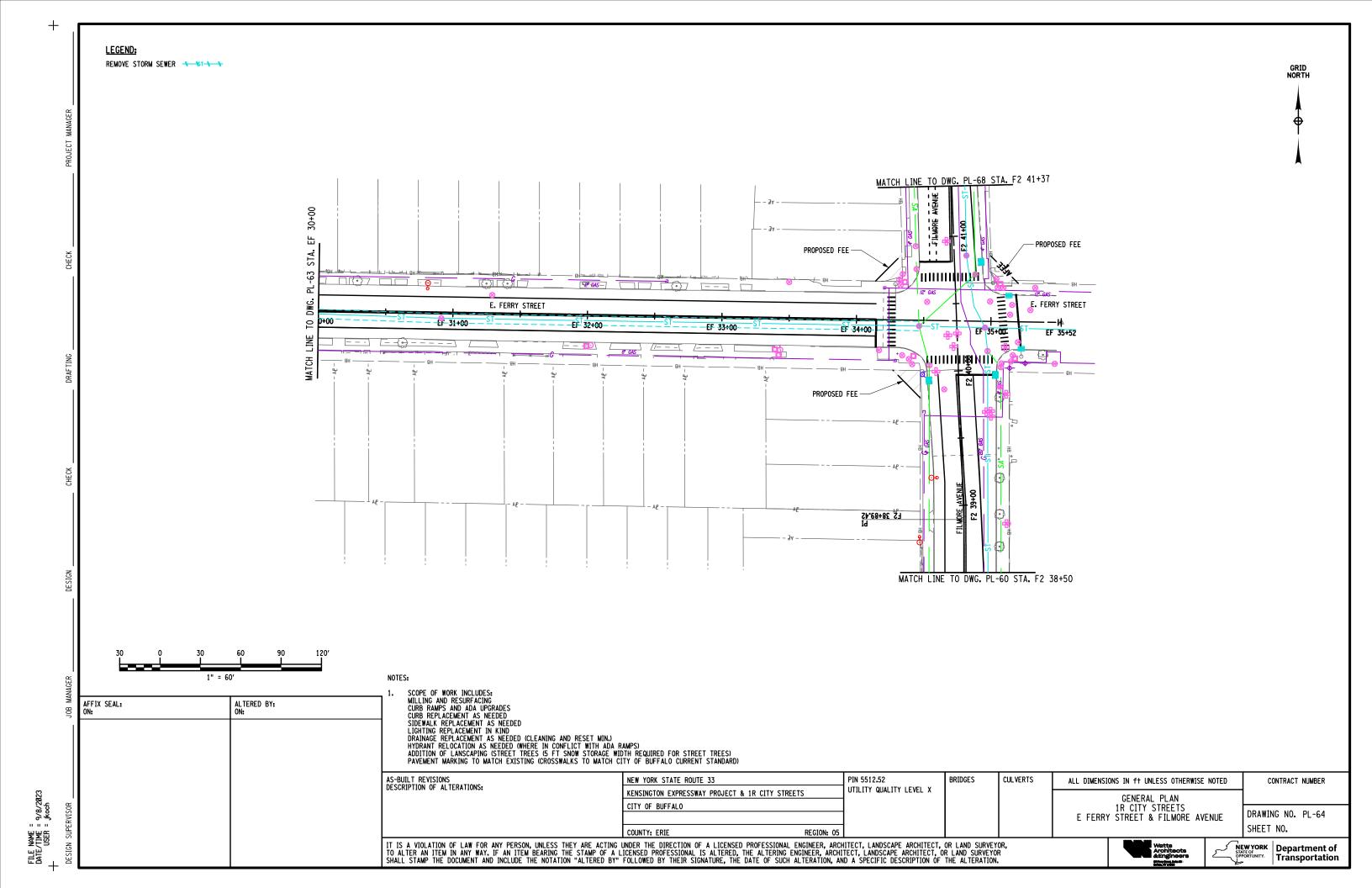


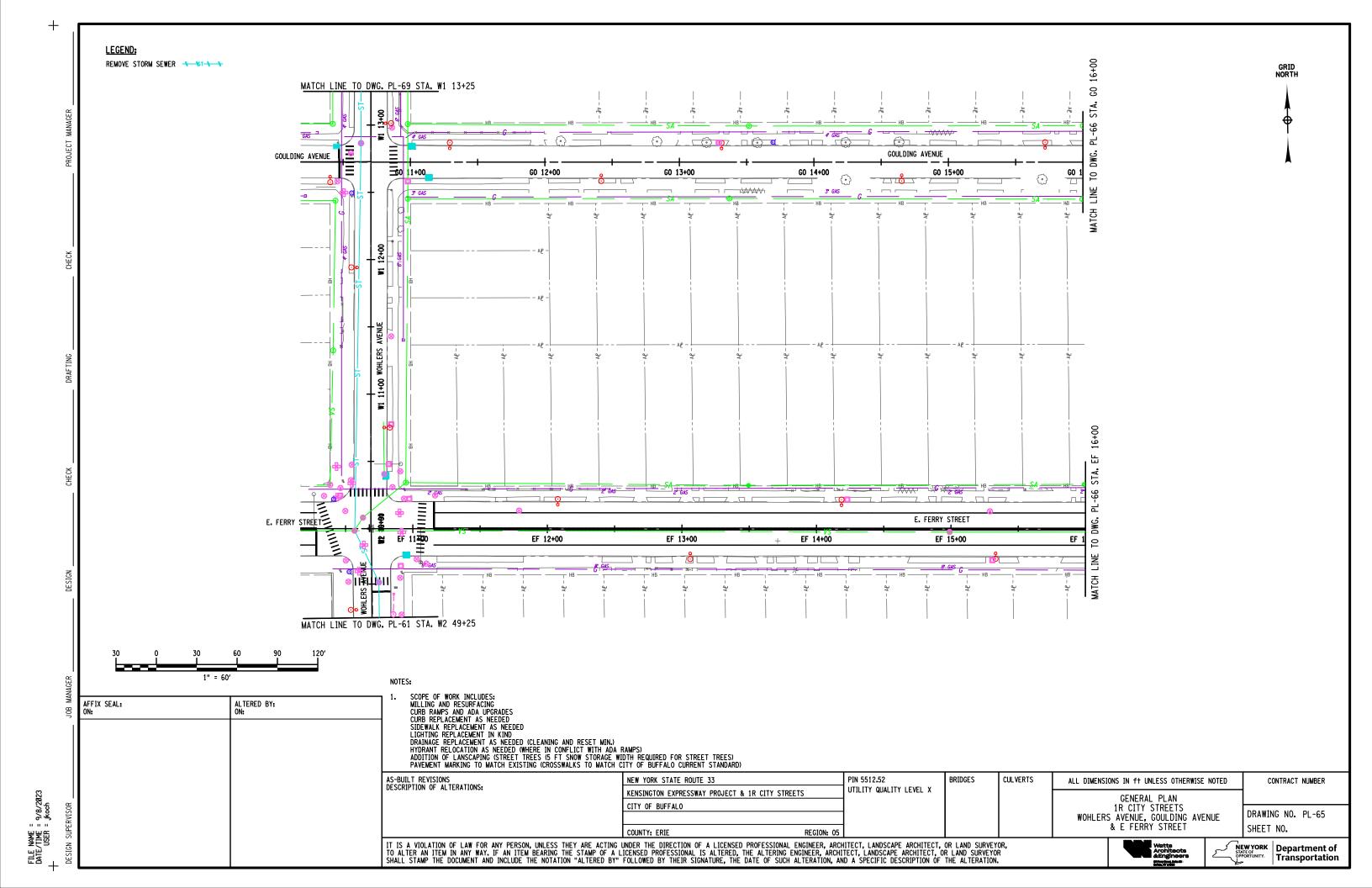


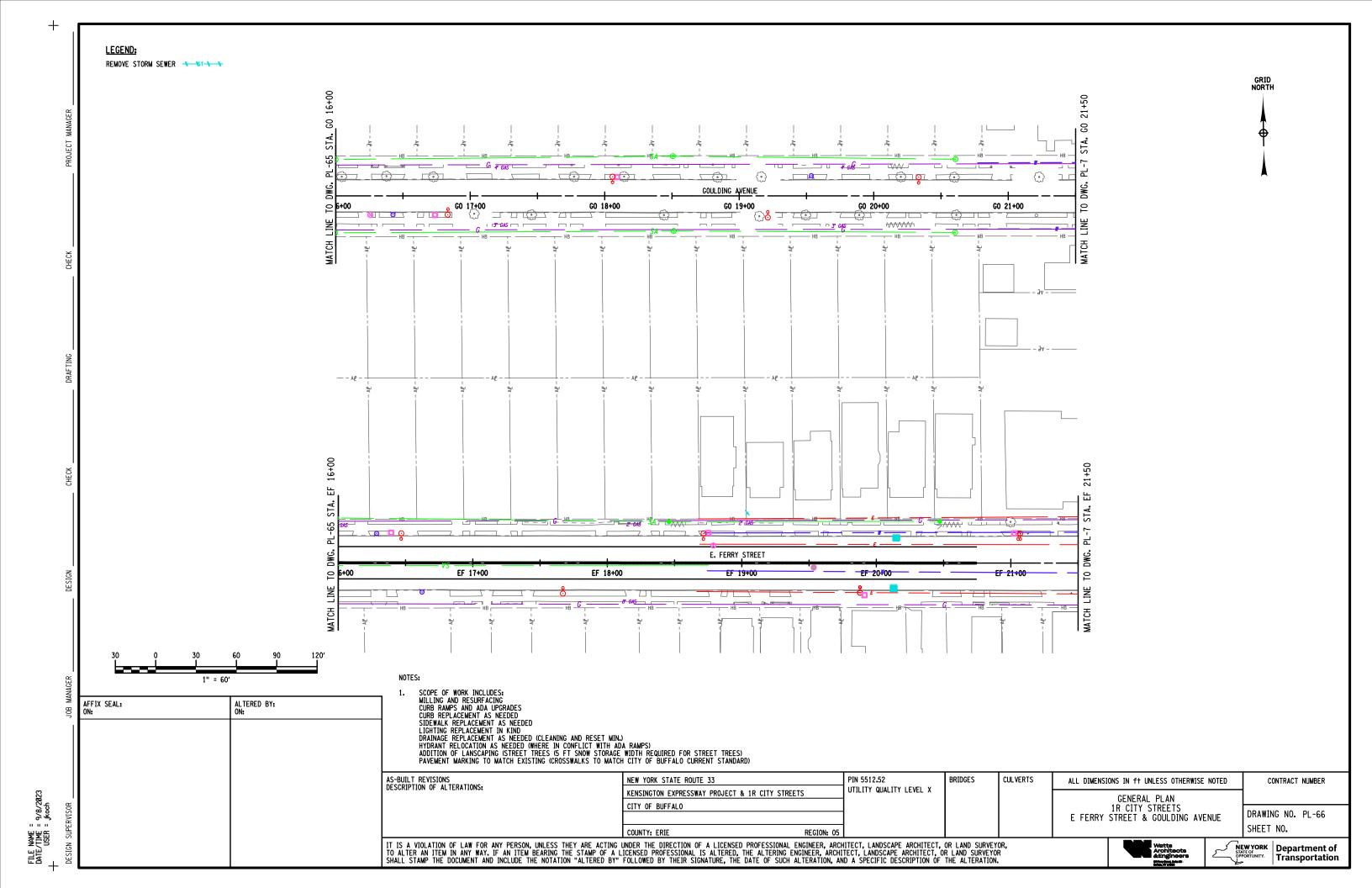


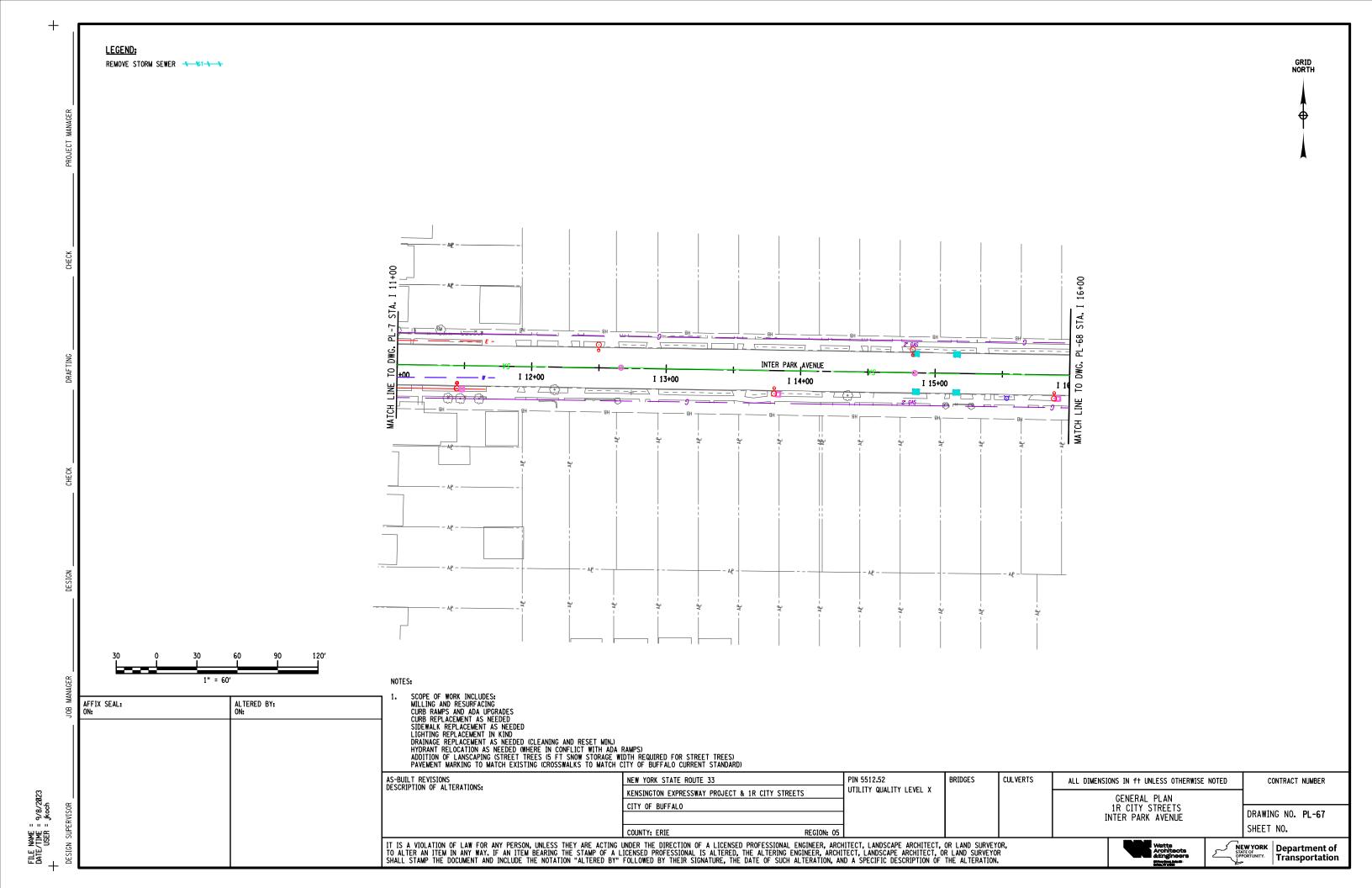


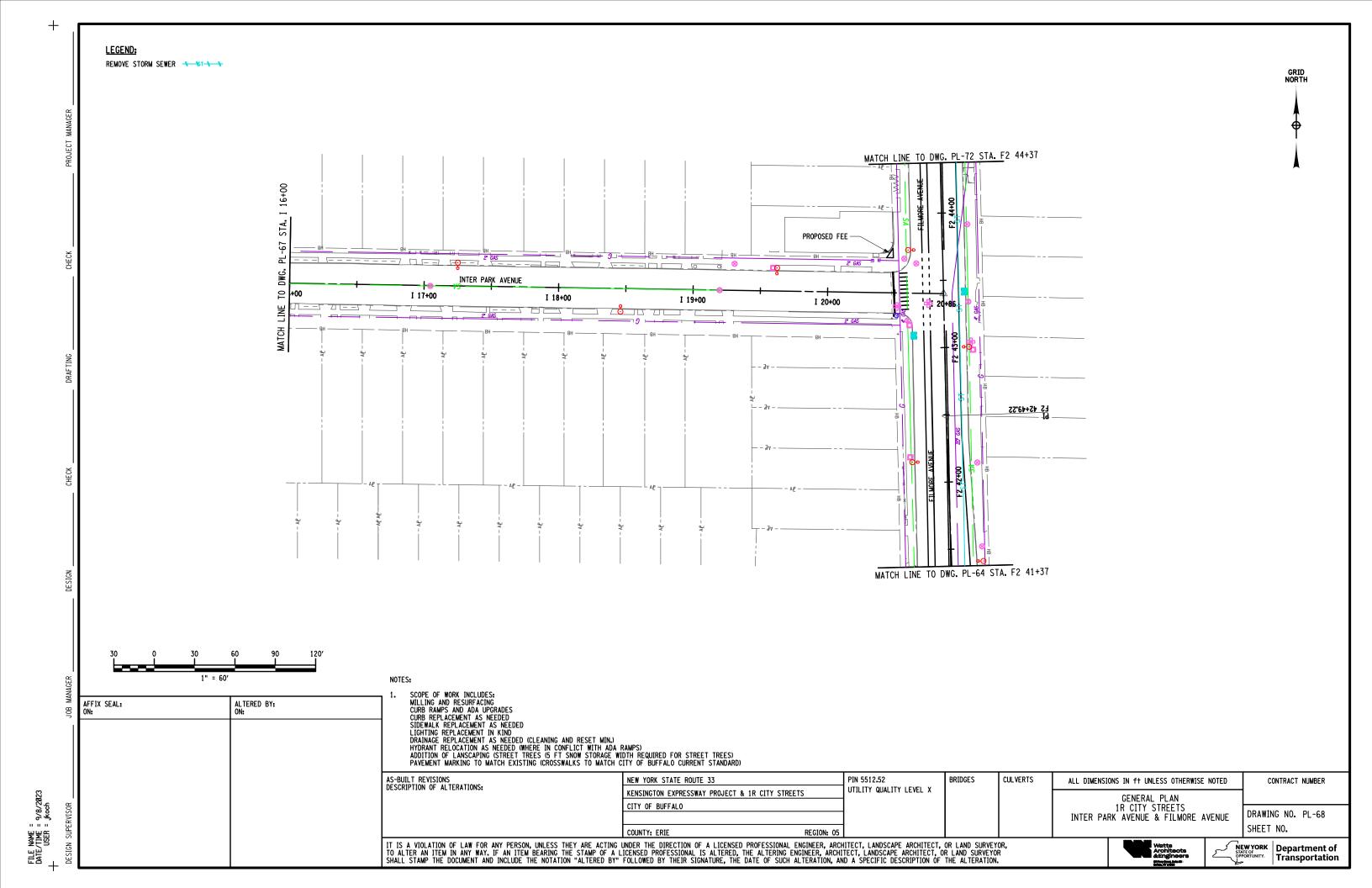


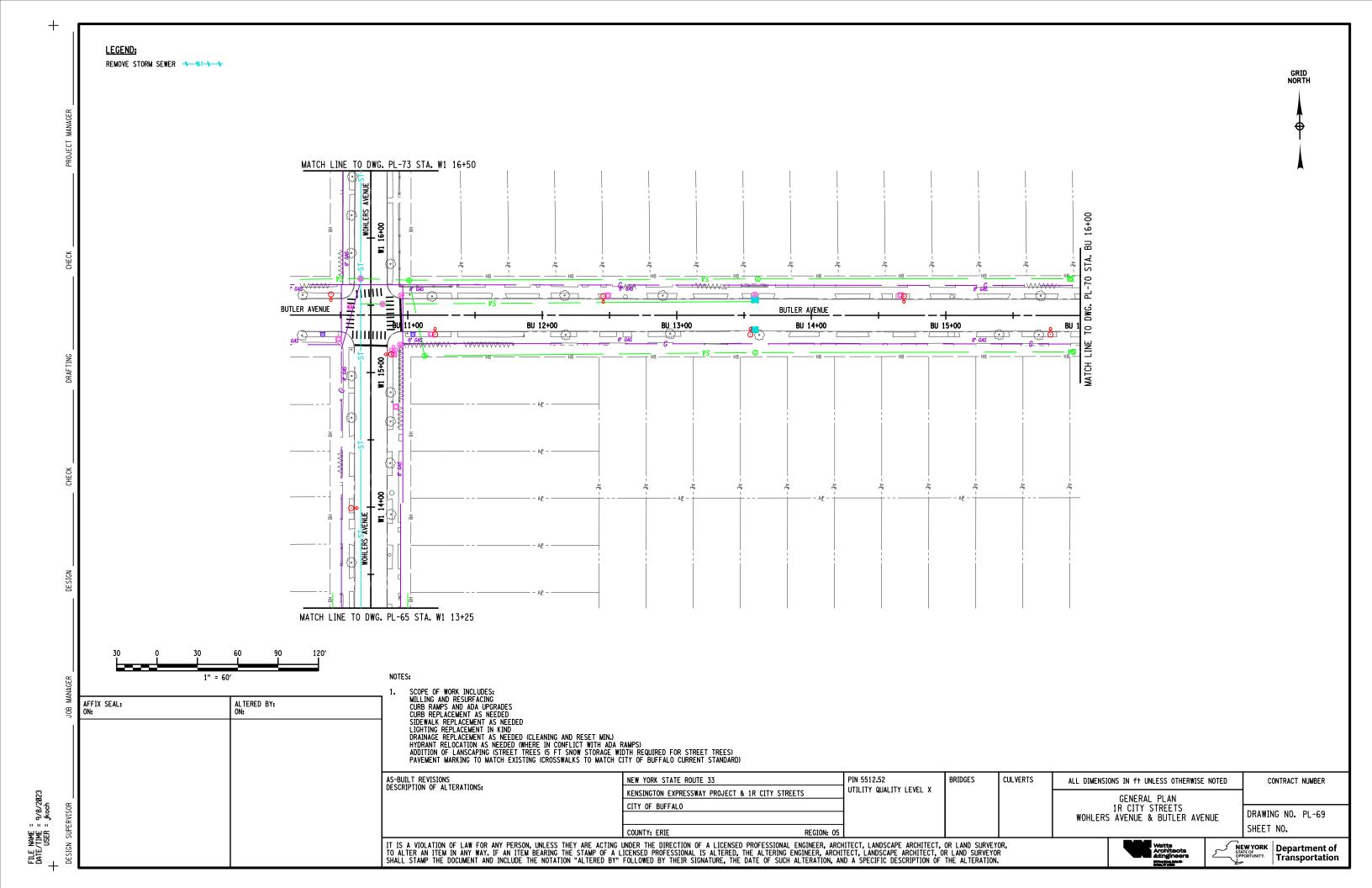


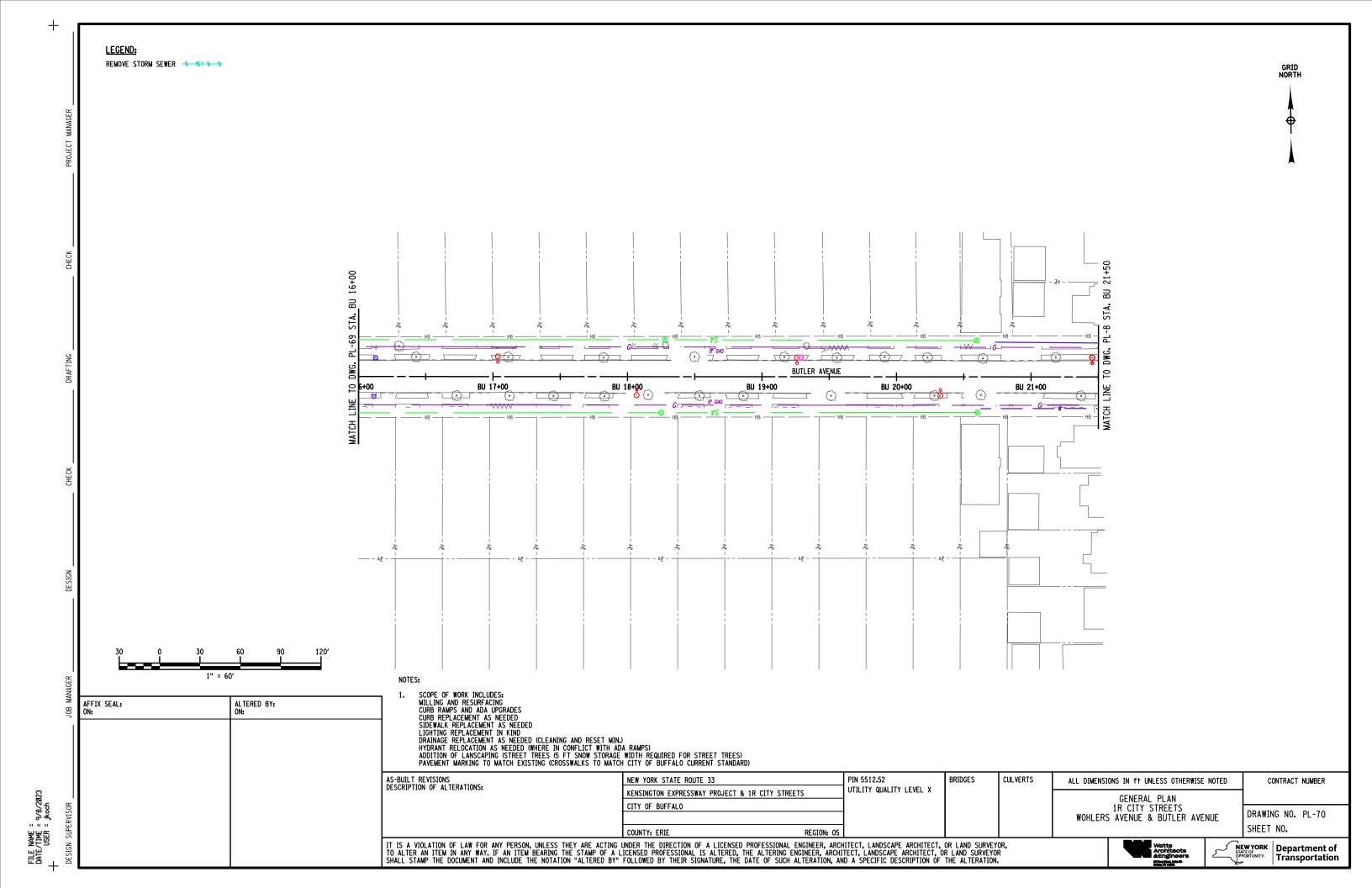


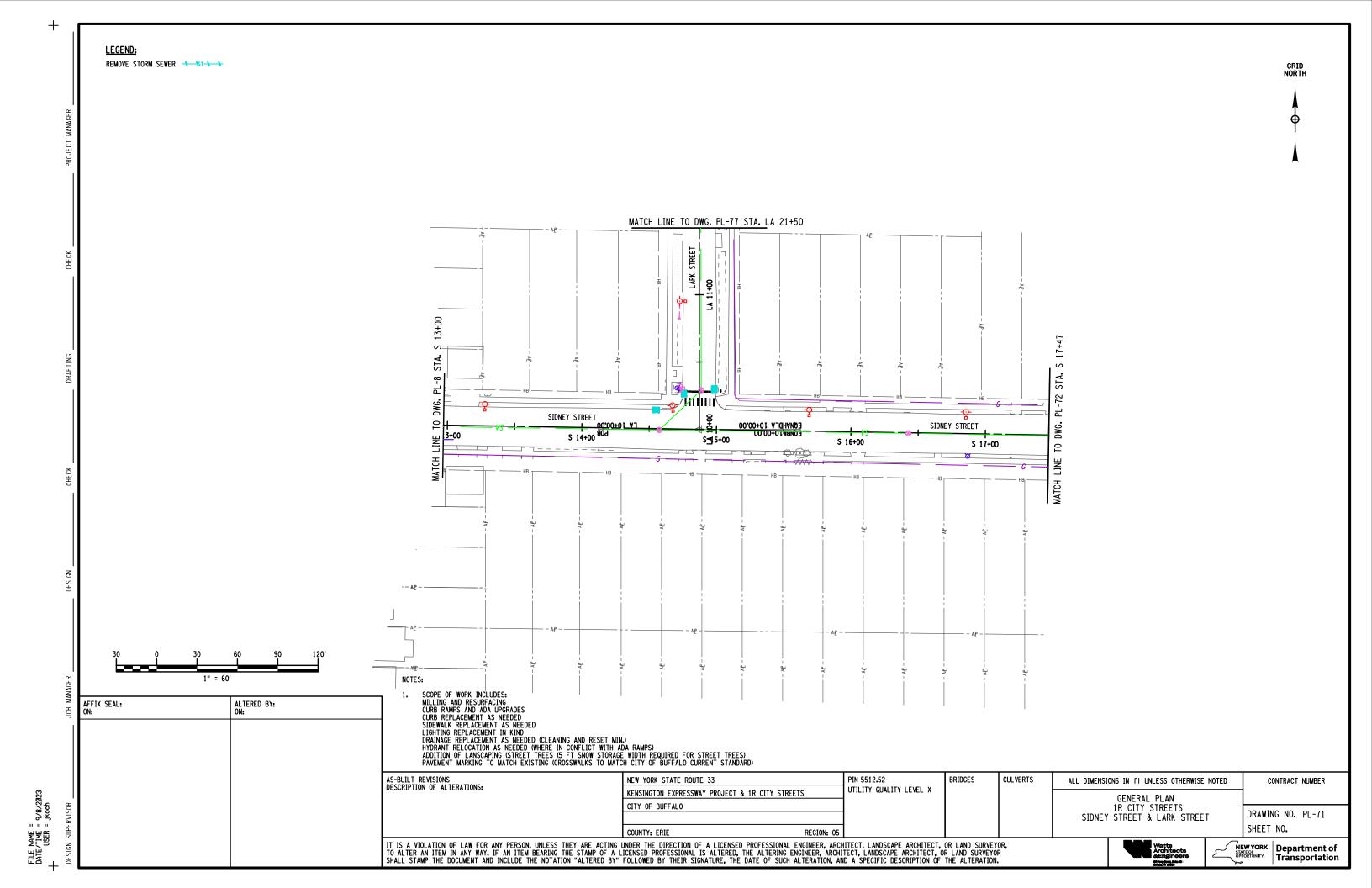


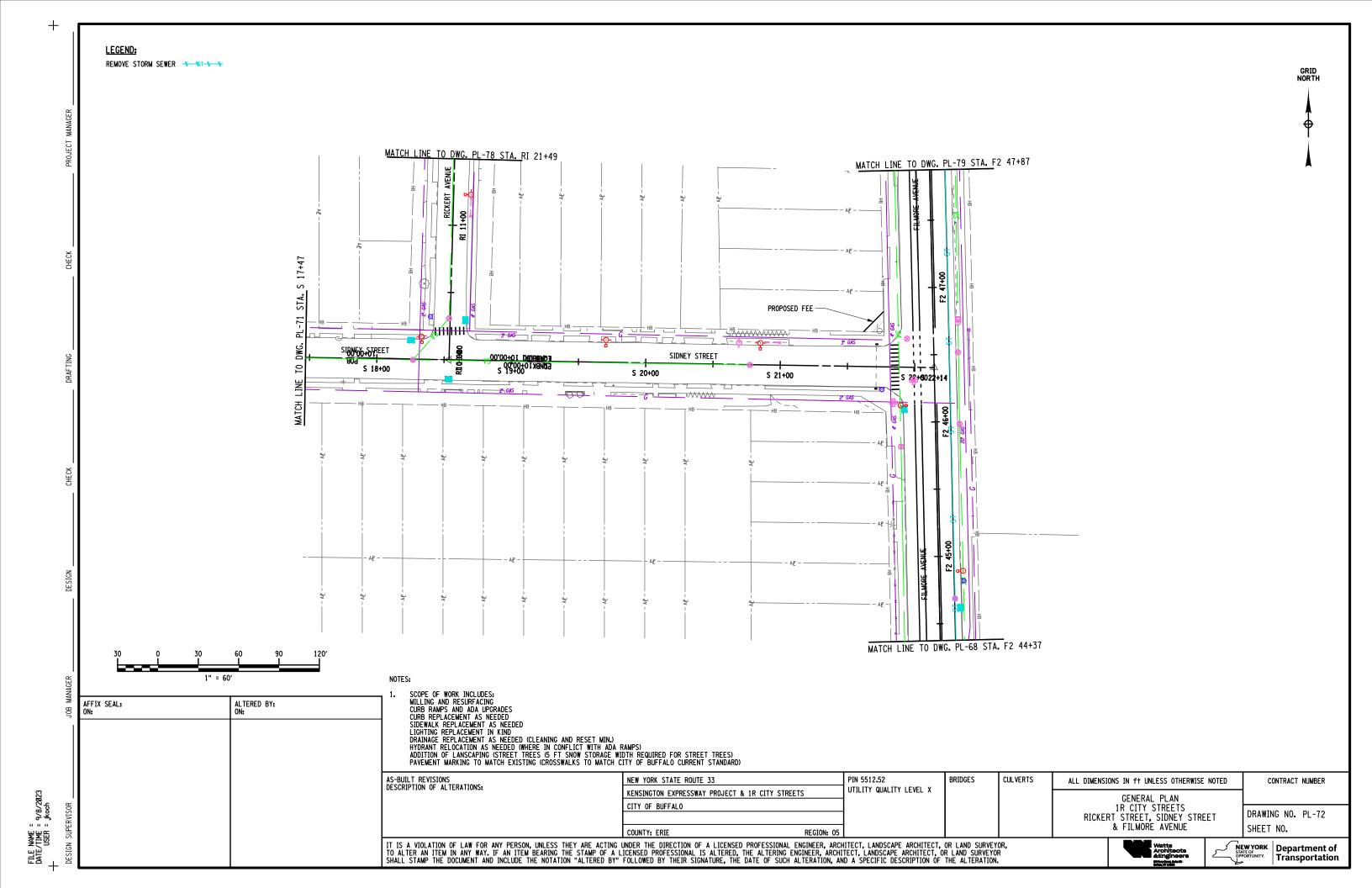


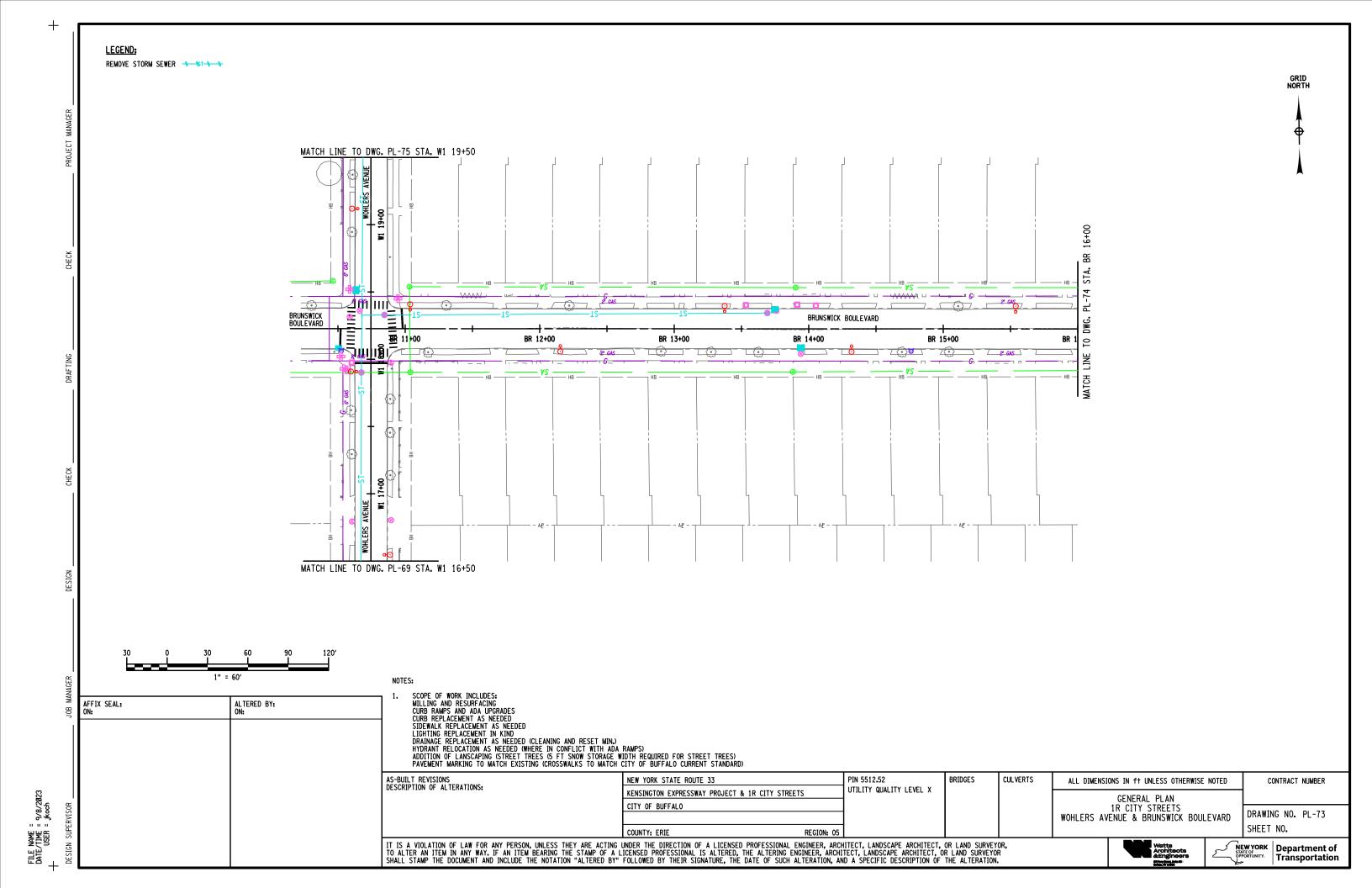


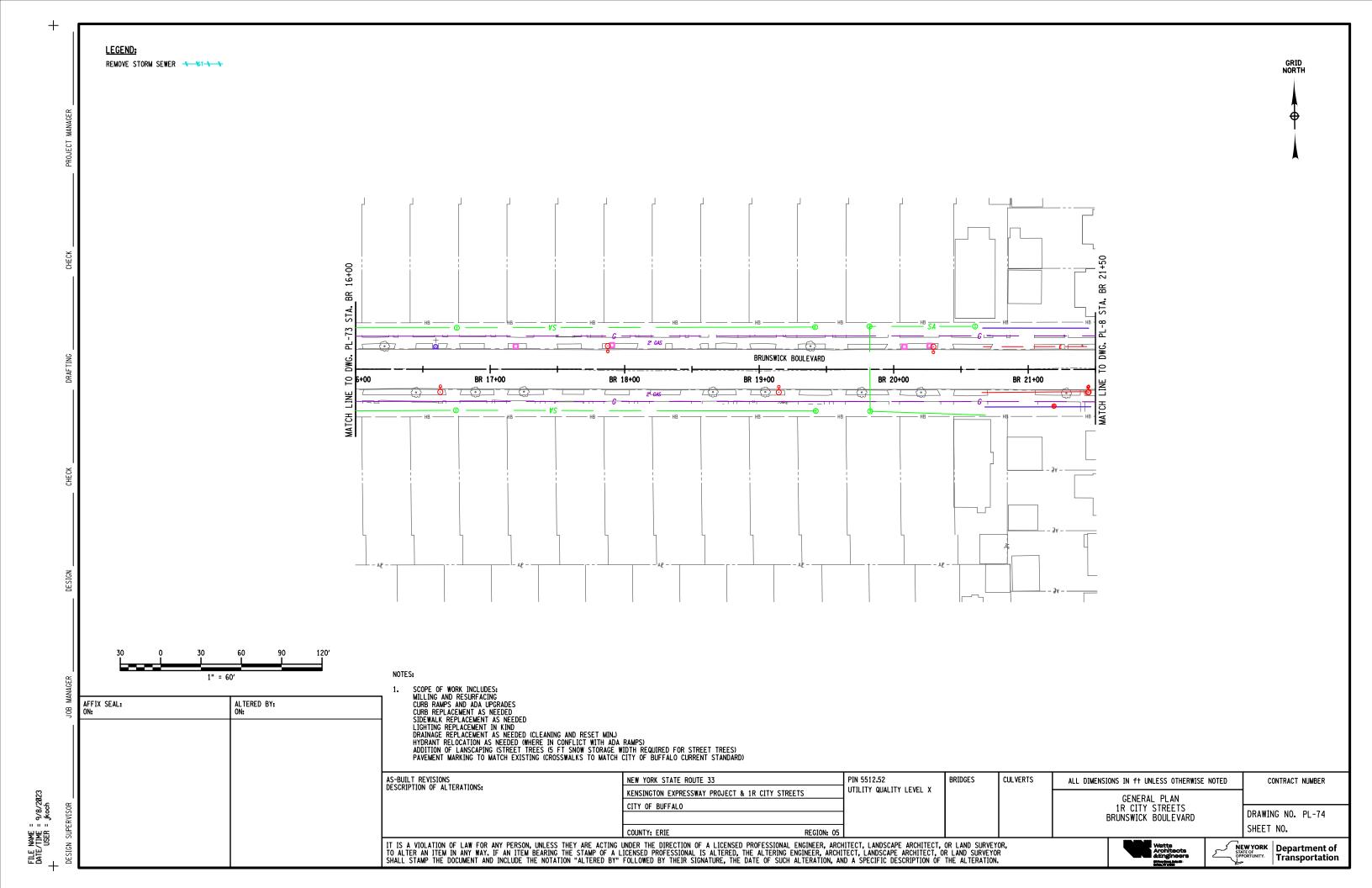


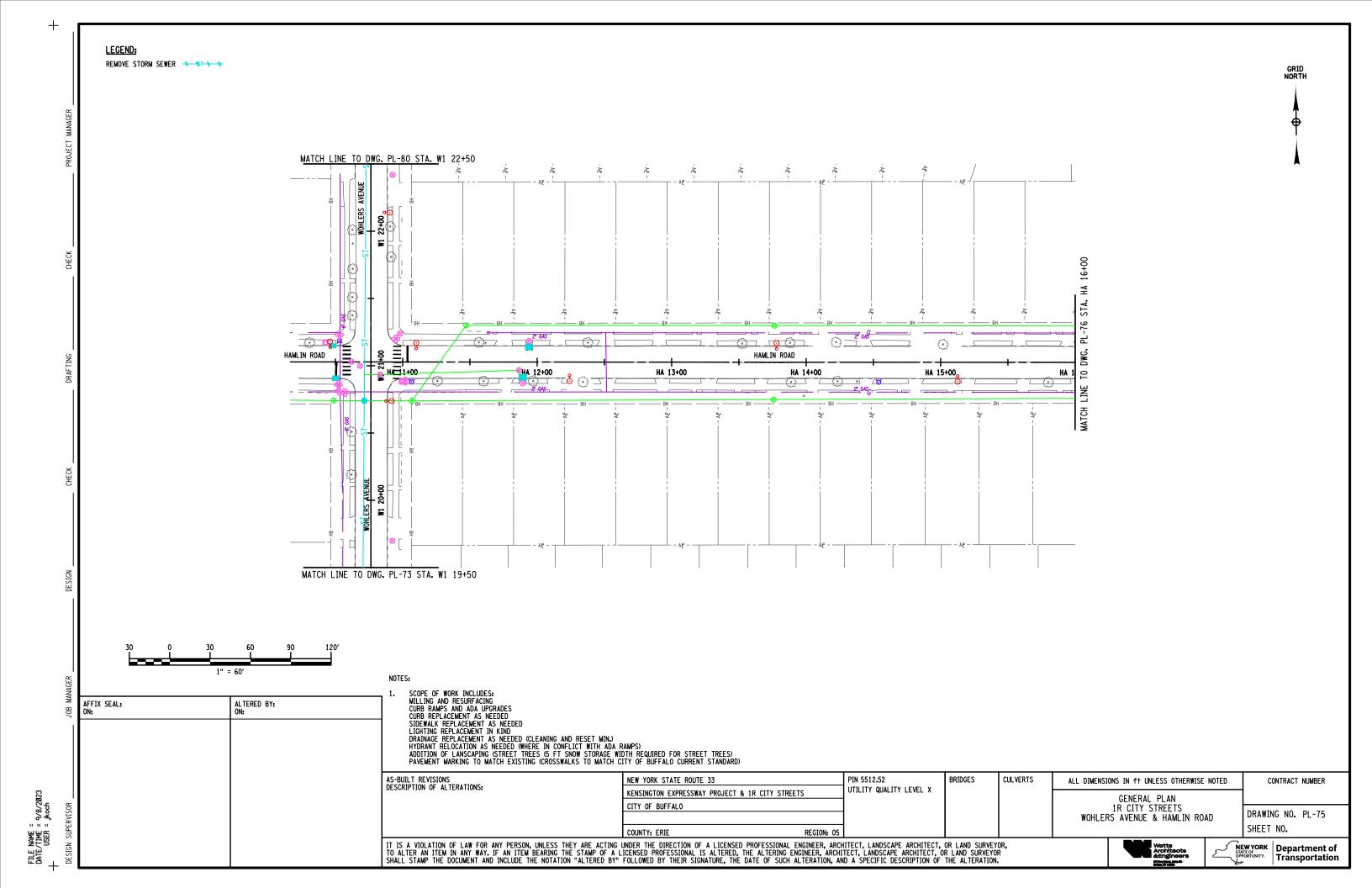


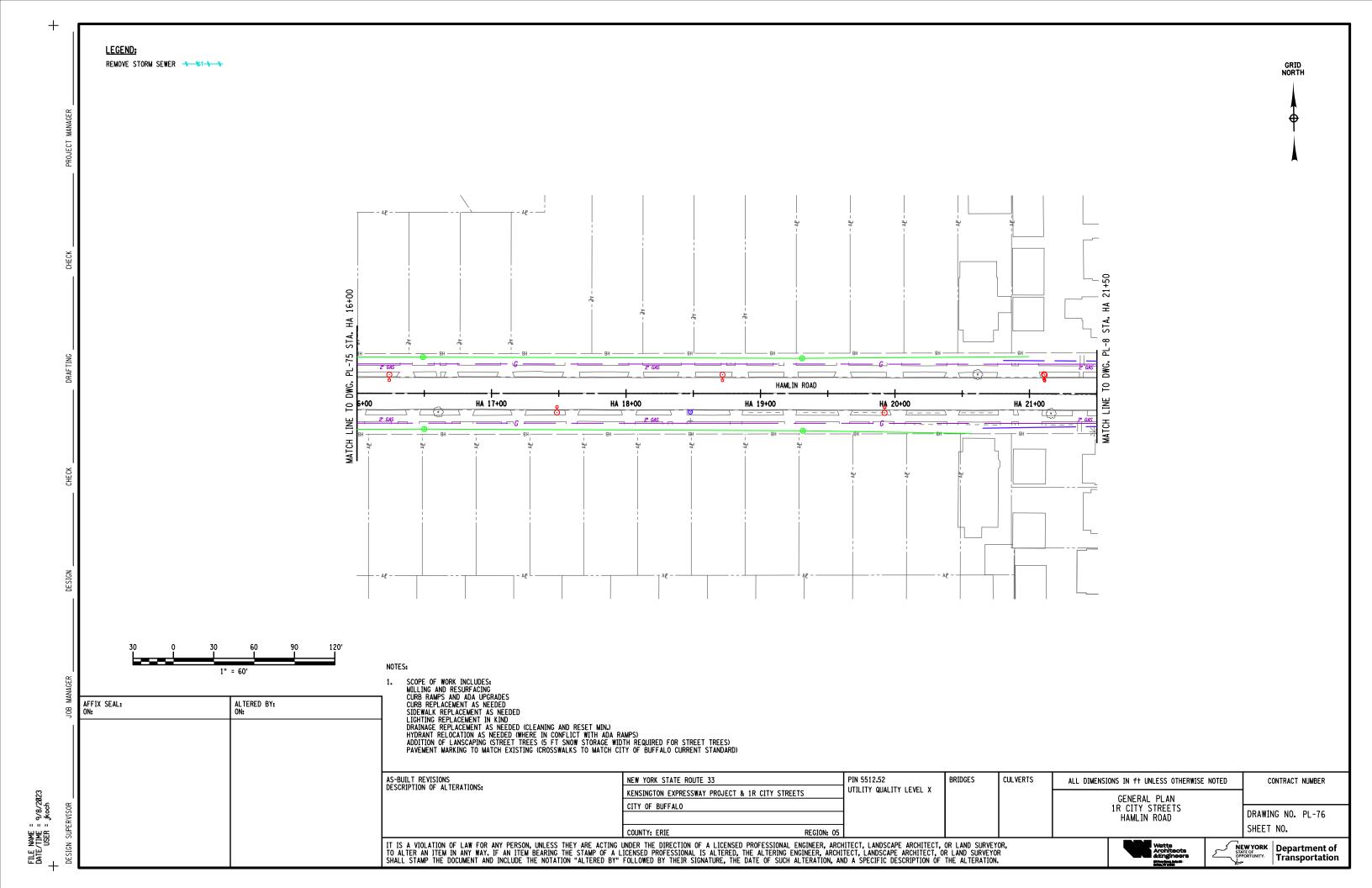


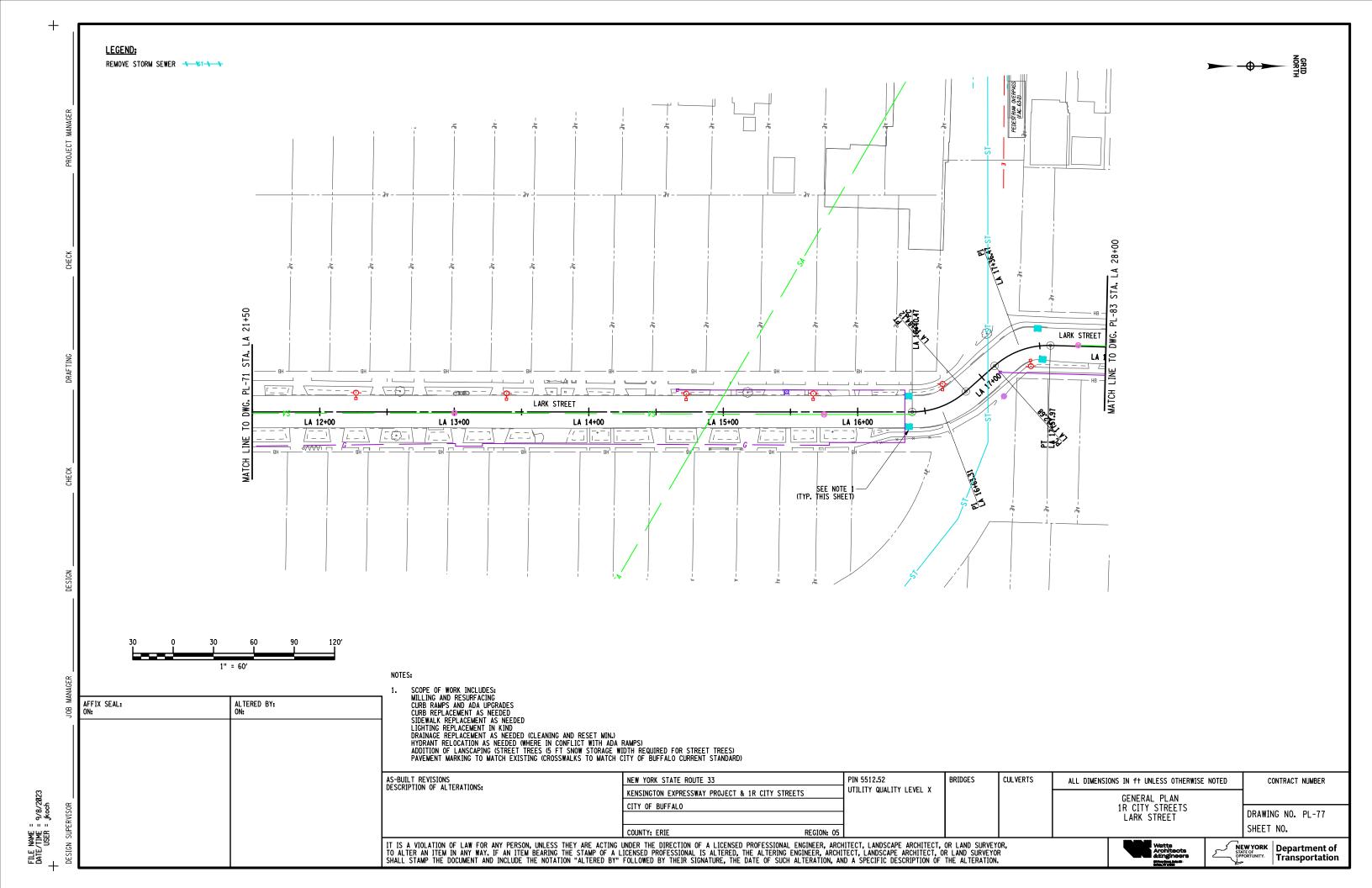


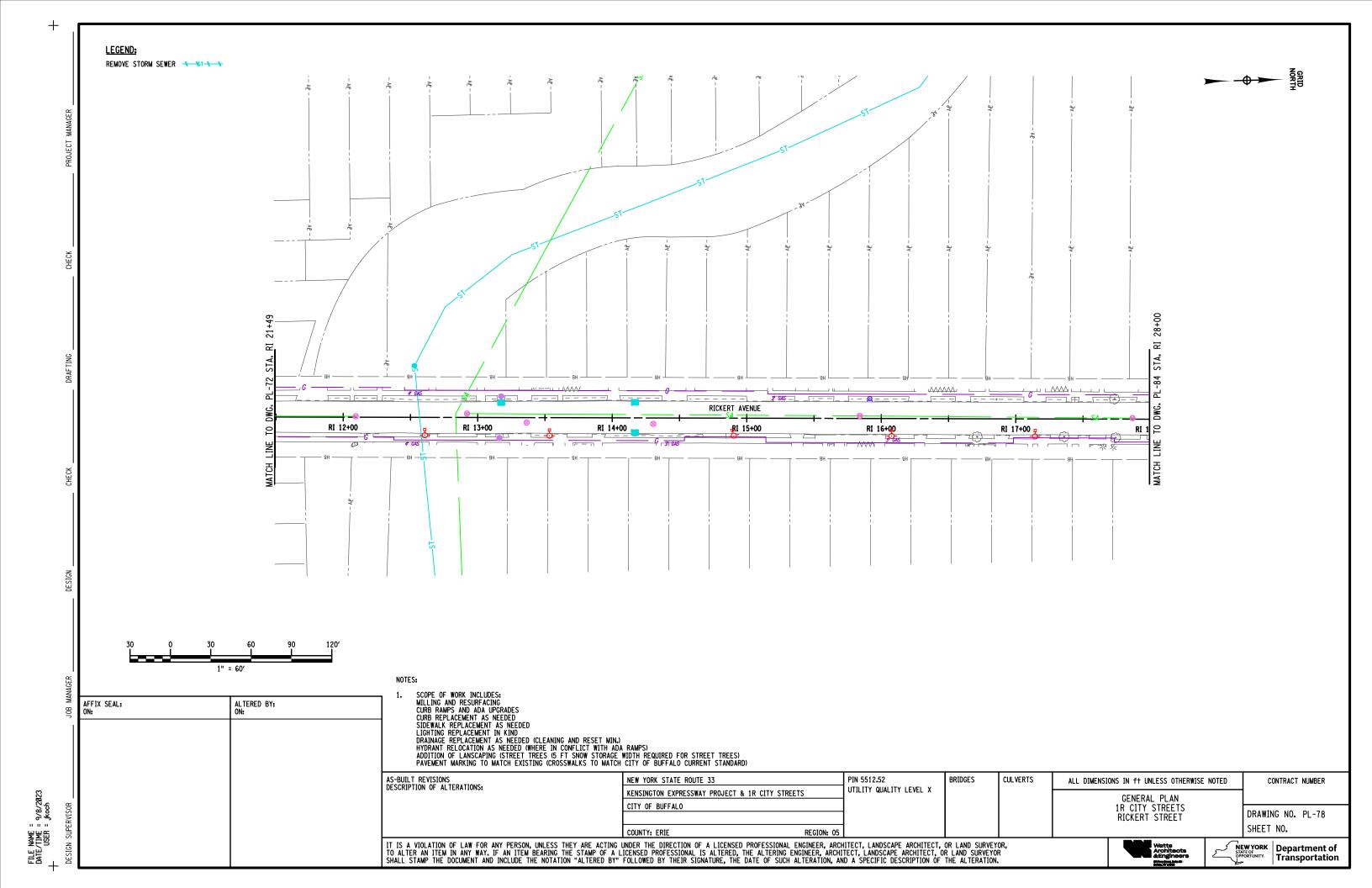


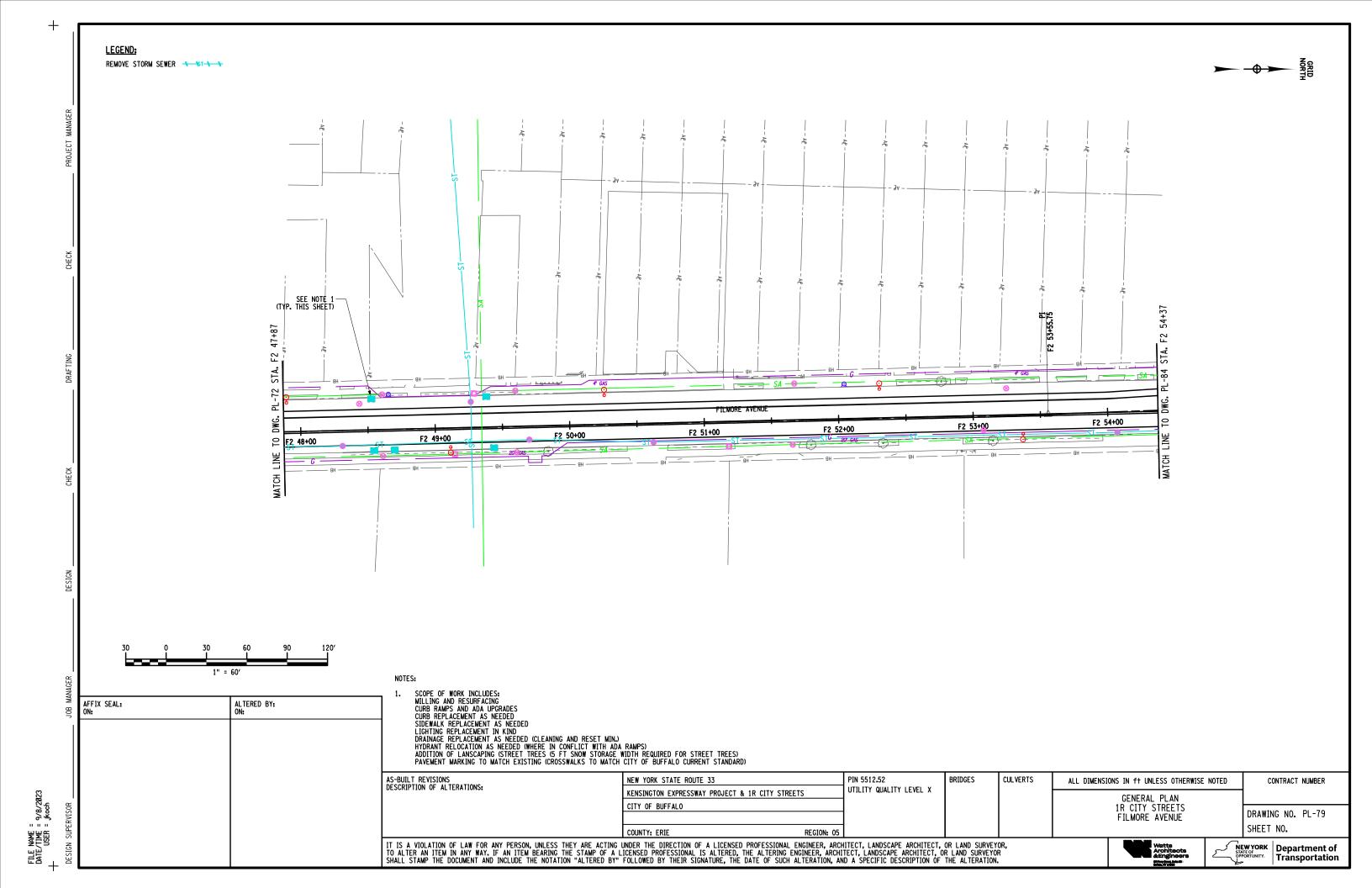


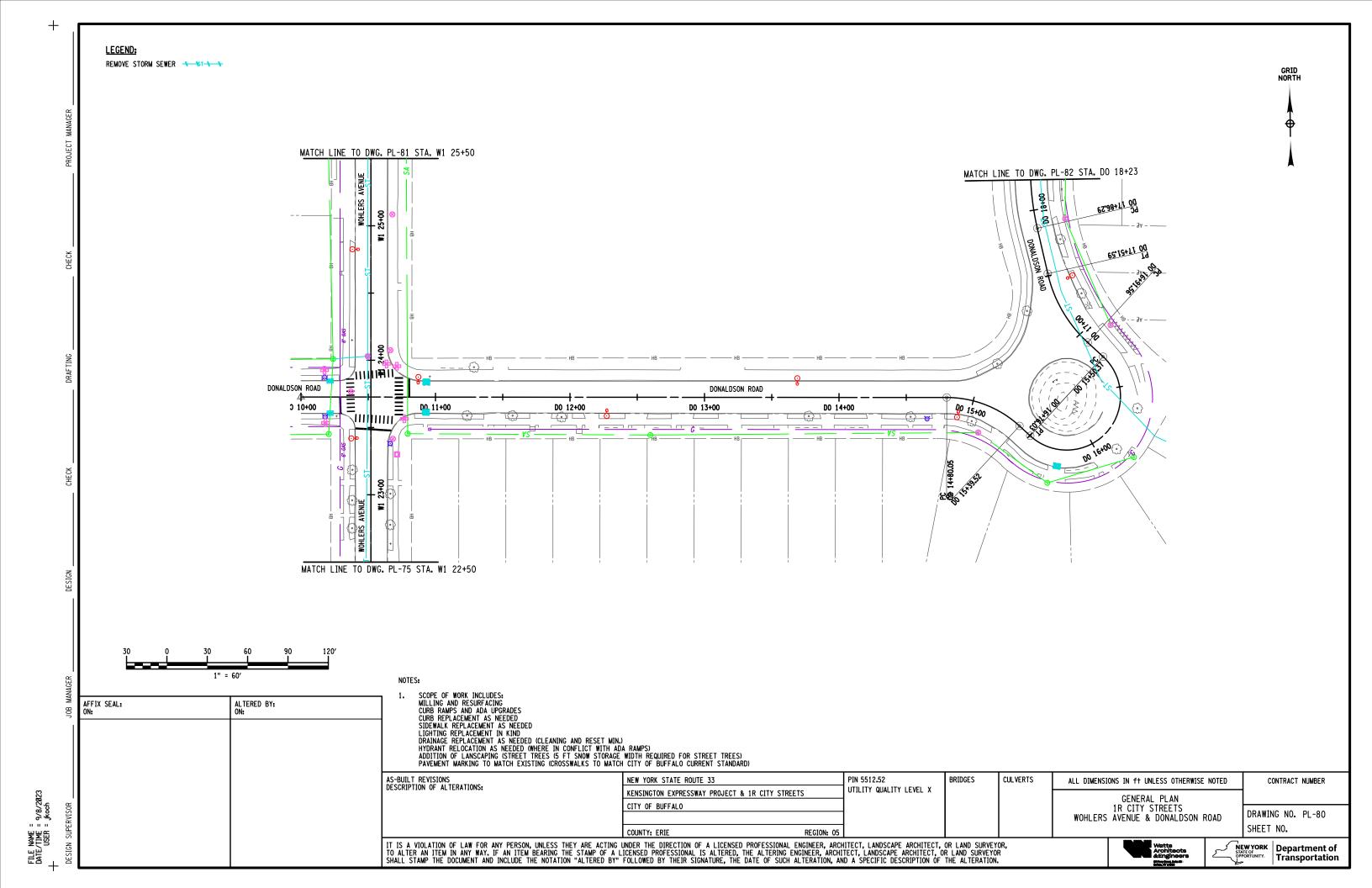


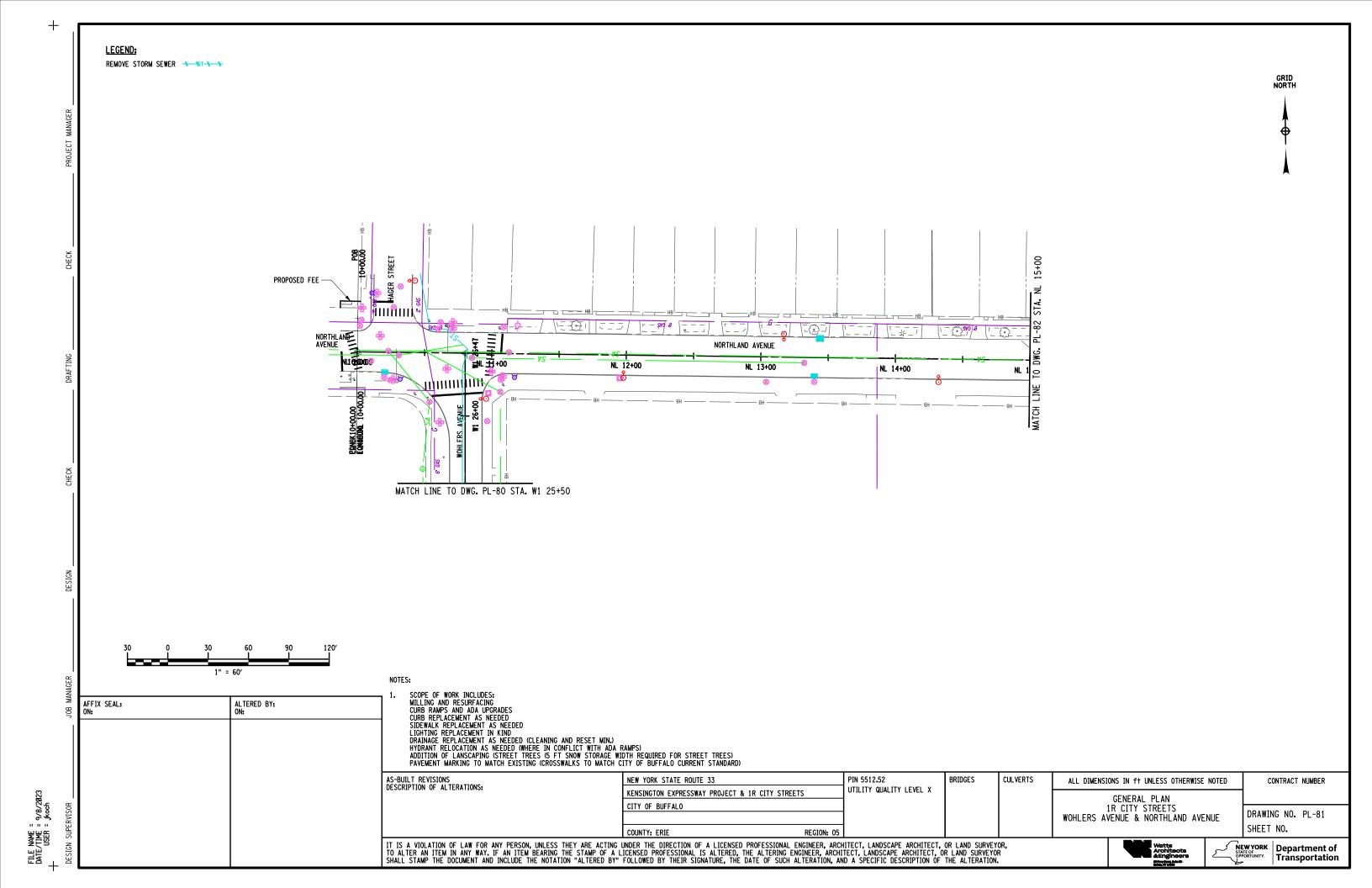


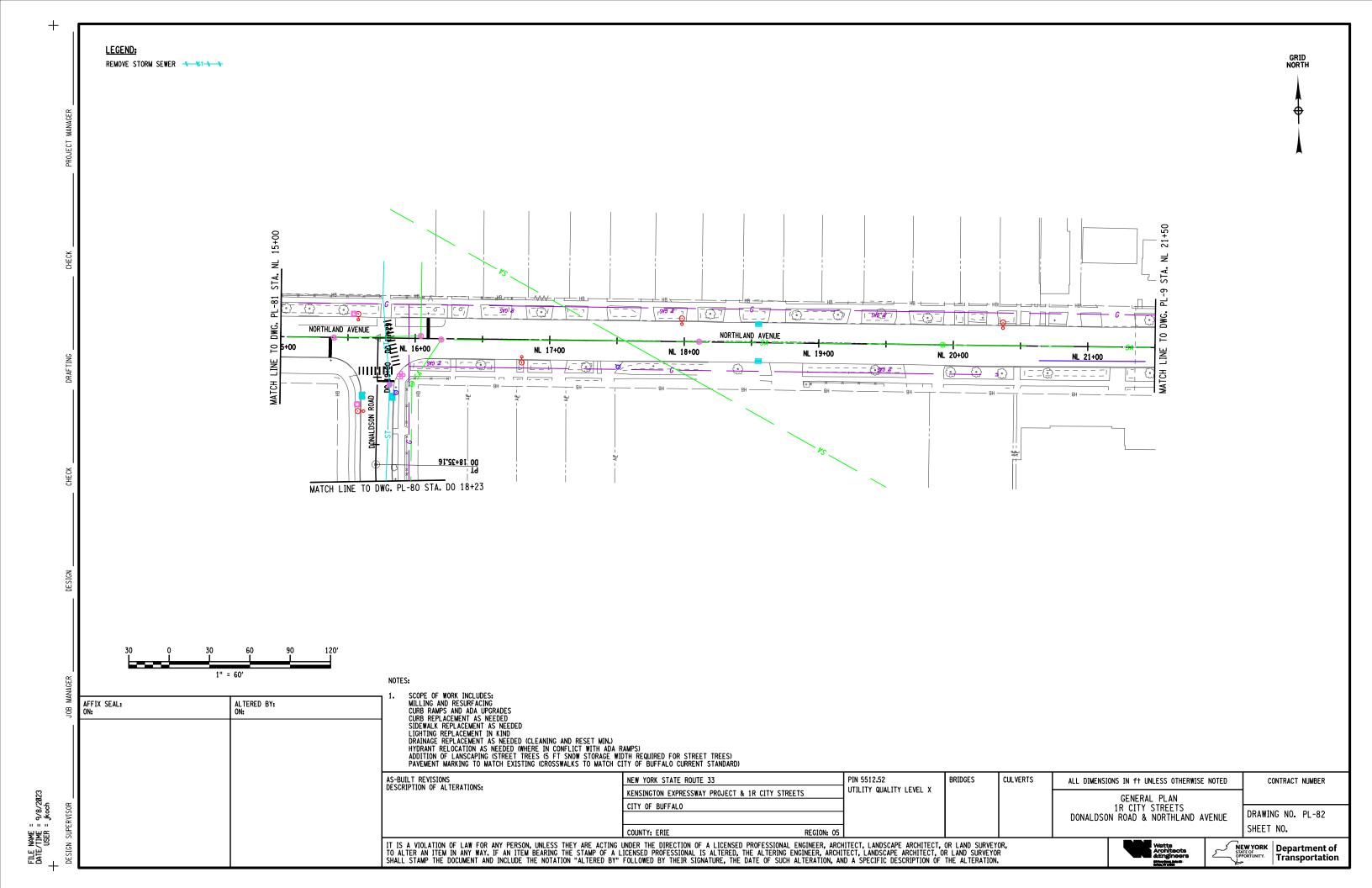


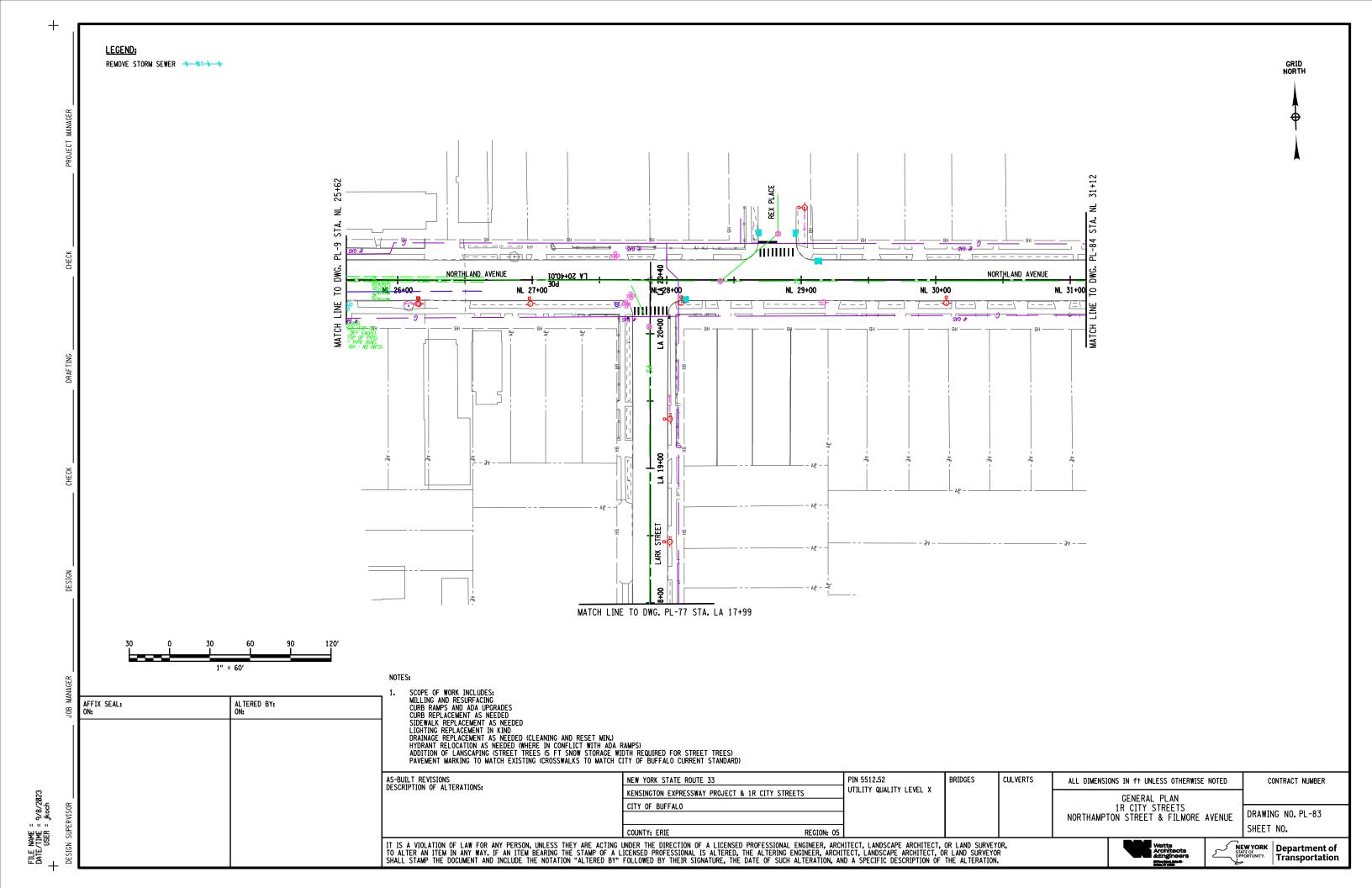


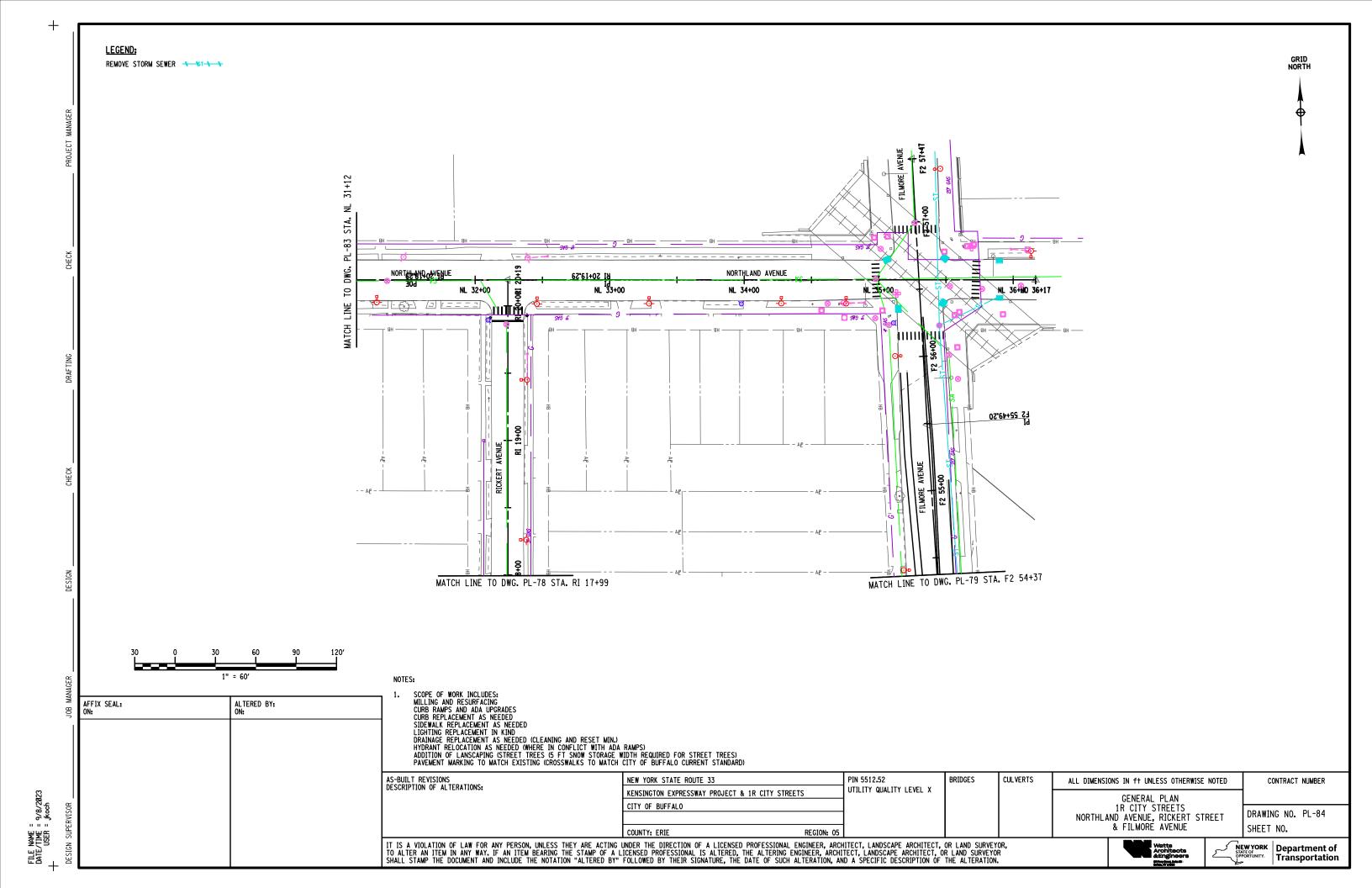


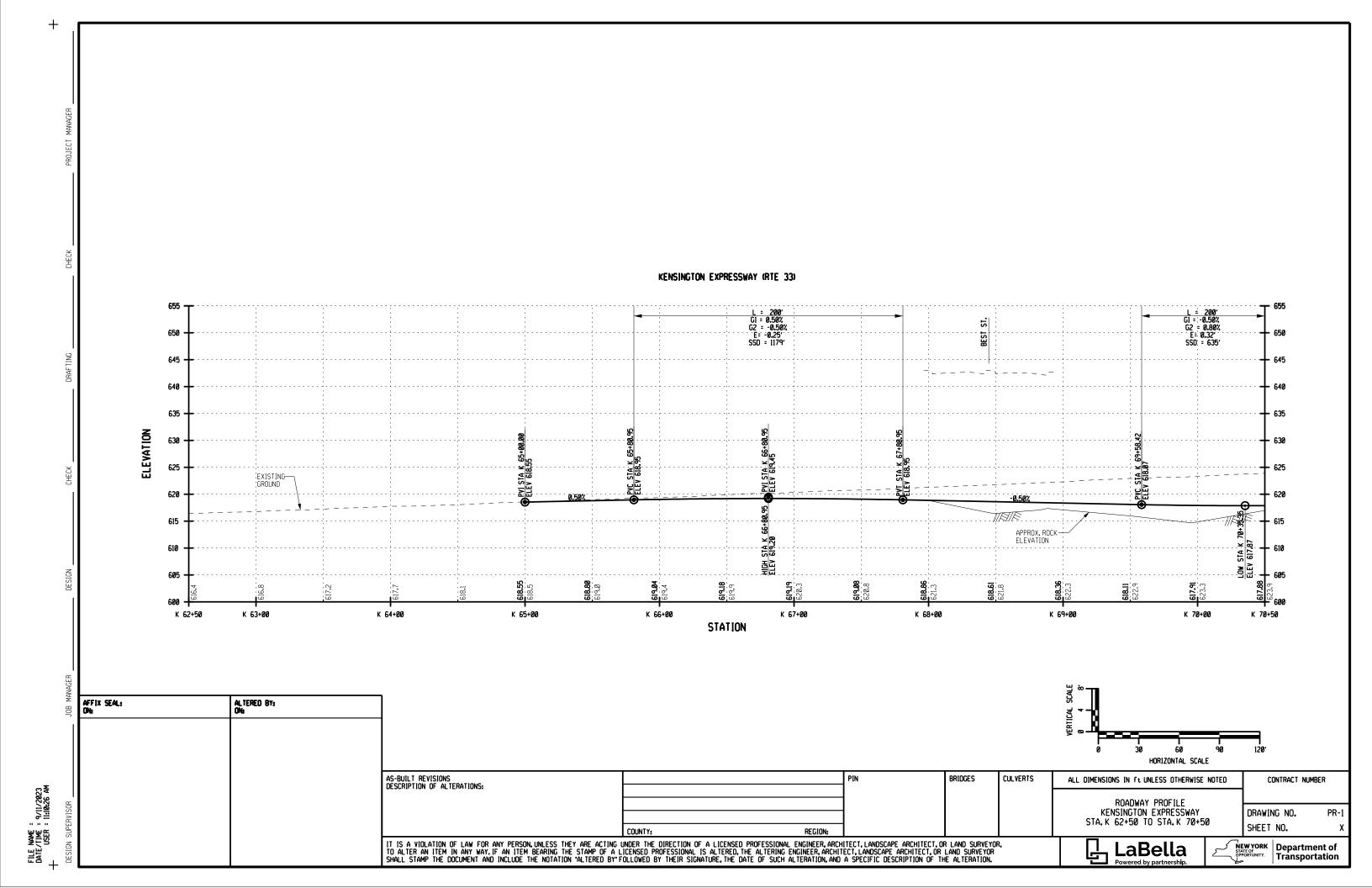


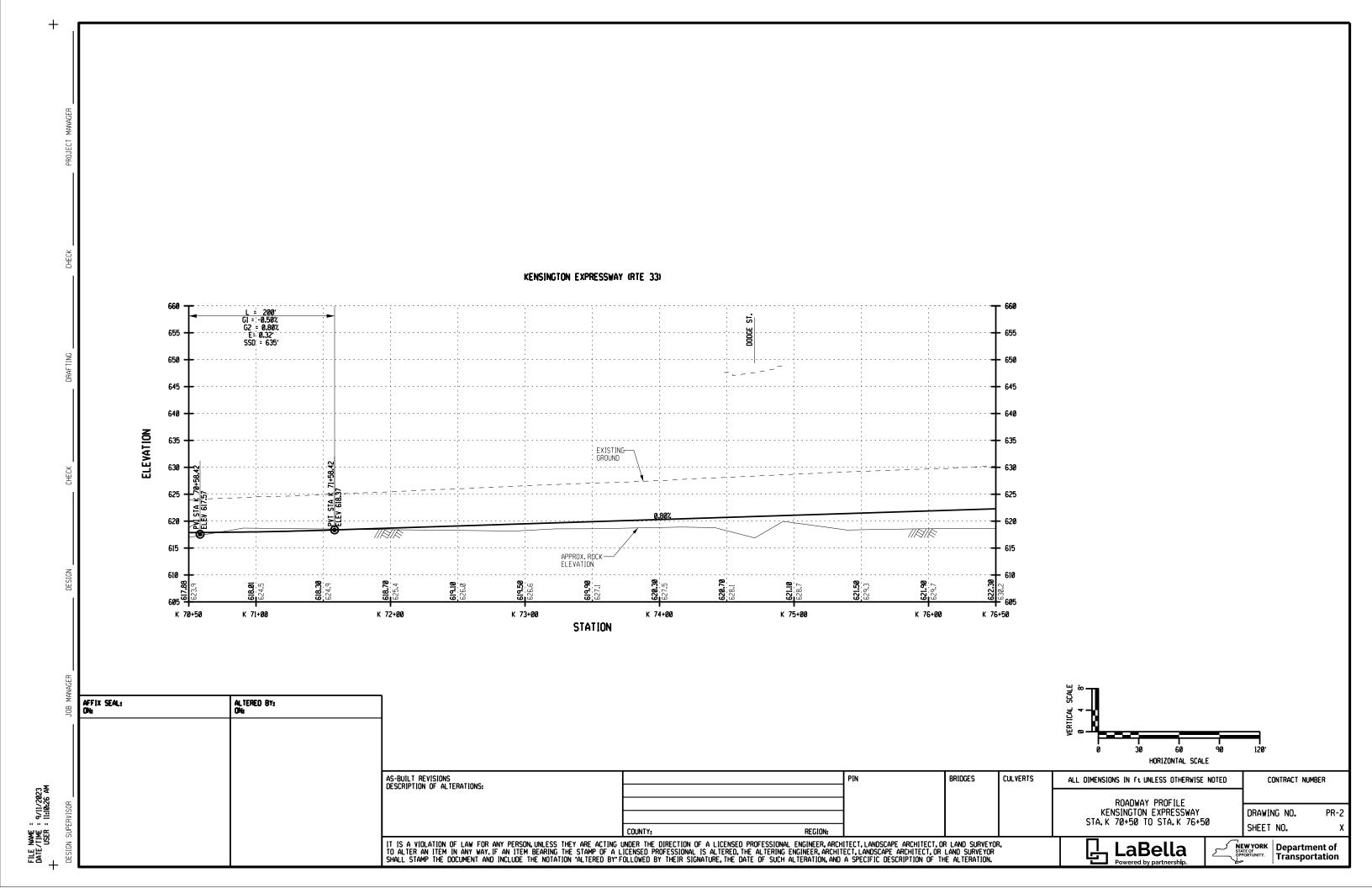


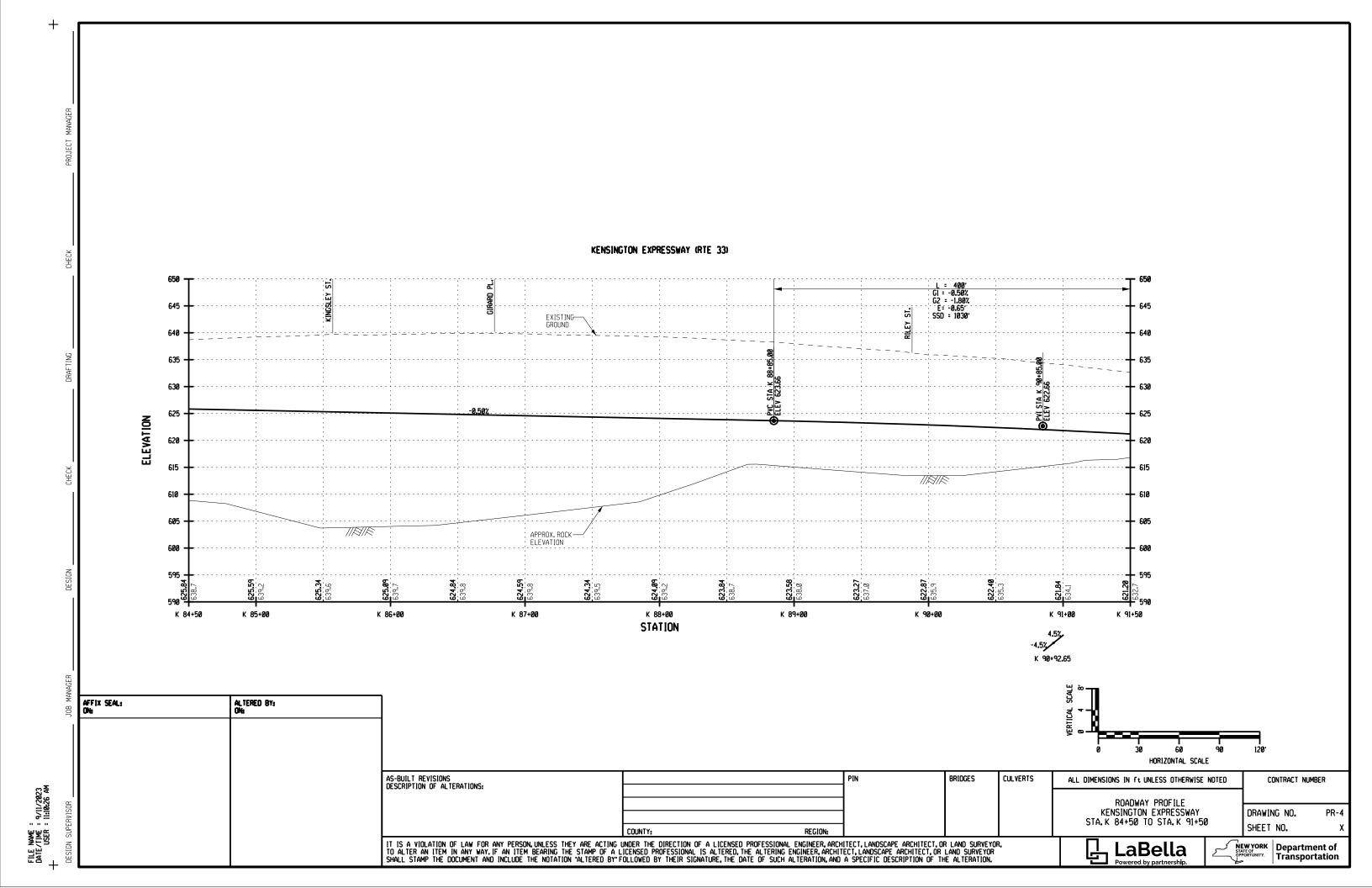


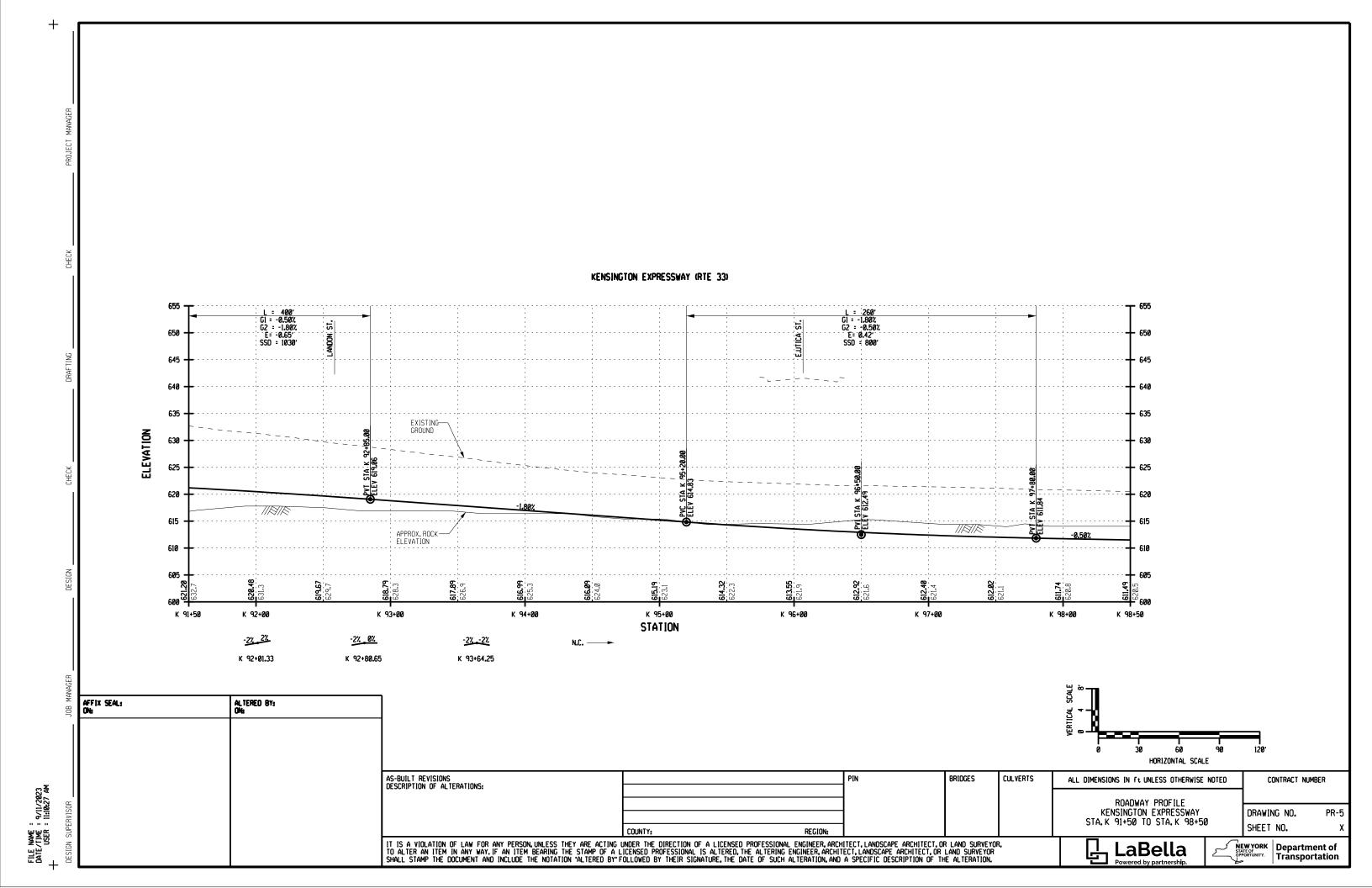


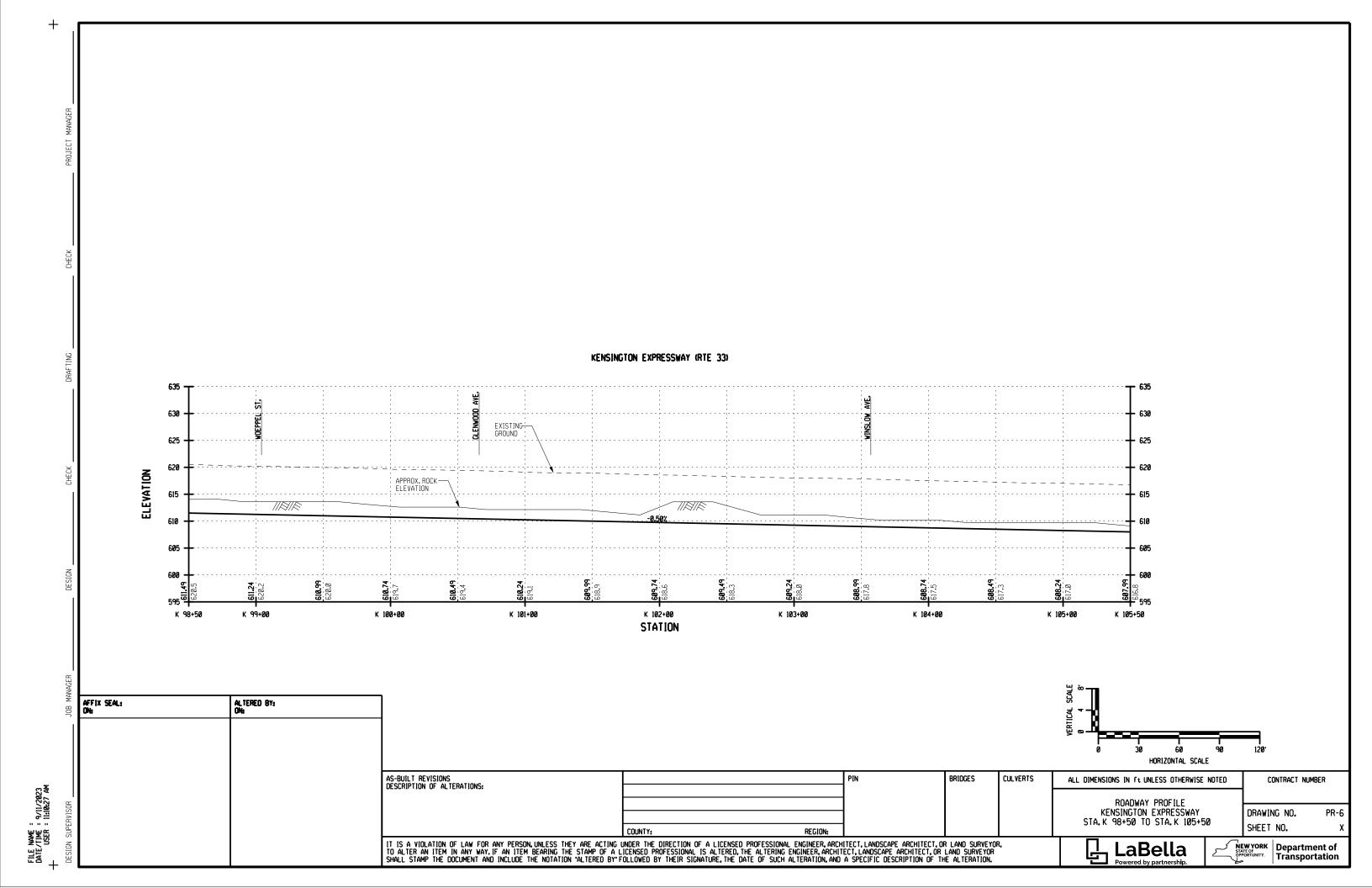


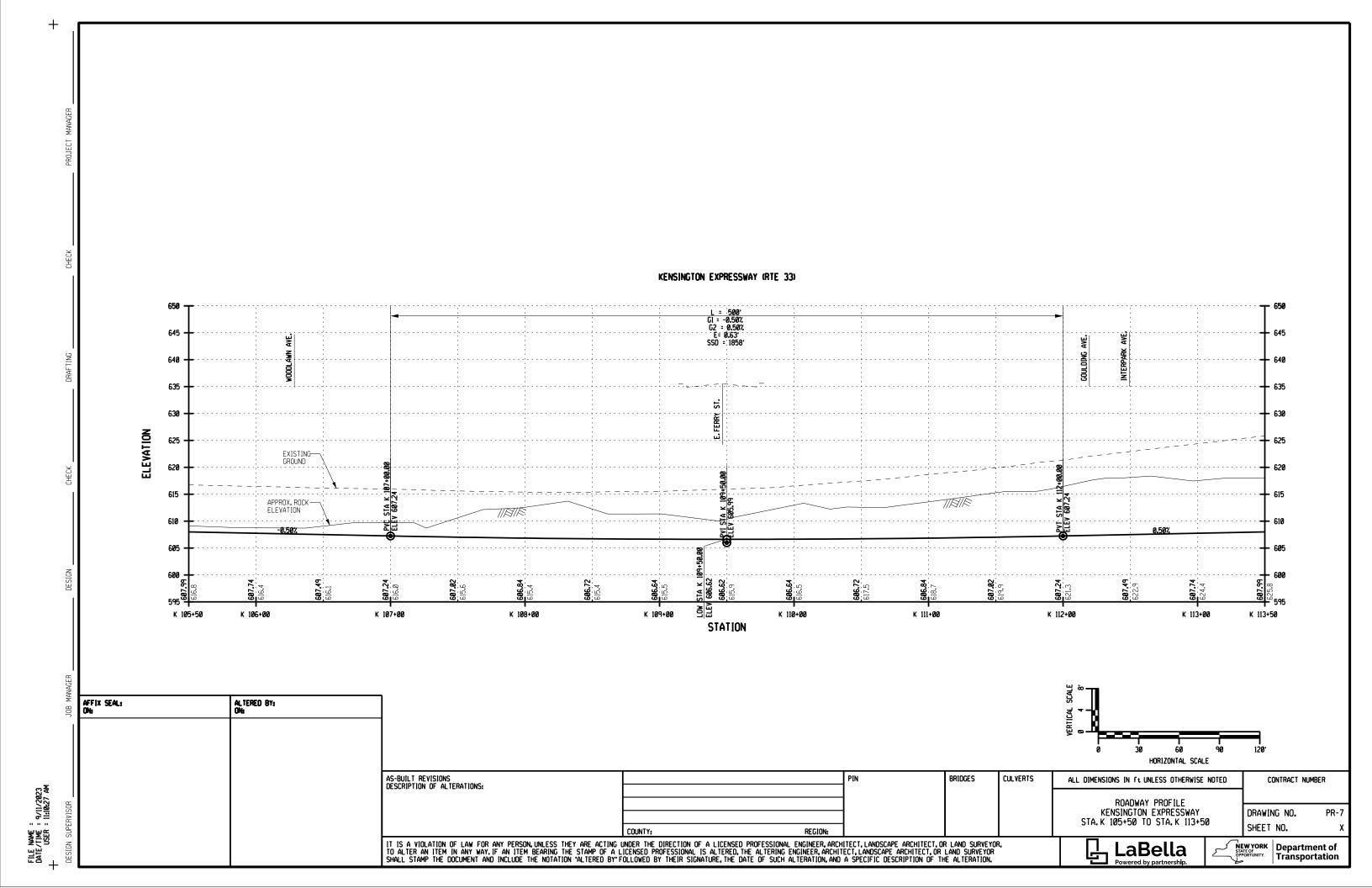


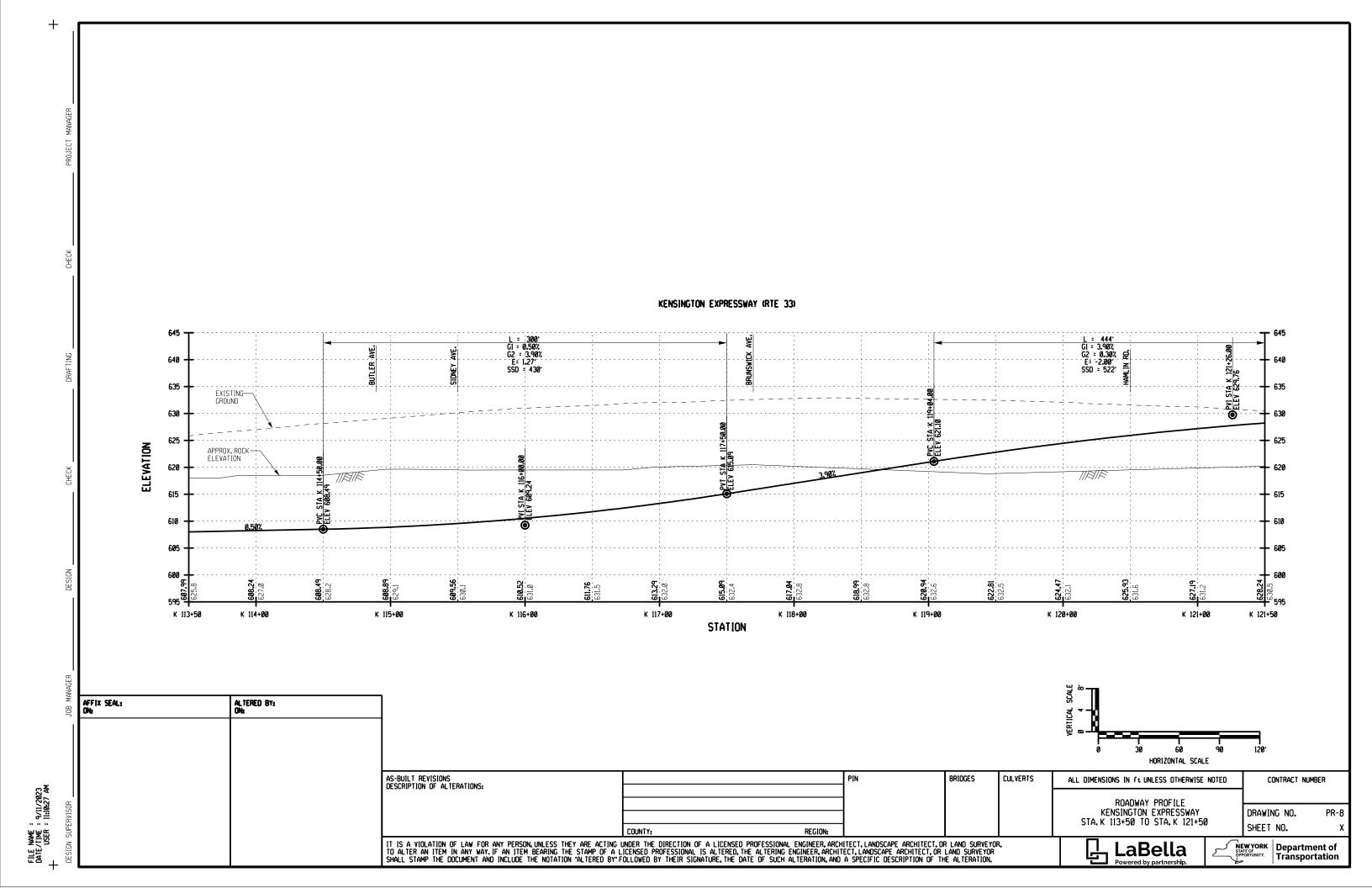


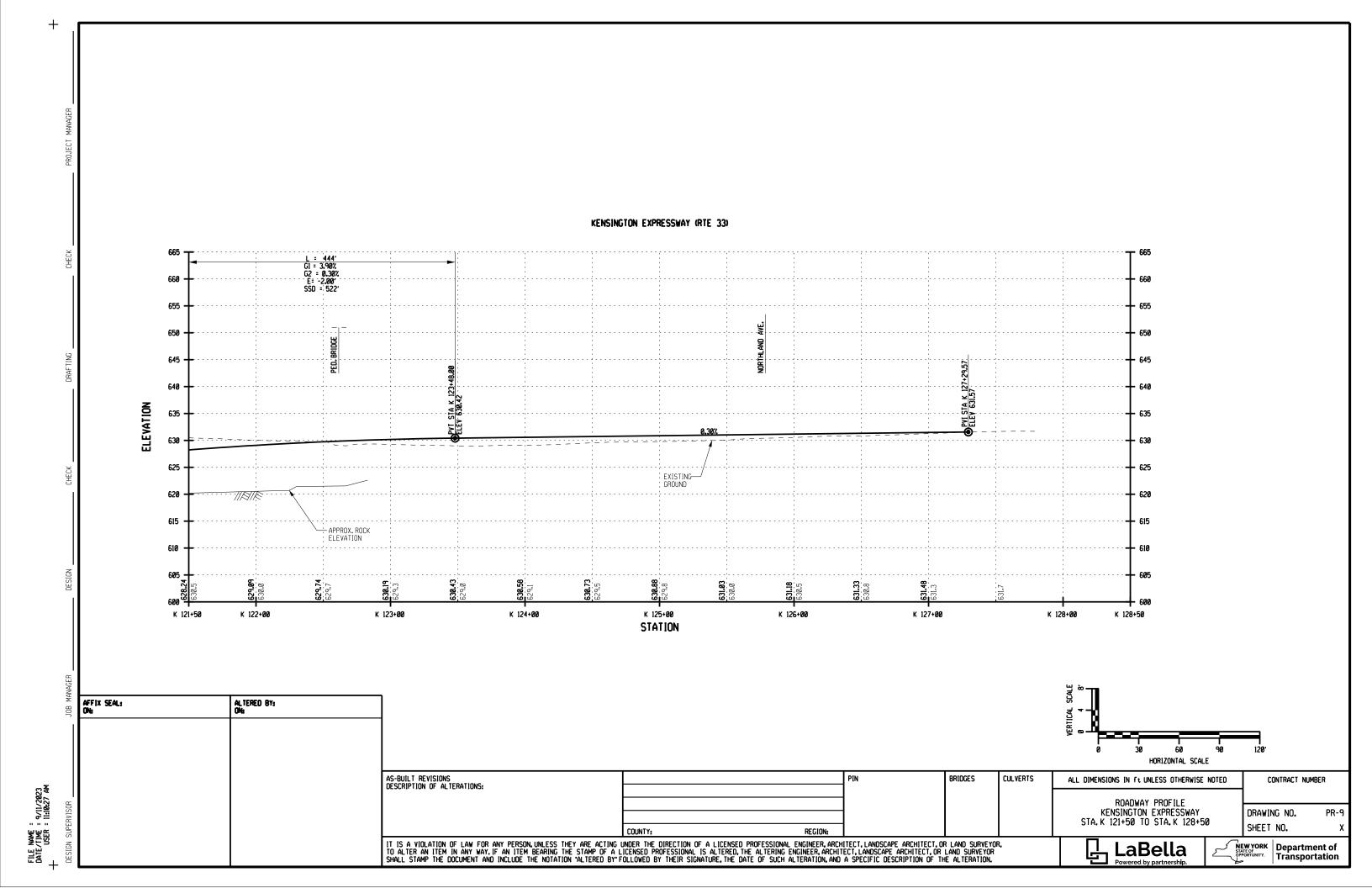


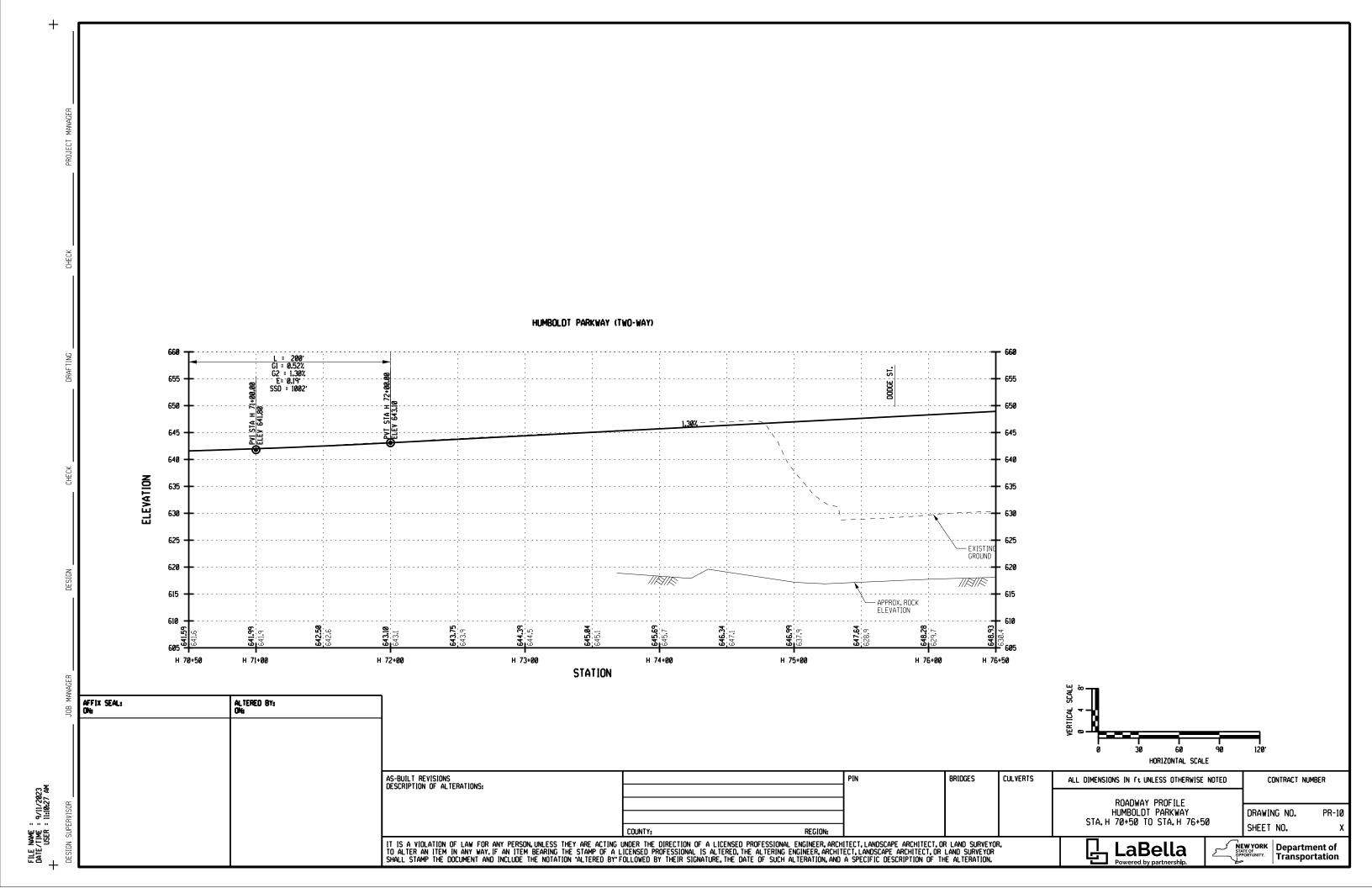


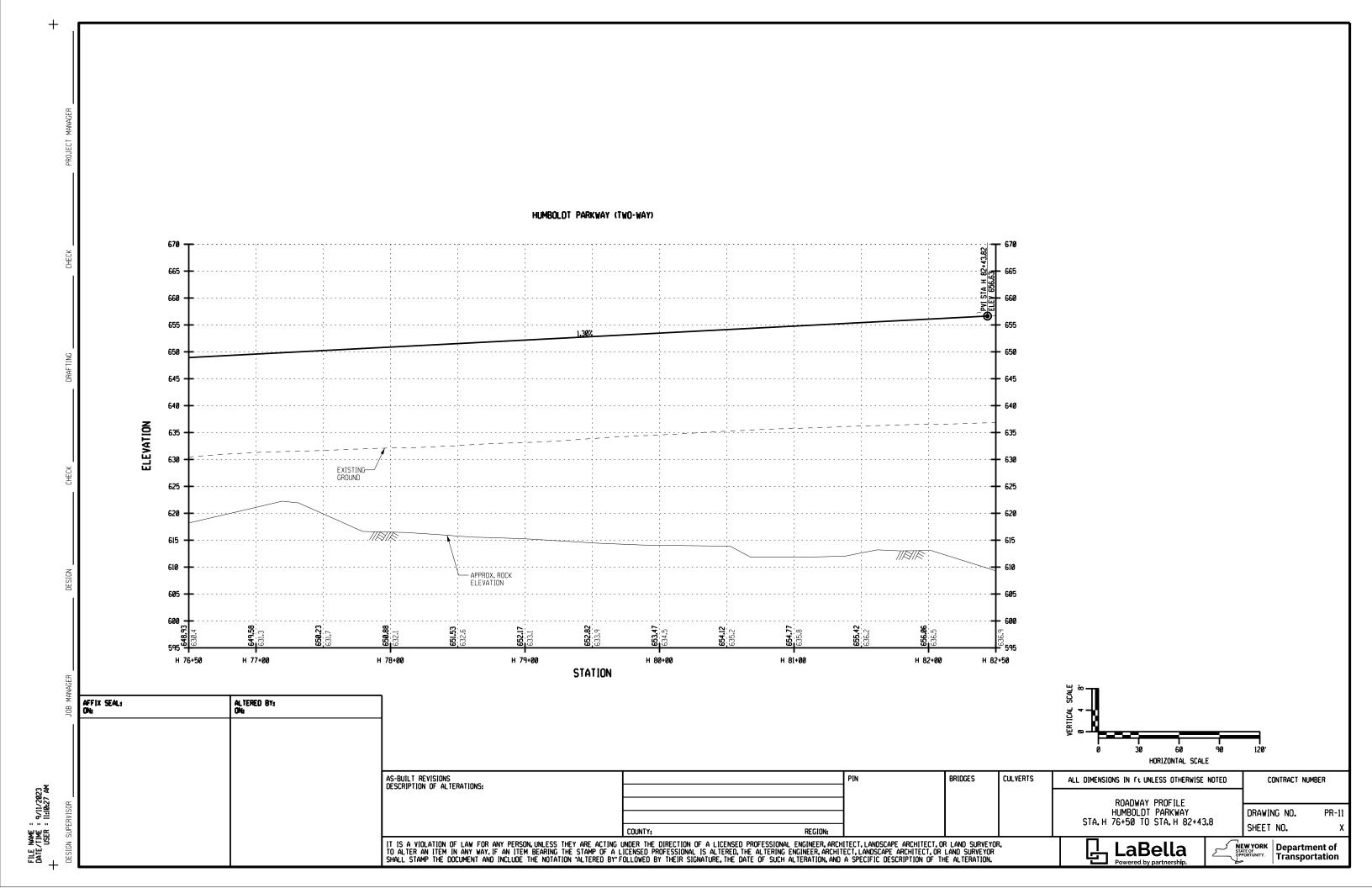




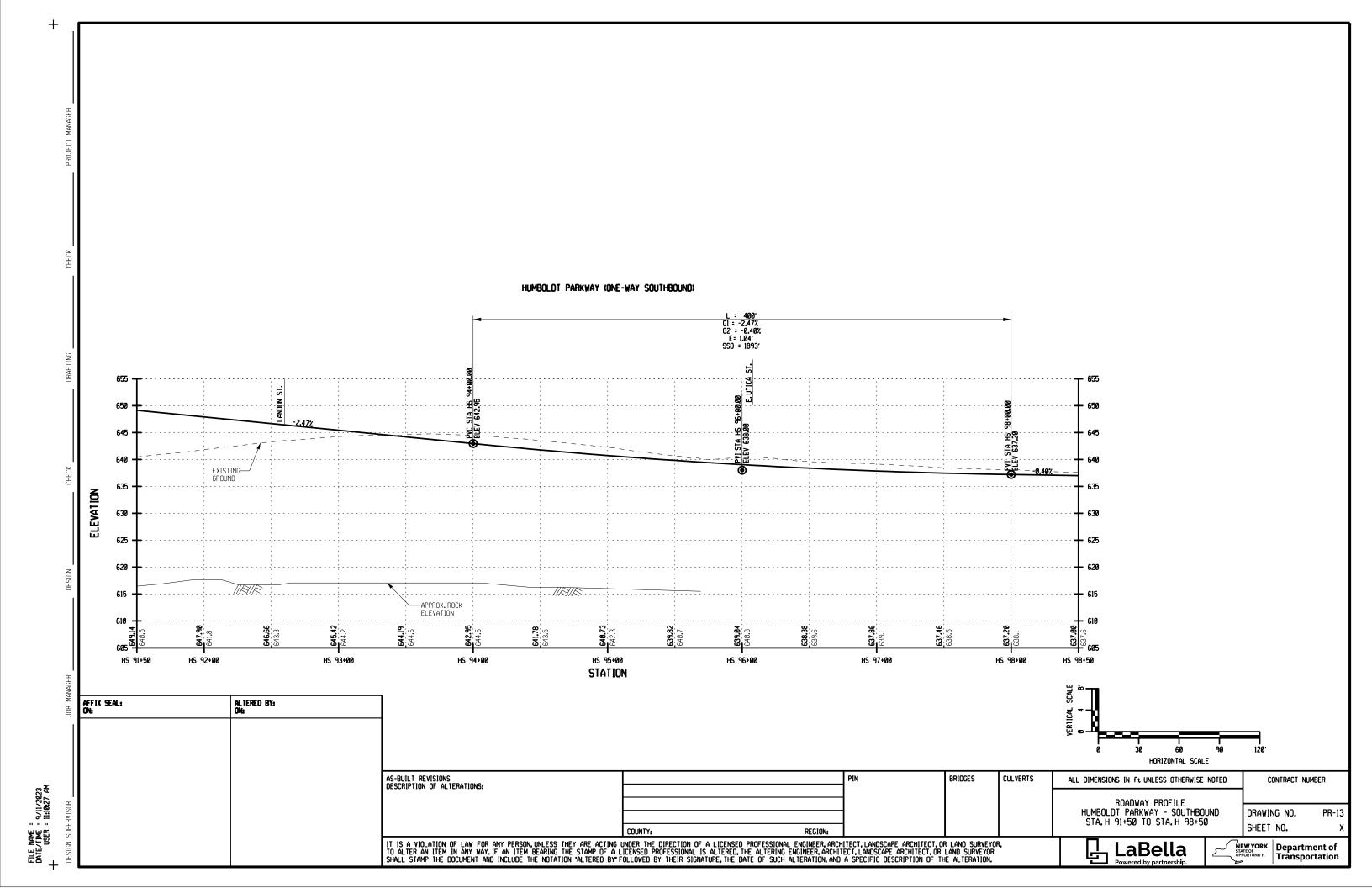


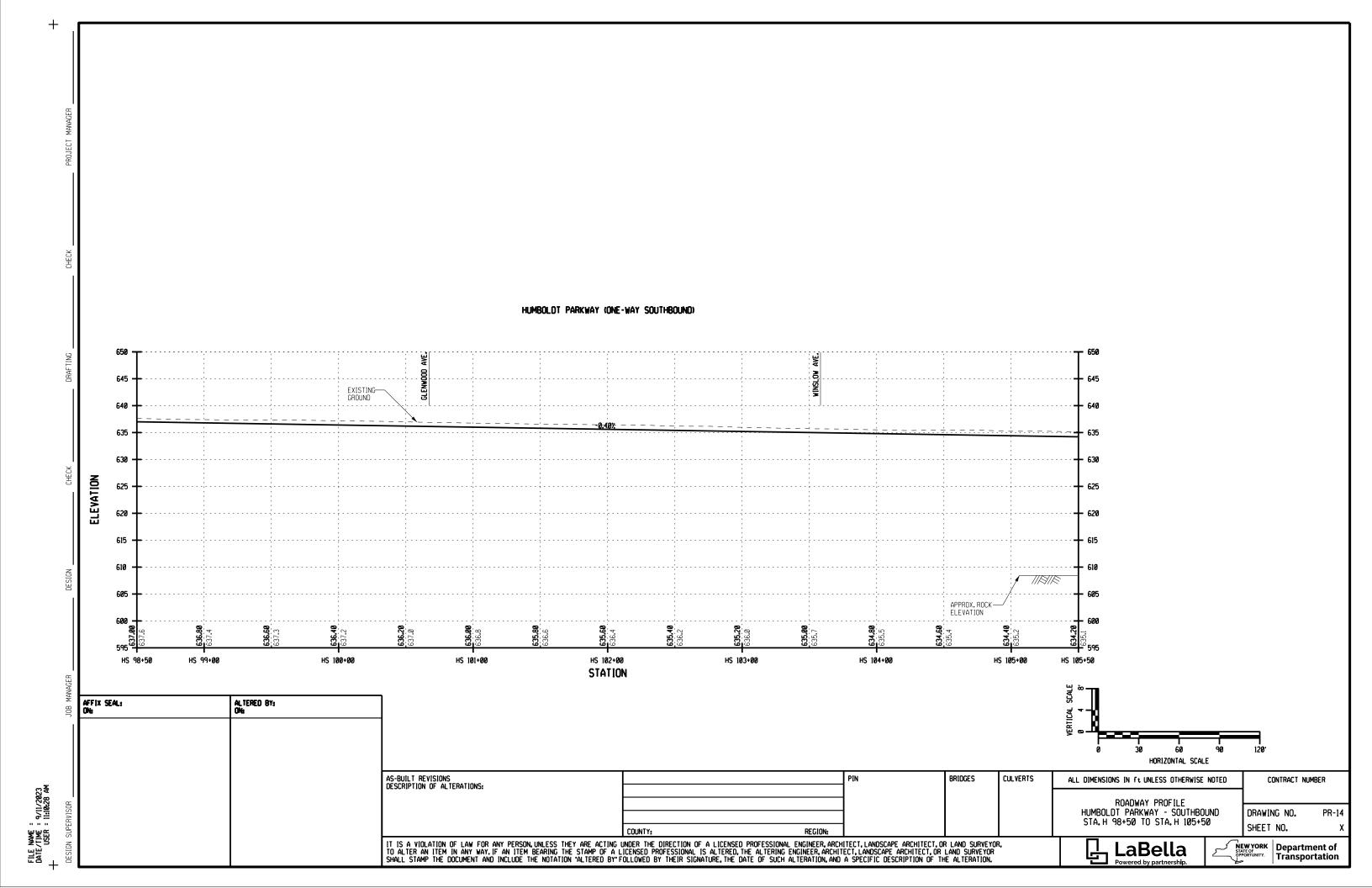


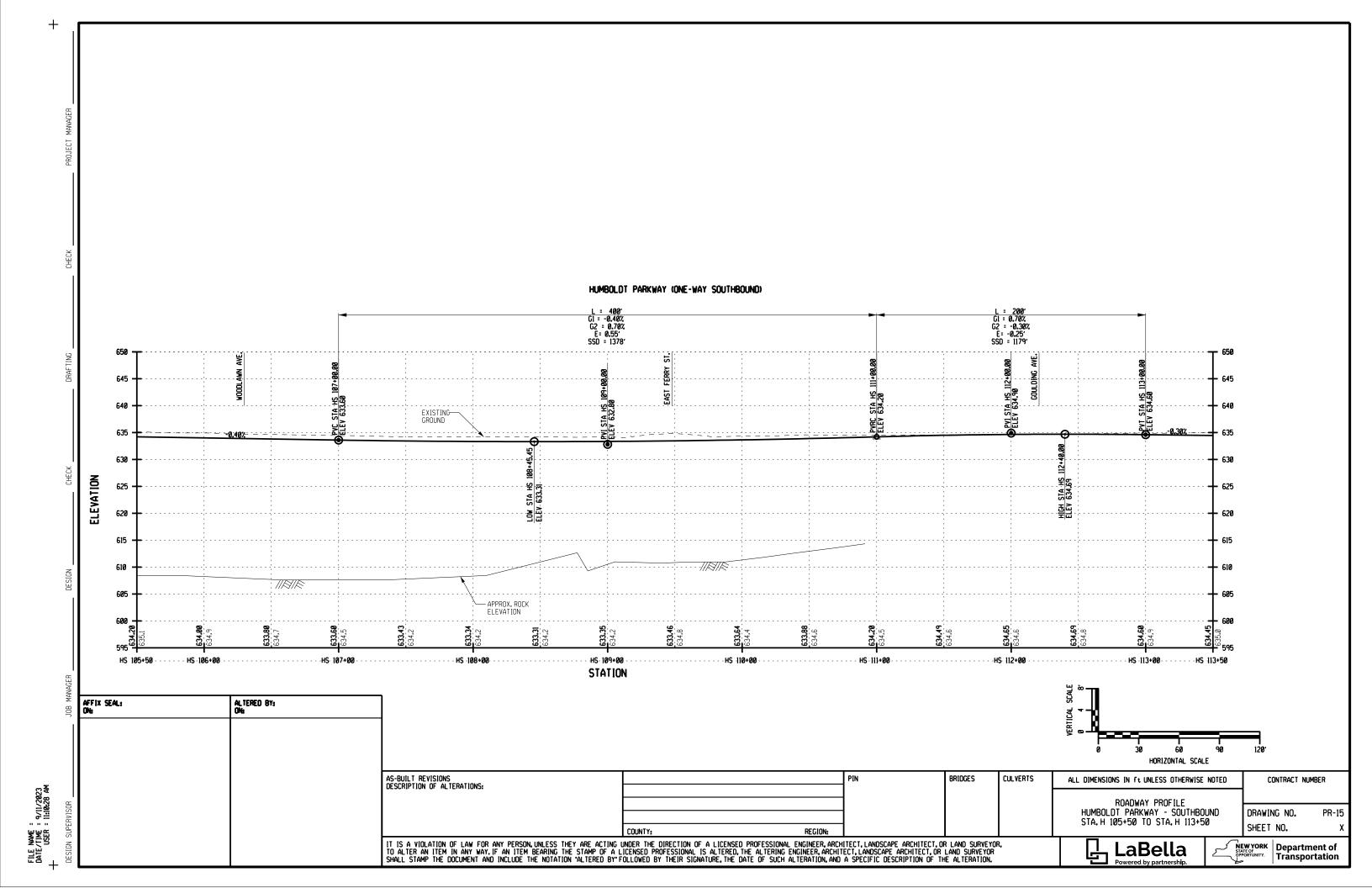


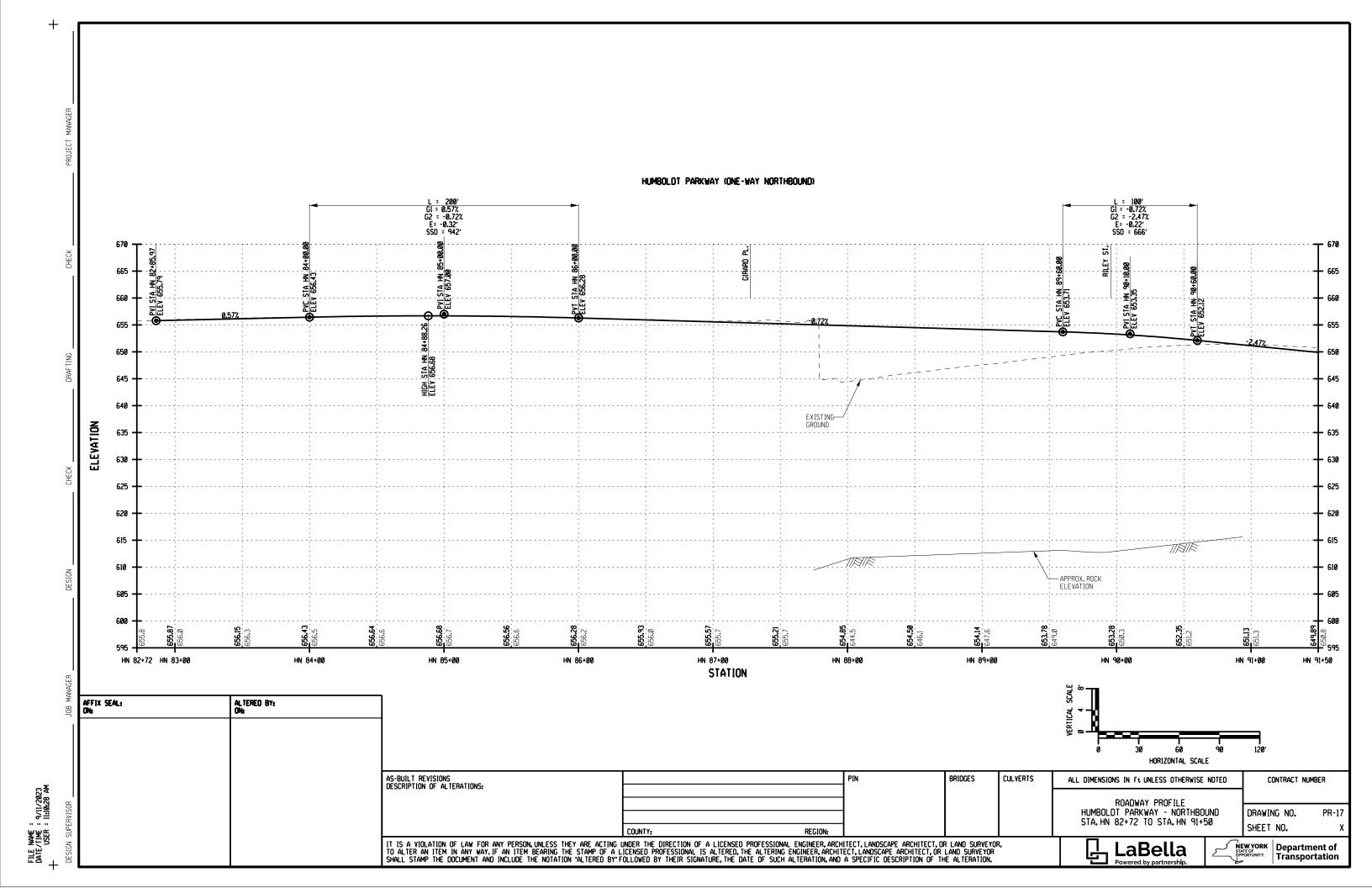


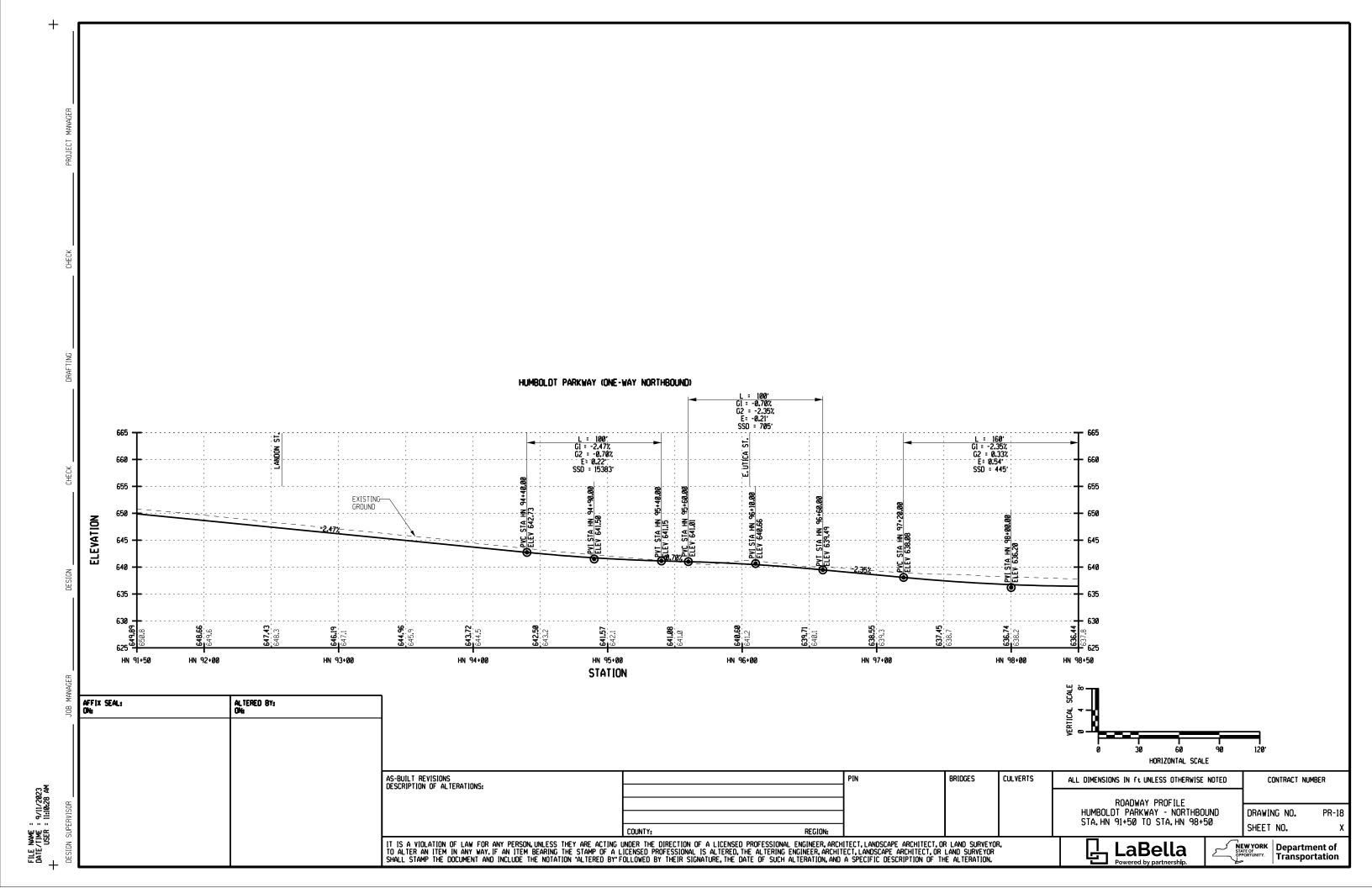
L = 200' G1 = -0.52% G2 = -2.47% E= -0.49' SSD = 652' HUMBOLDT PARKWAY (ONE-WAY SOUTHBOUND) RILEY ST. 665 660 655 650 ELEVATION EXISTING-GROUND 625 620 615 HS 87+00 STATION HS 82+66 HS 83+00 HS 84+00 HS 85+00 HS 86+00 HS 88+00 HS 89+00 AFFIX SEAL: ALTERED BY: HORIZONTAL SCALE AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: CULVERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER ROADWAY PROFILE HUMBOLDT PARKWAY - SOUTHBOUND STA.H 82+82 TO STA.H 91+50 DRAWING NO. PR-12 SHEET NO. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. NEW YORK STATE OF OPPORTUNITY. Department of Transportation LaBella Powered by partnership.

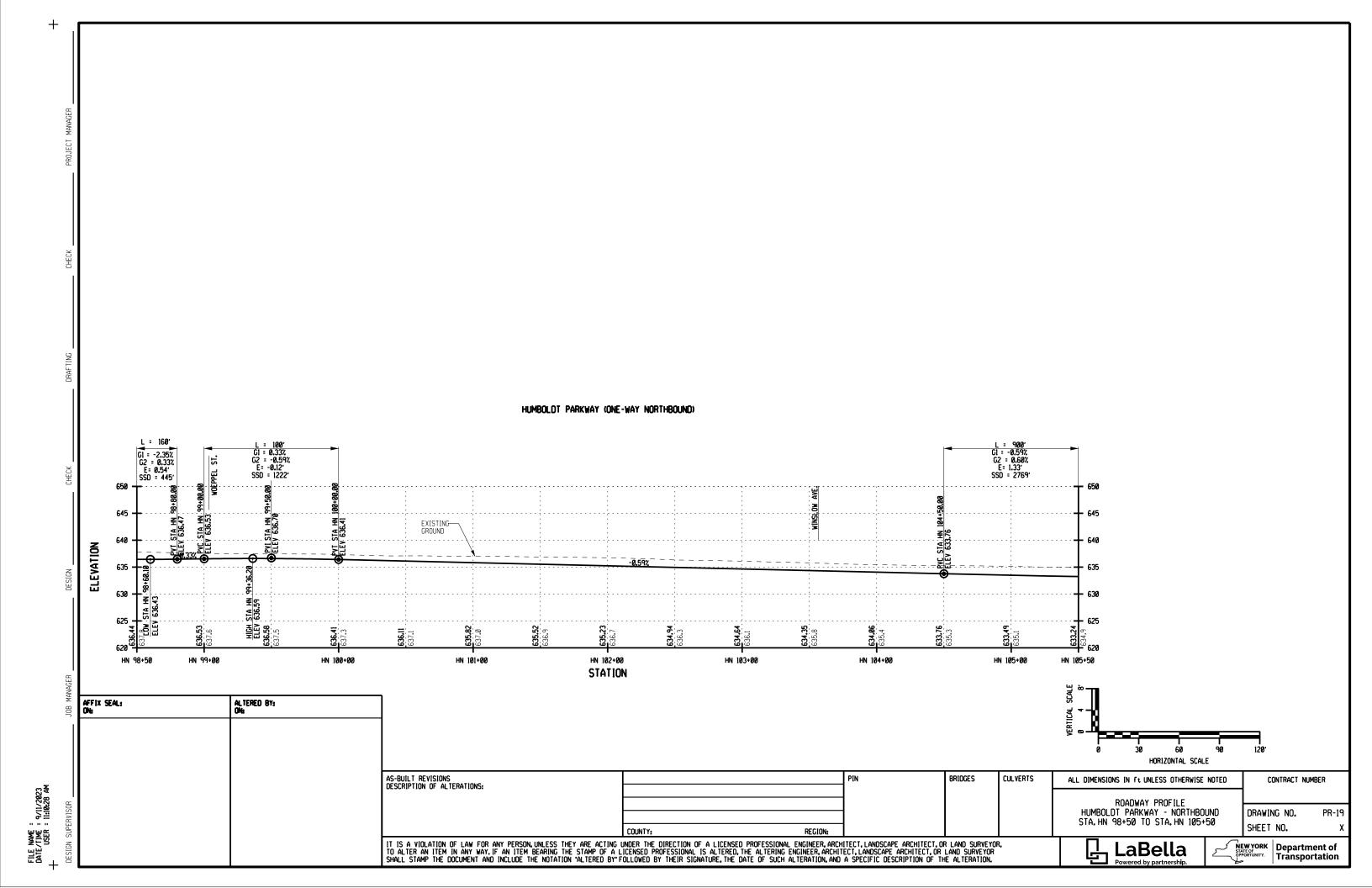


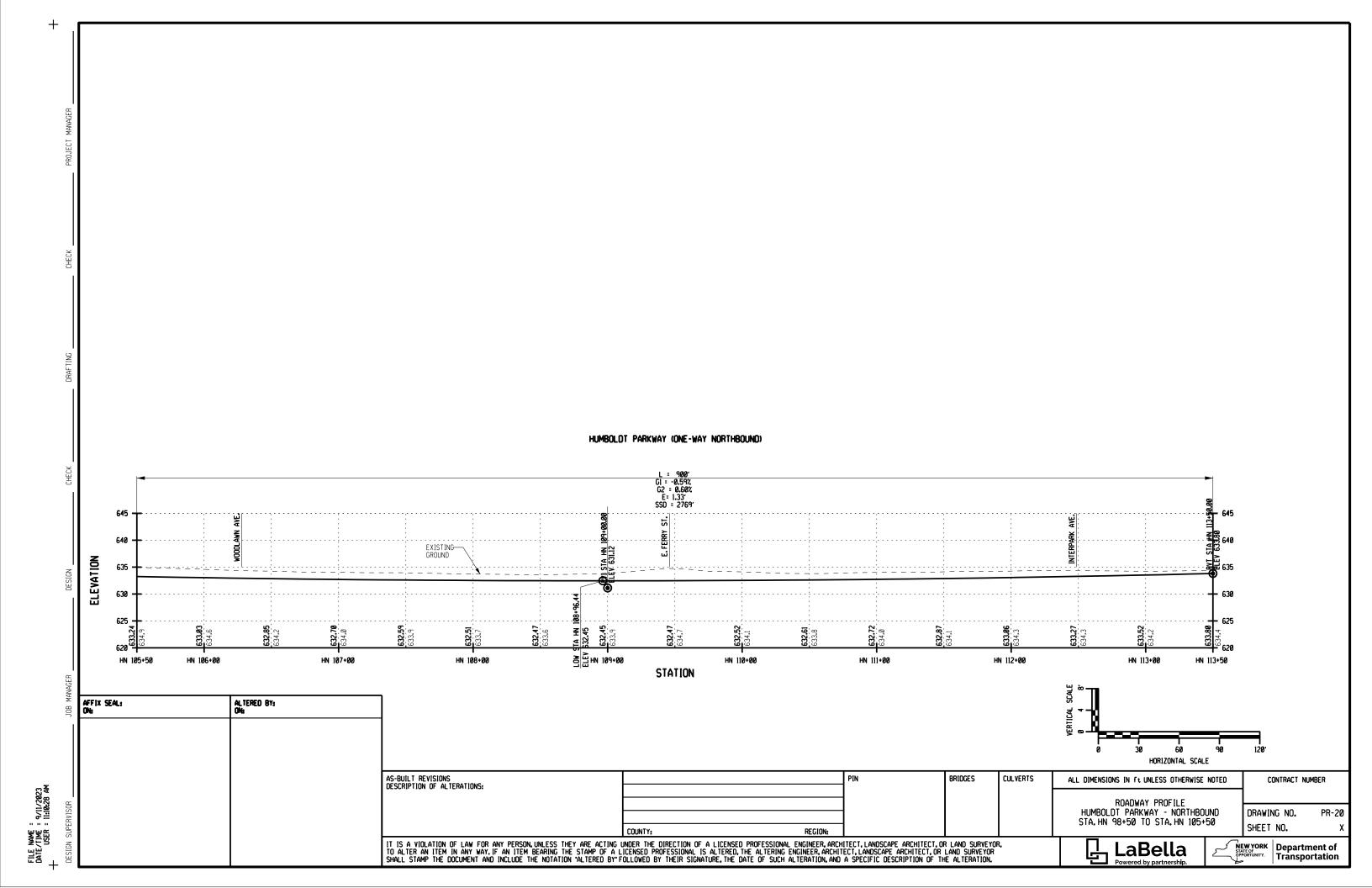


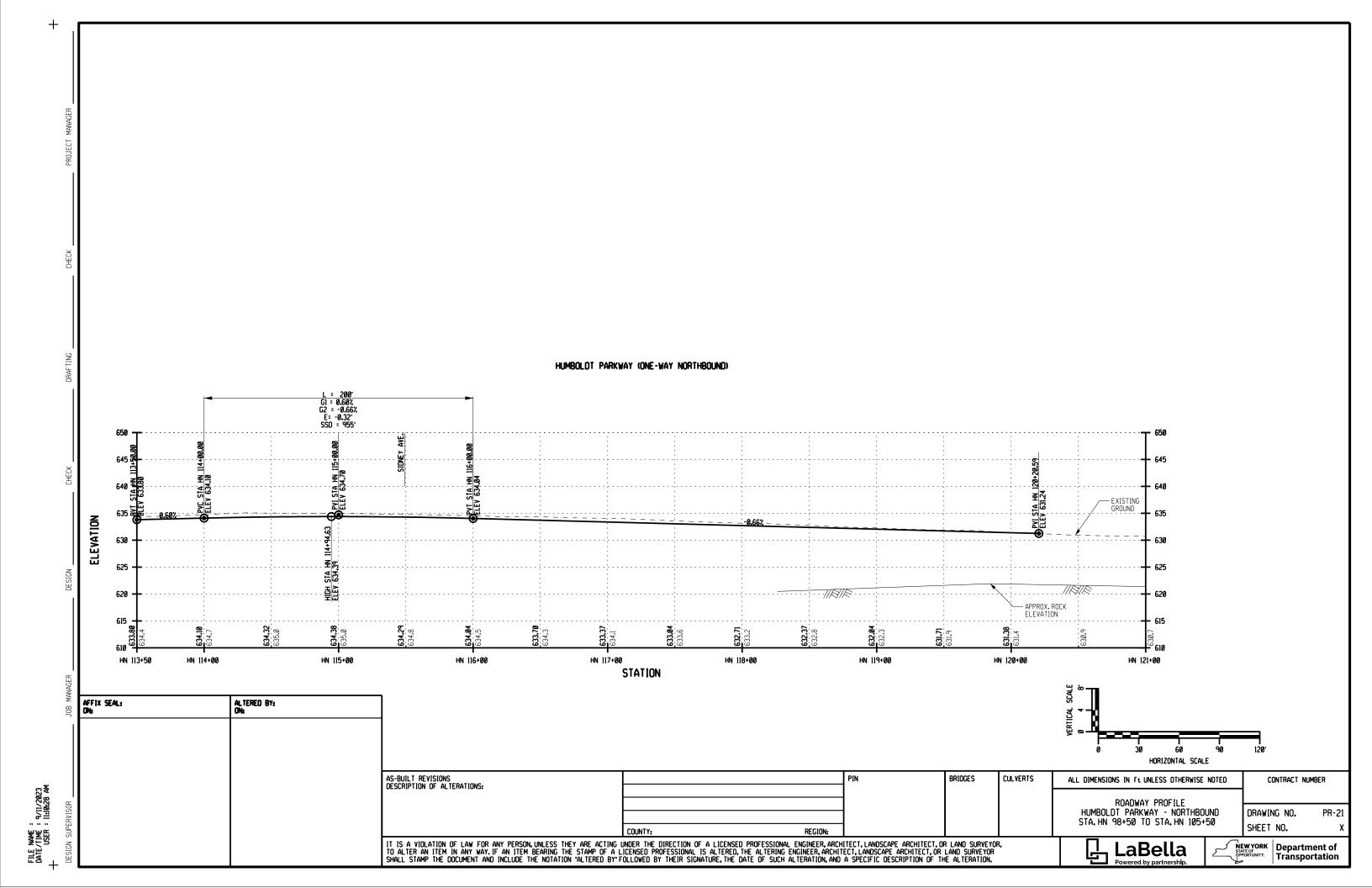


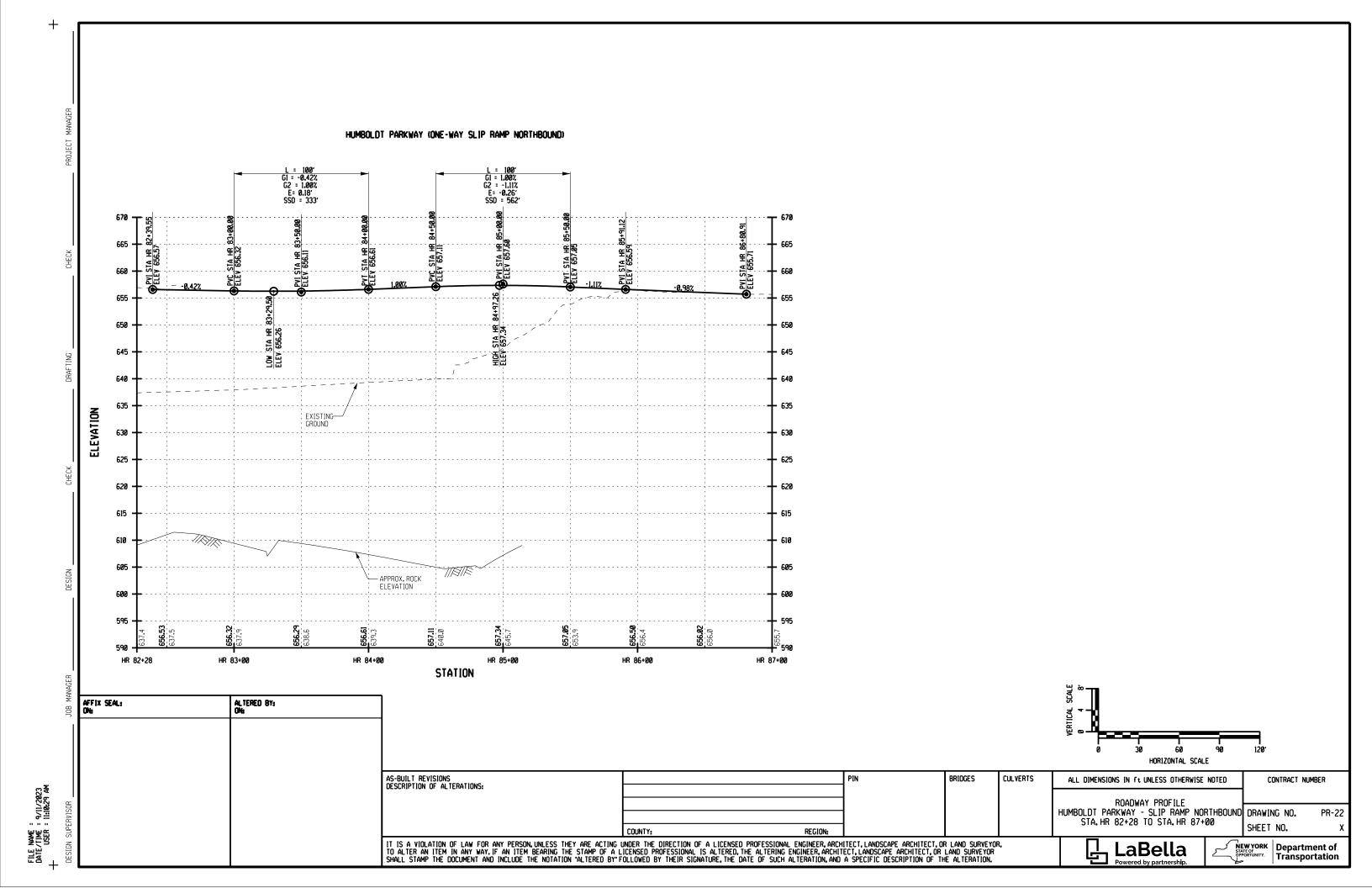


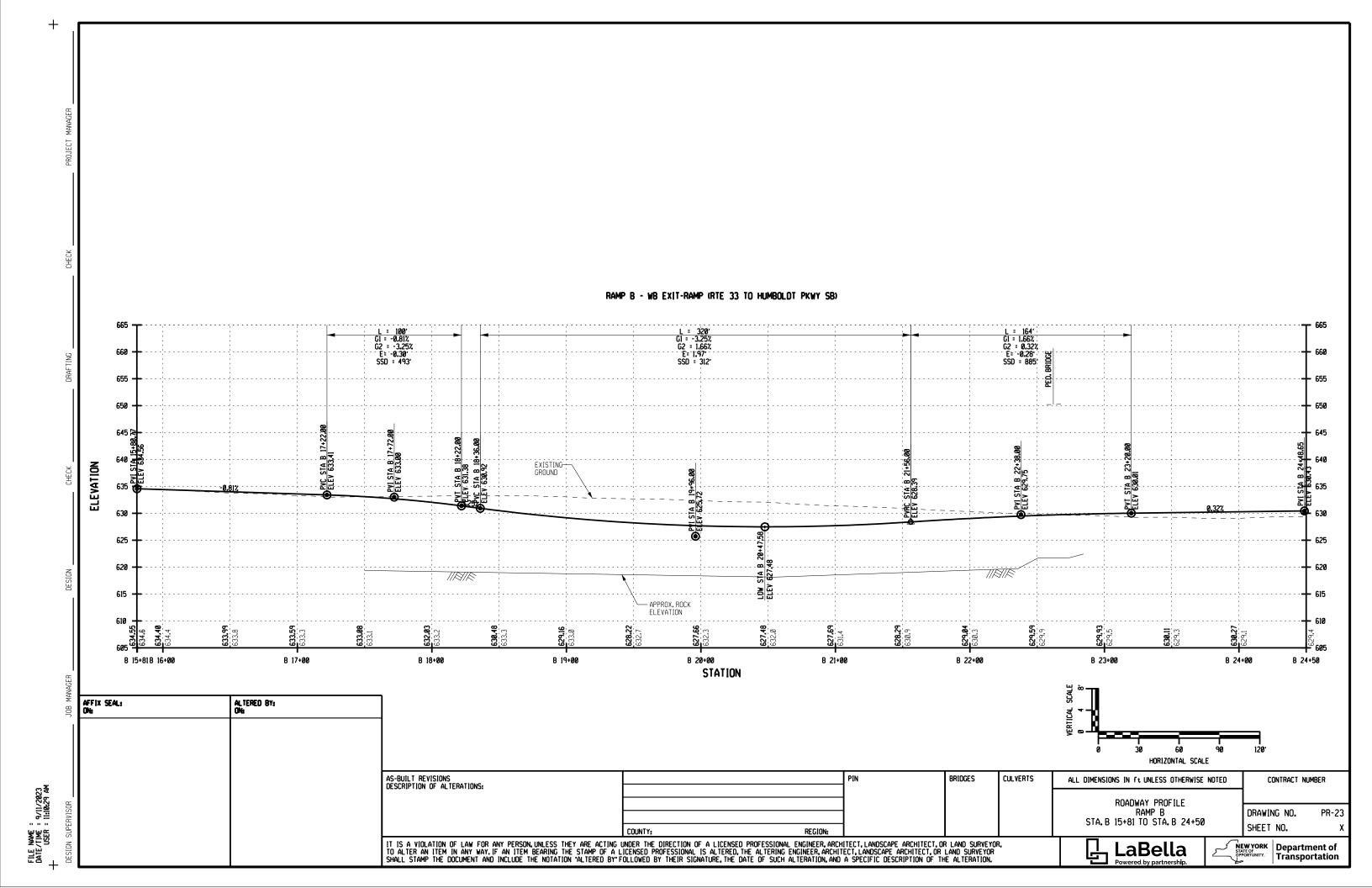


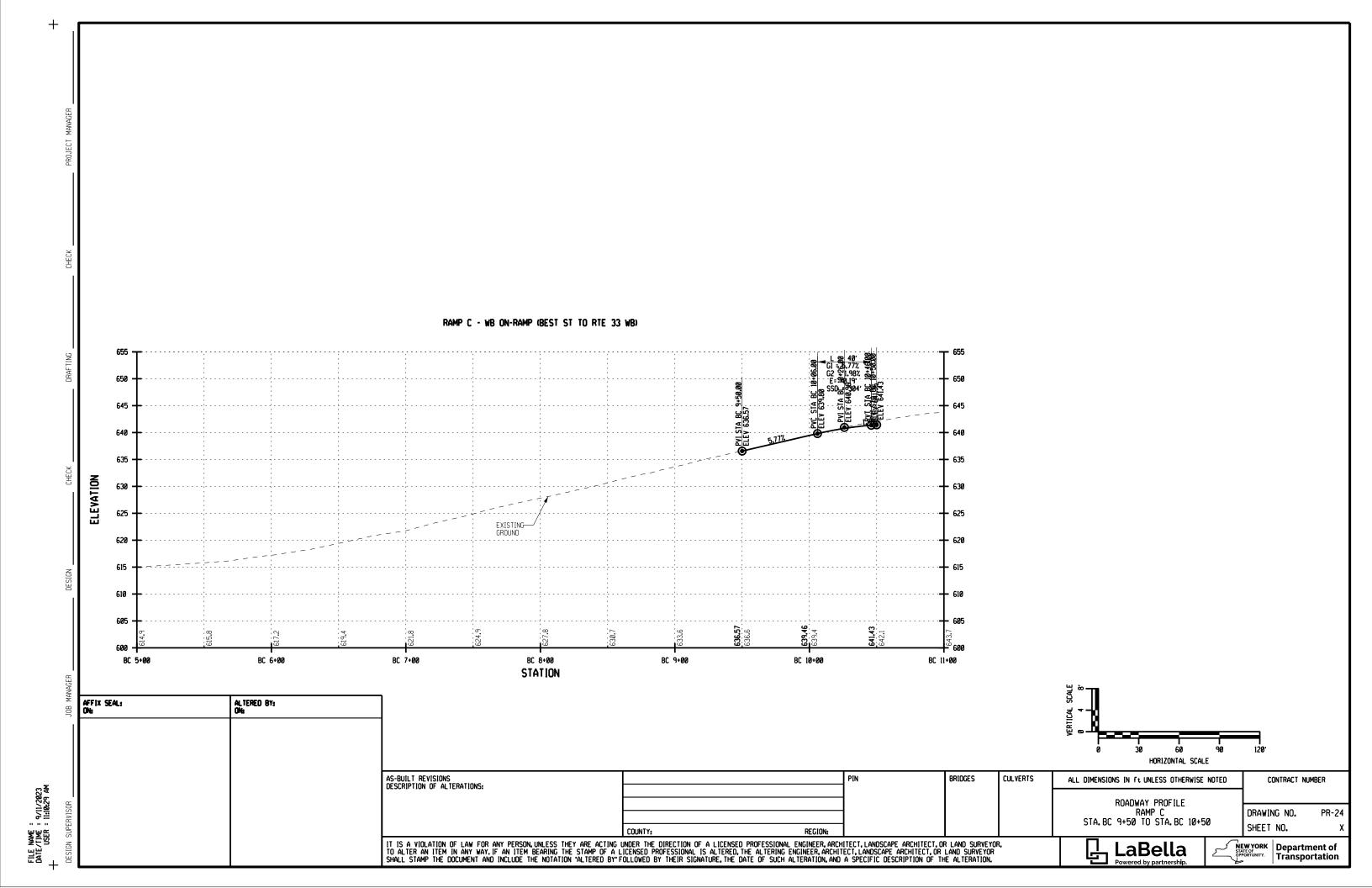


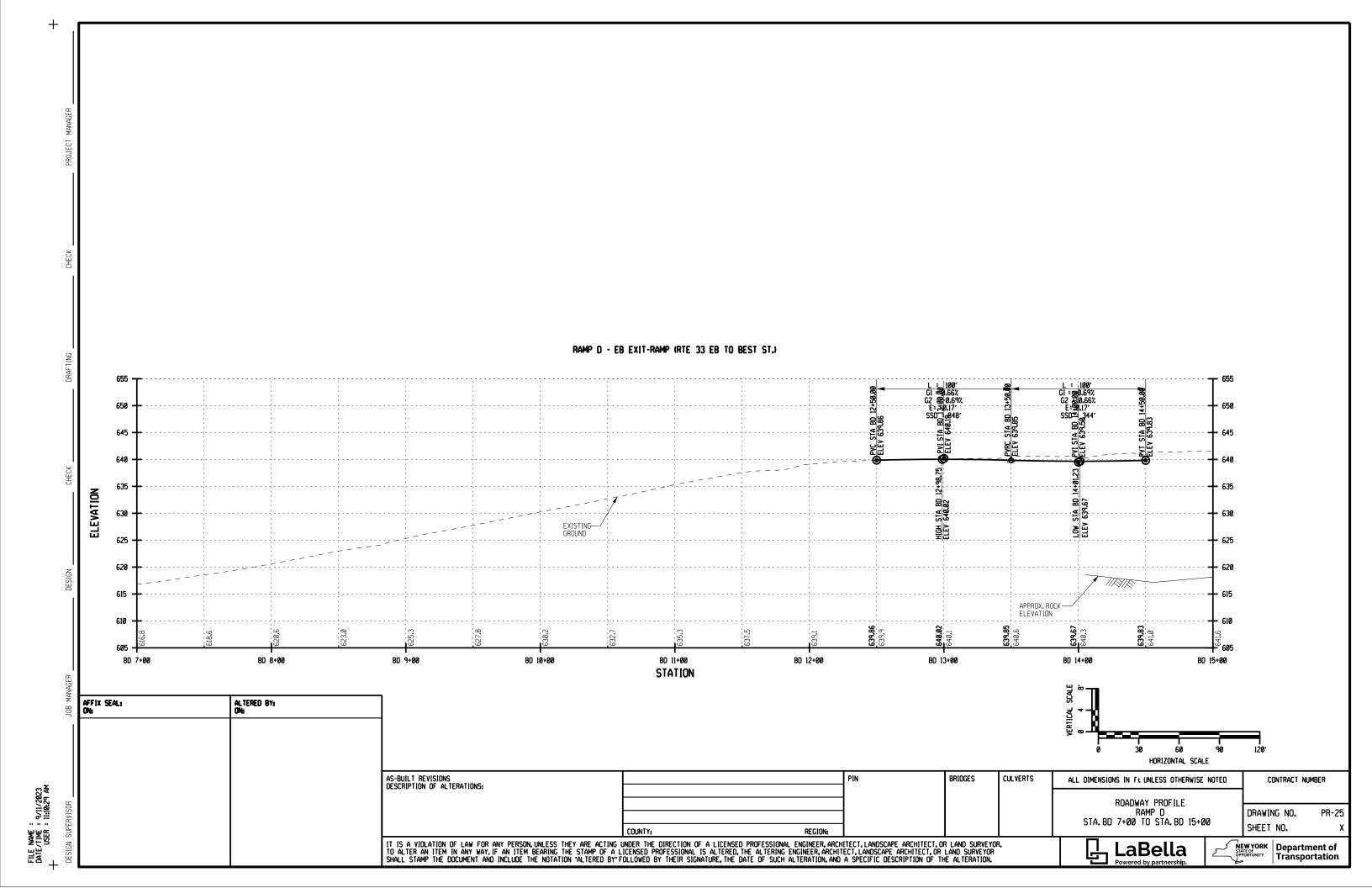


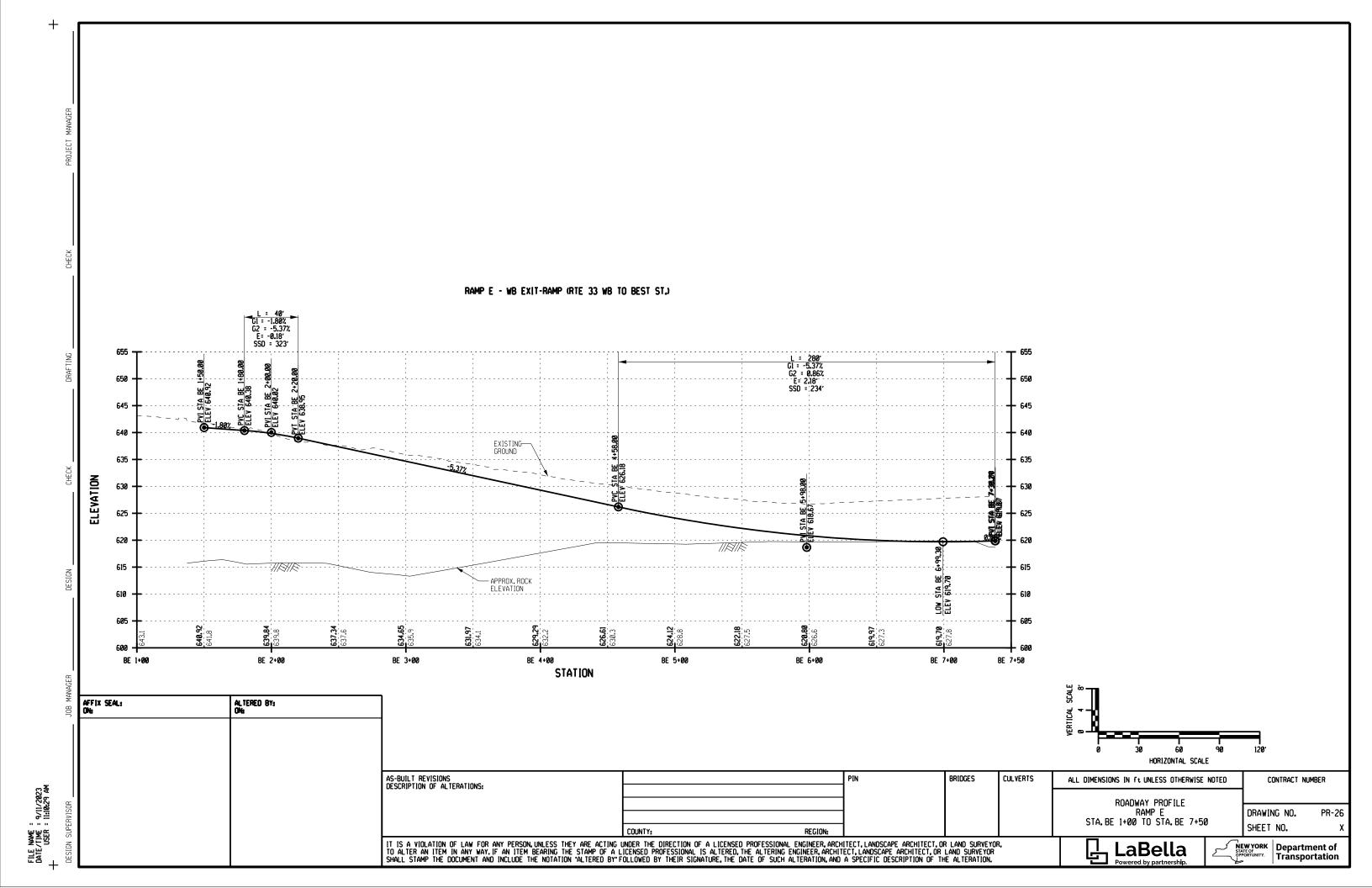


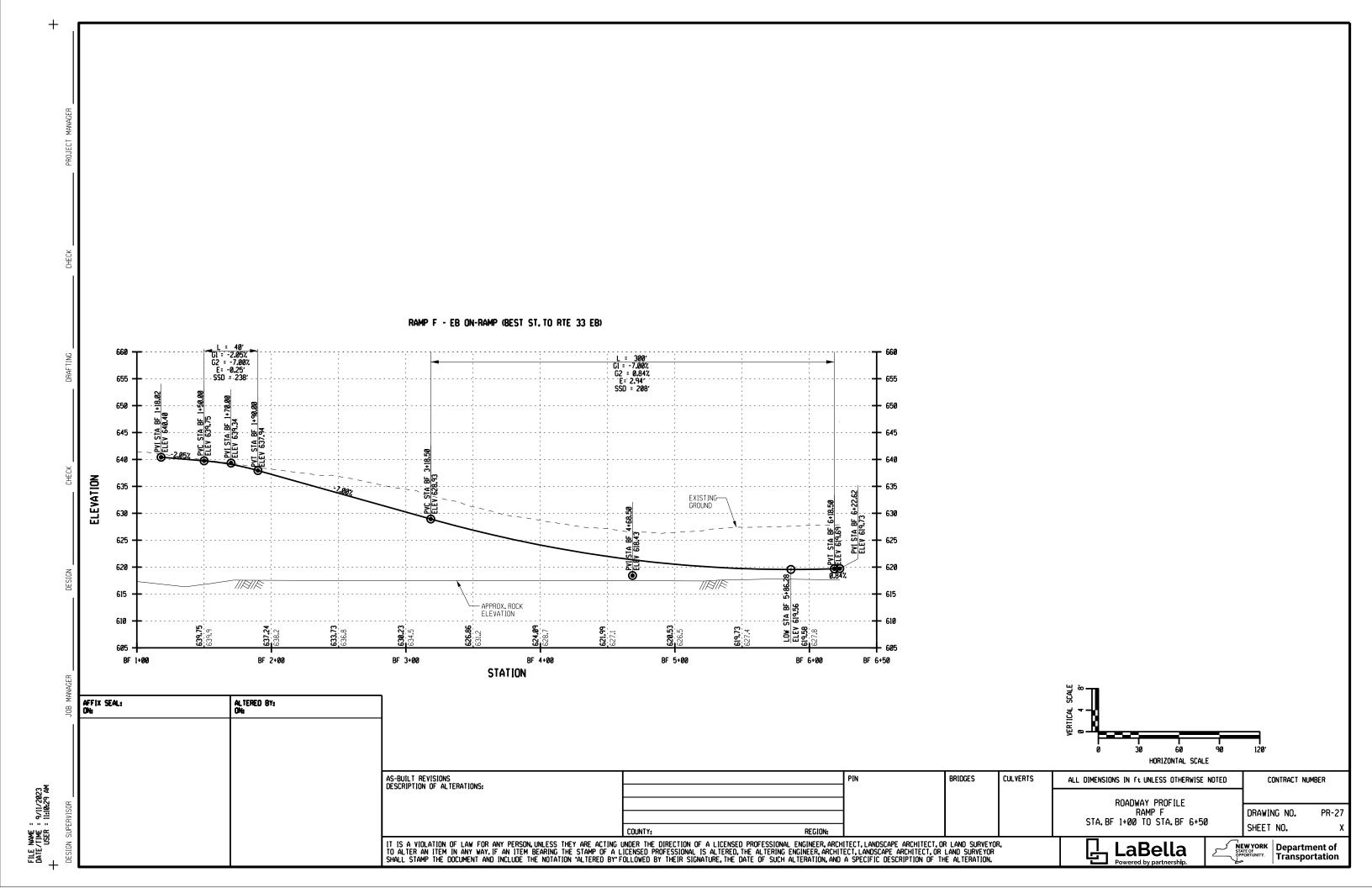




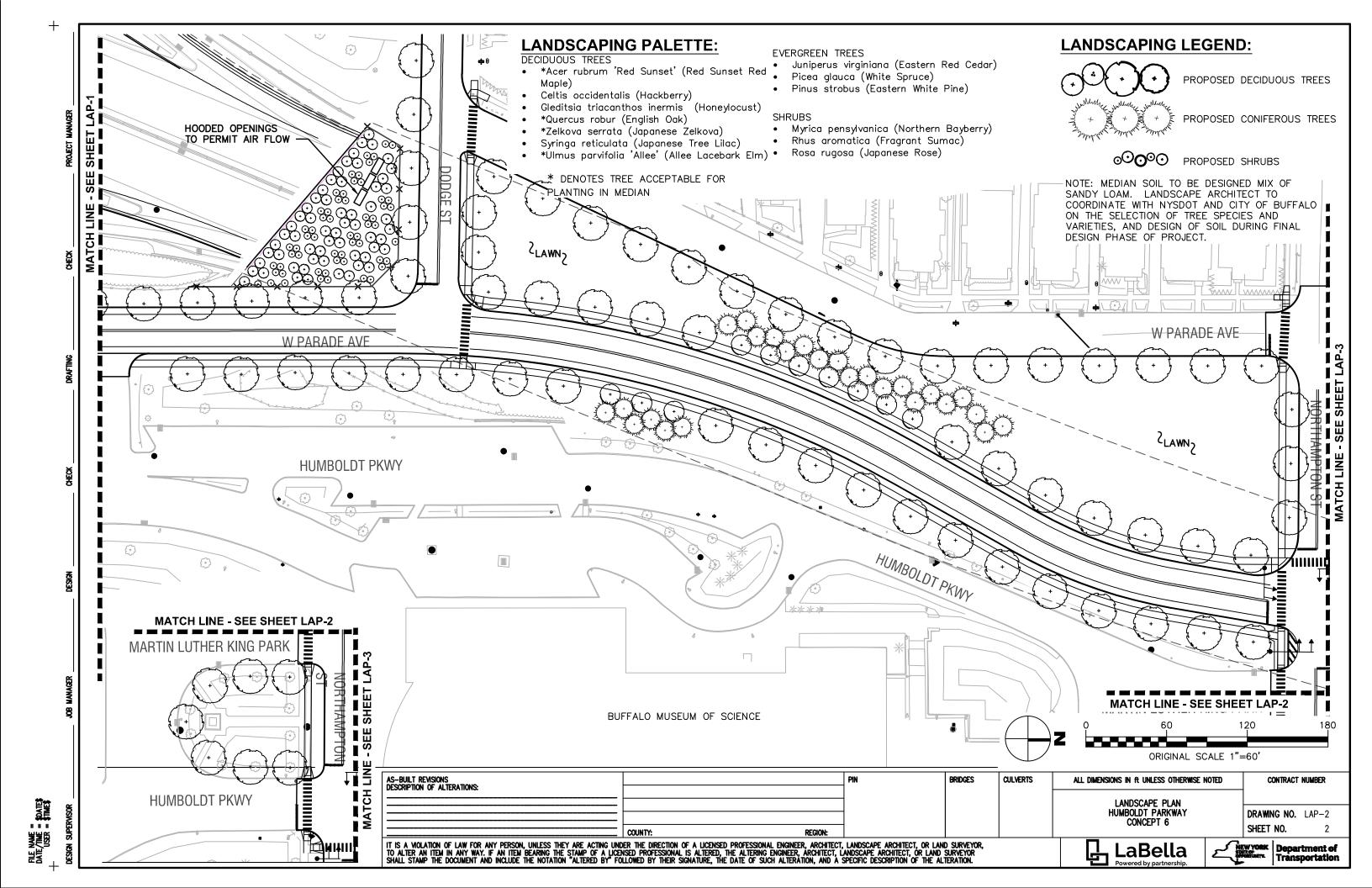


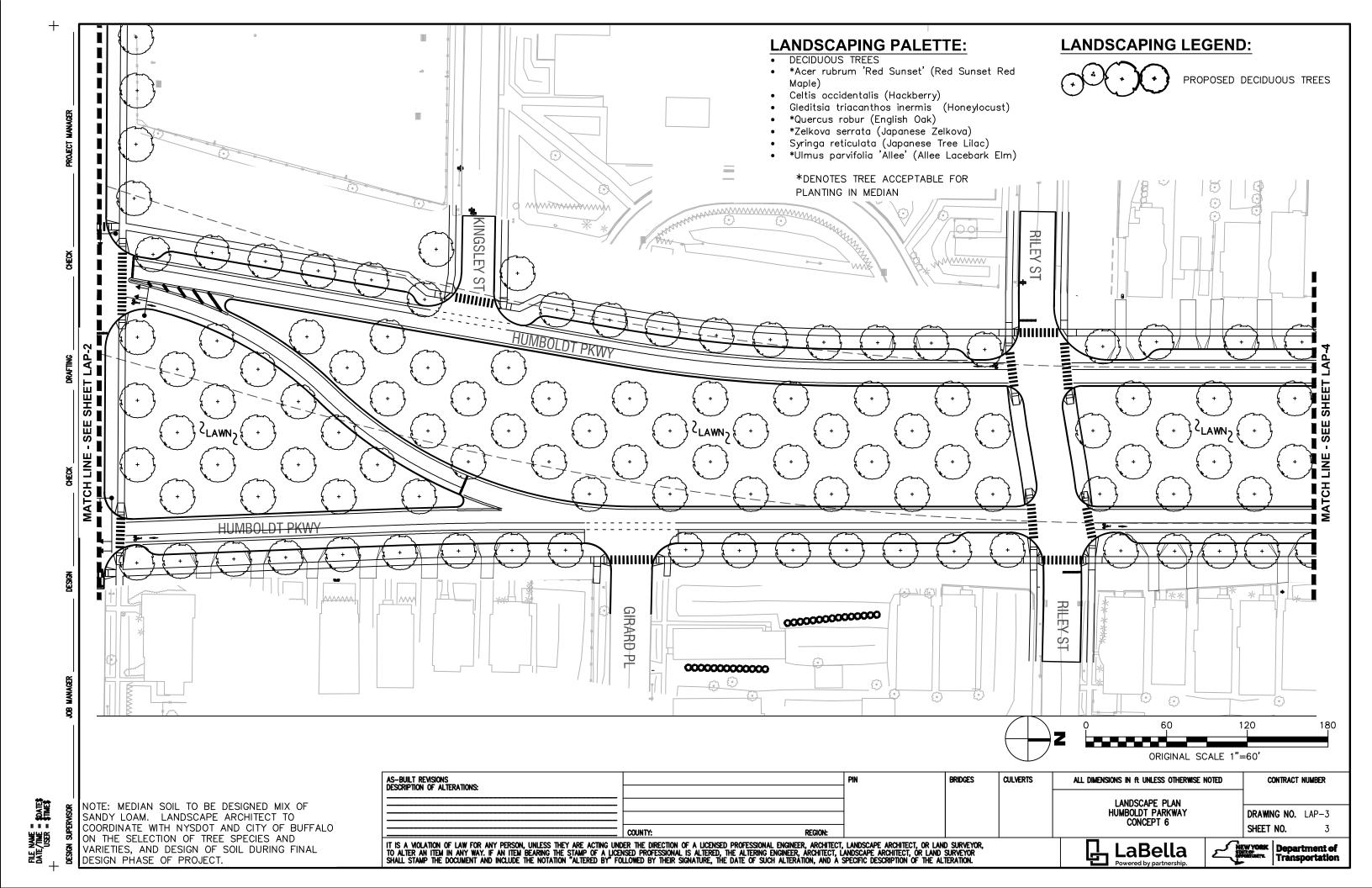


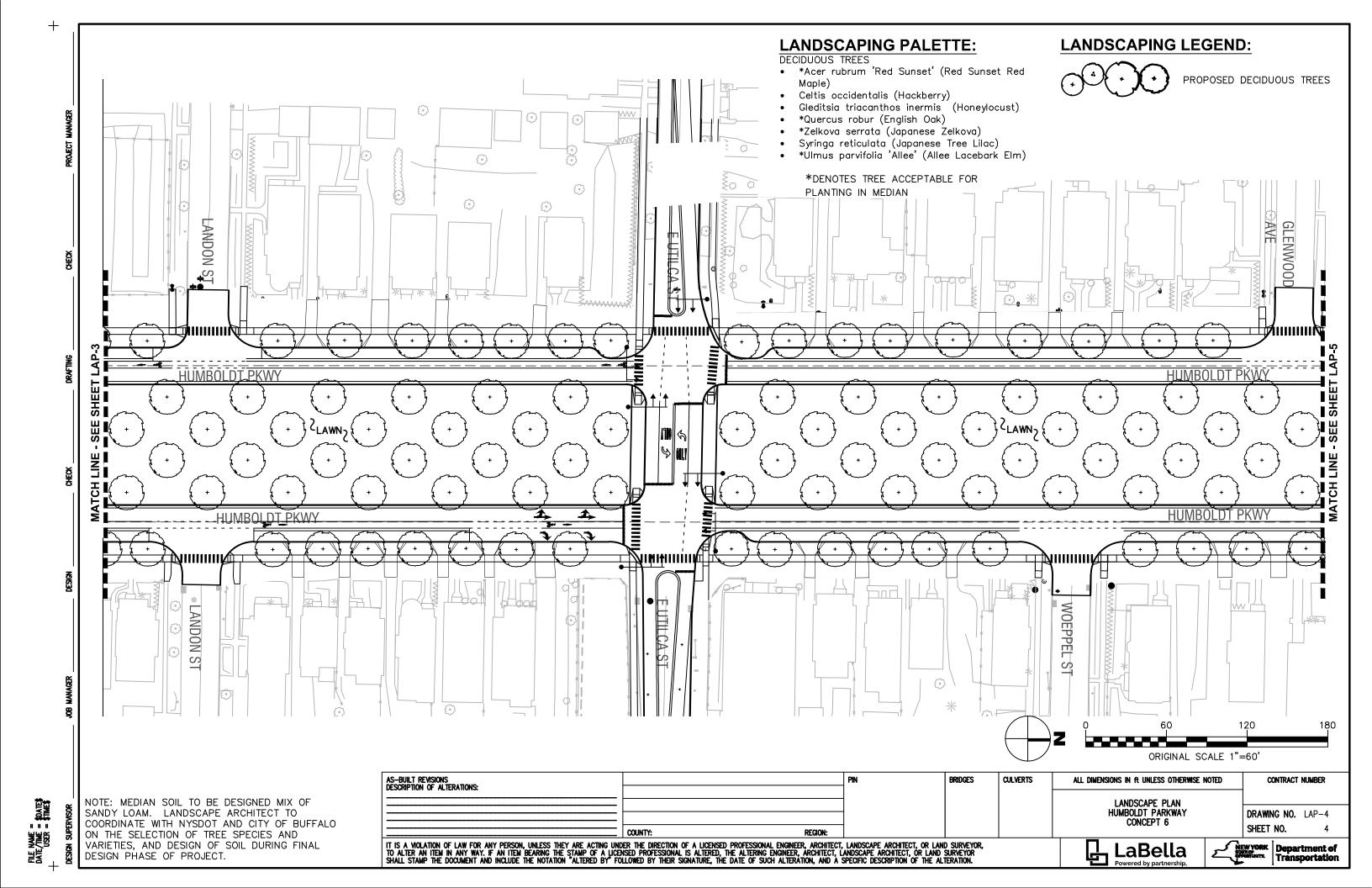


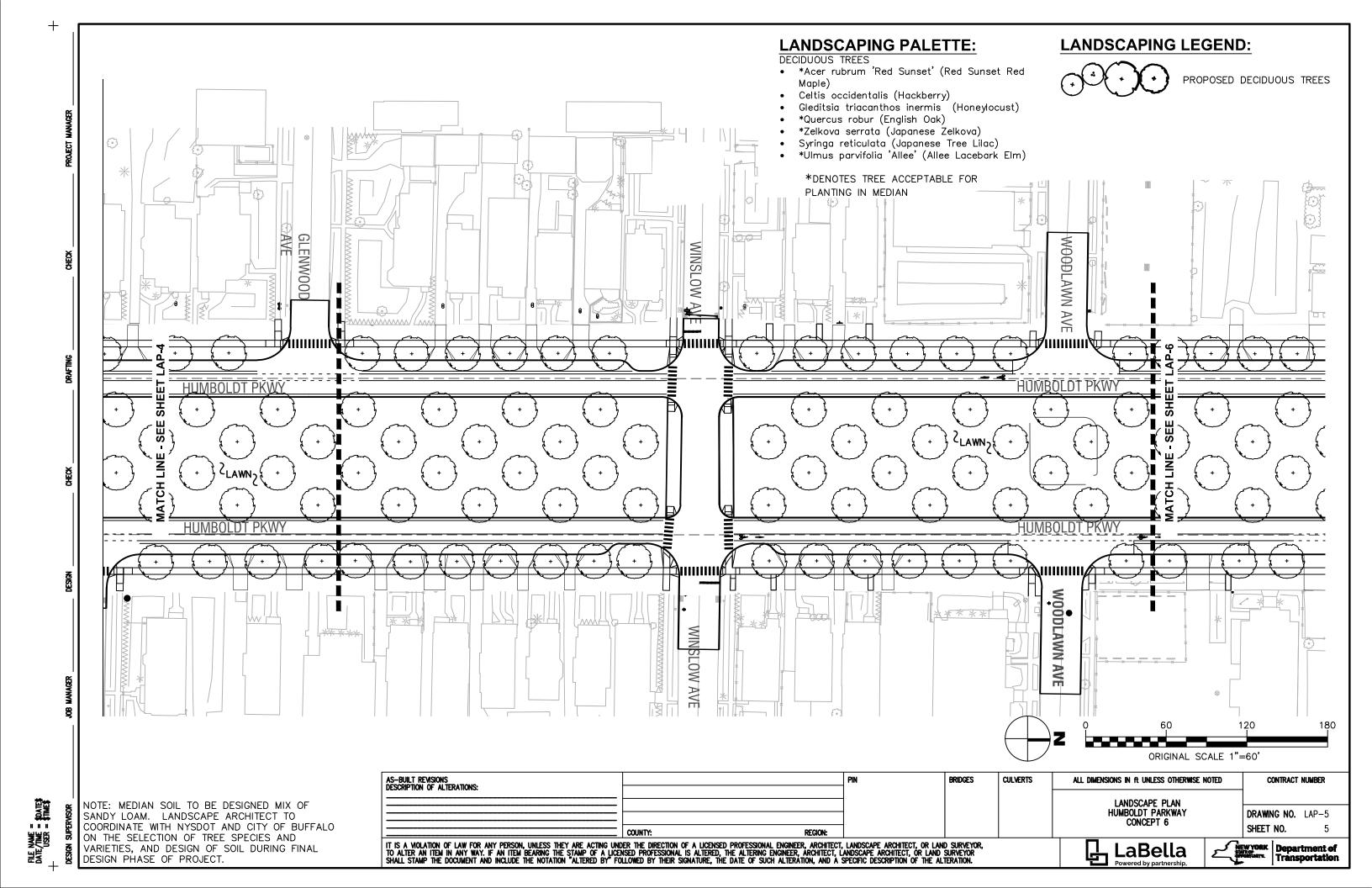


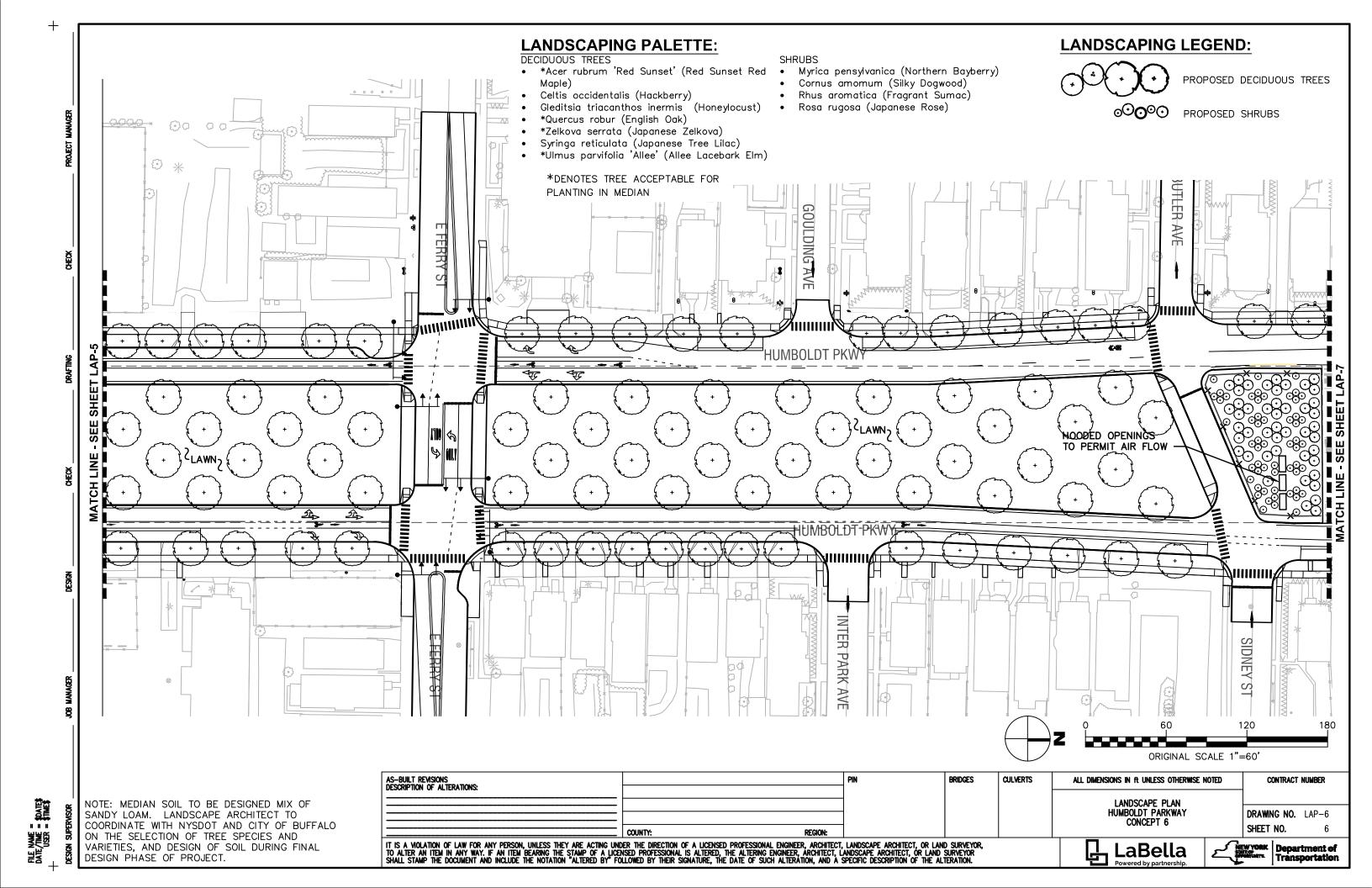
SHERMAN ST **LANDSCAPING PALETTE: LANDSCAPING LEGEND:** DECIDUOUS TREES **SHRUBS** Acer rubrum 'Red Sunset' (Red Sunset Red • Clethra alnifolia 'Hummingbird' (Hummingbird PROPOSED DECIDUOUS TREES Maple) Summersweet) • Cornus amomum (Silky Dogwood) Celtis occidentalis (Hackberry) Gleditsia triacanthos inermis (Honeylocust) • Rhus aromatica (Fragrant Sumac) **⊙⊙**⊙⊙ Quercus robur (English Oak) • Rosa rugosa (Japanese Rose) PROPOSED SHRUBS Zelkova serrata (Japanese Zelkova) Syringa reticulata (Japanese Tree Lilac) Ulmus parvifolia 'Allee' (Allee Lacebark Elm) **MATCH LINE - SEE SHEET LAP-1** ORIGINAL SCALE 1"=60' **MATCH LINE - SEE SHEET LAP-1** AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: BRIDGES CULVERTS ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER FILE NAME = SDATE\$
DATE/TIME = \$DATE\$
USER = \$TIME\$ NOTE: MEDIAN SOIL TO BE DESIGNED MIX OF SANDY LOAM. LANDSCAPE ARCHITECT TO LANDSCAPE PLAN HUMBOLDT PARKWAY CONCEPT 6 DRAWING NO. LAP-1 COORDINATE WITH NYSDOT AND CITY OF BUFFALO ON THE SELECTION OF TREE SPECIES AND SHEET NO. COUNTY: REGION: IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. VARIETIES, AND DESIGN OF SOIL DURING FINAL NEW YORK Department of Transportation LaBella DESIGN PHASE OF PROJECT.

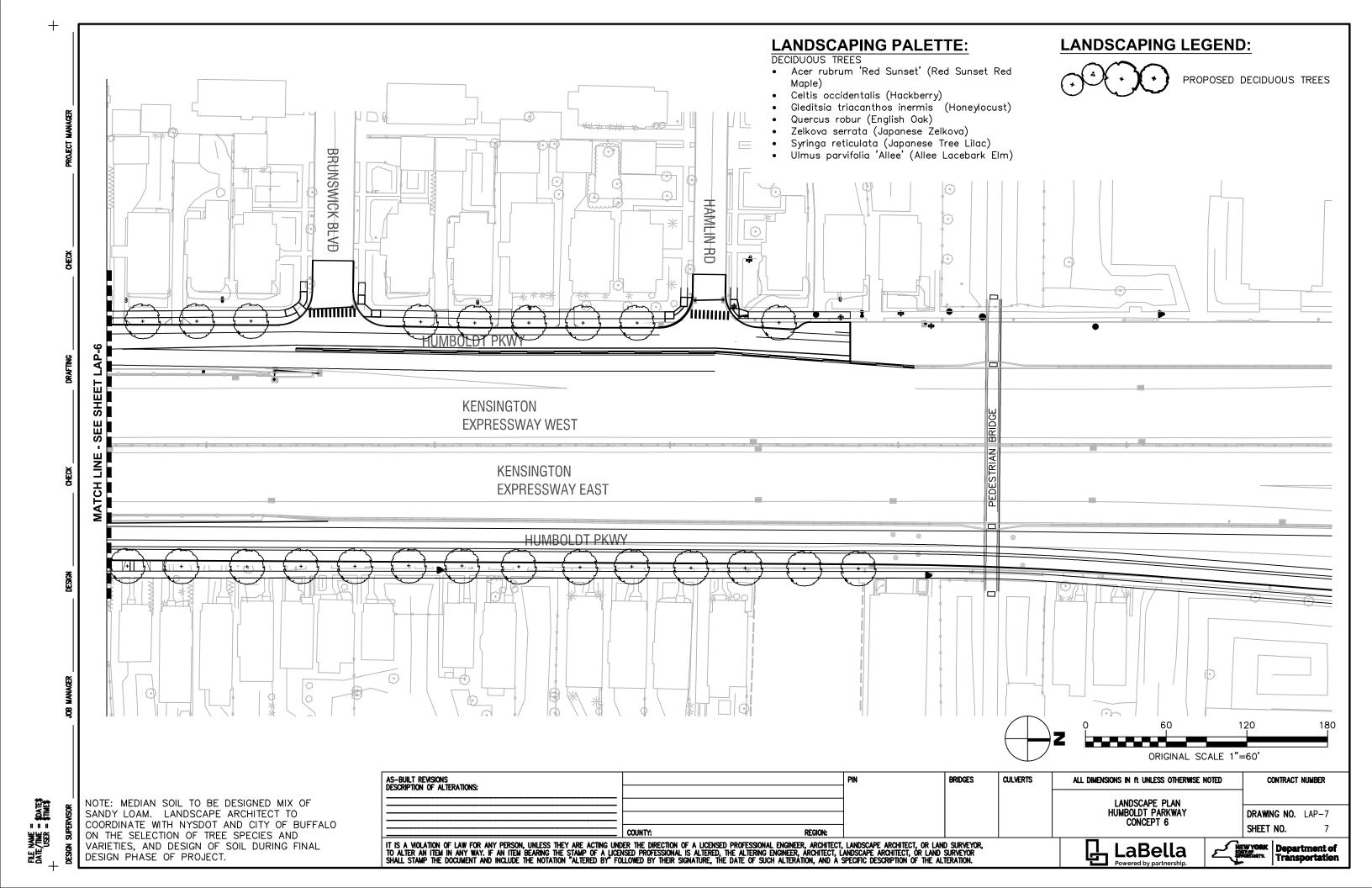


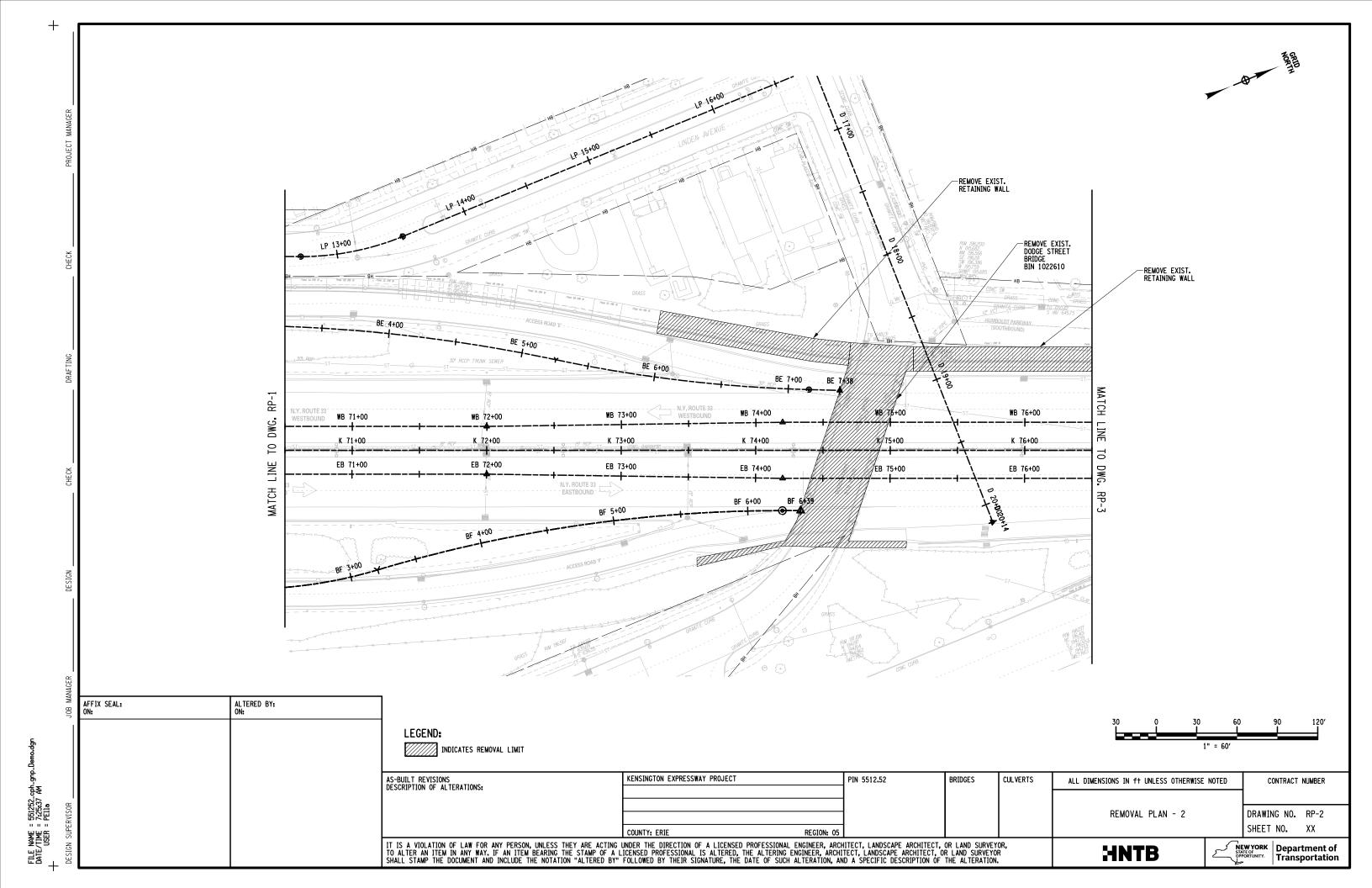


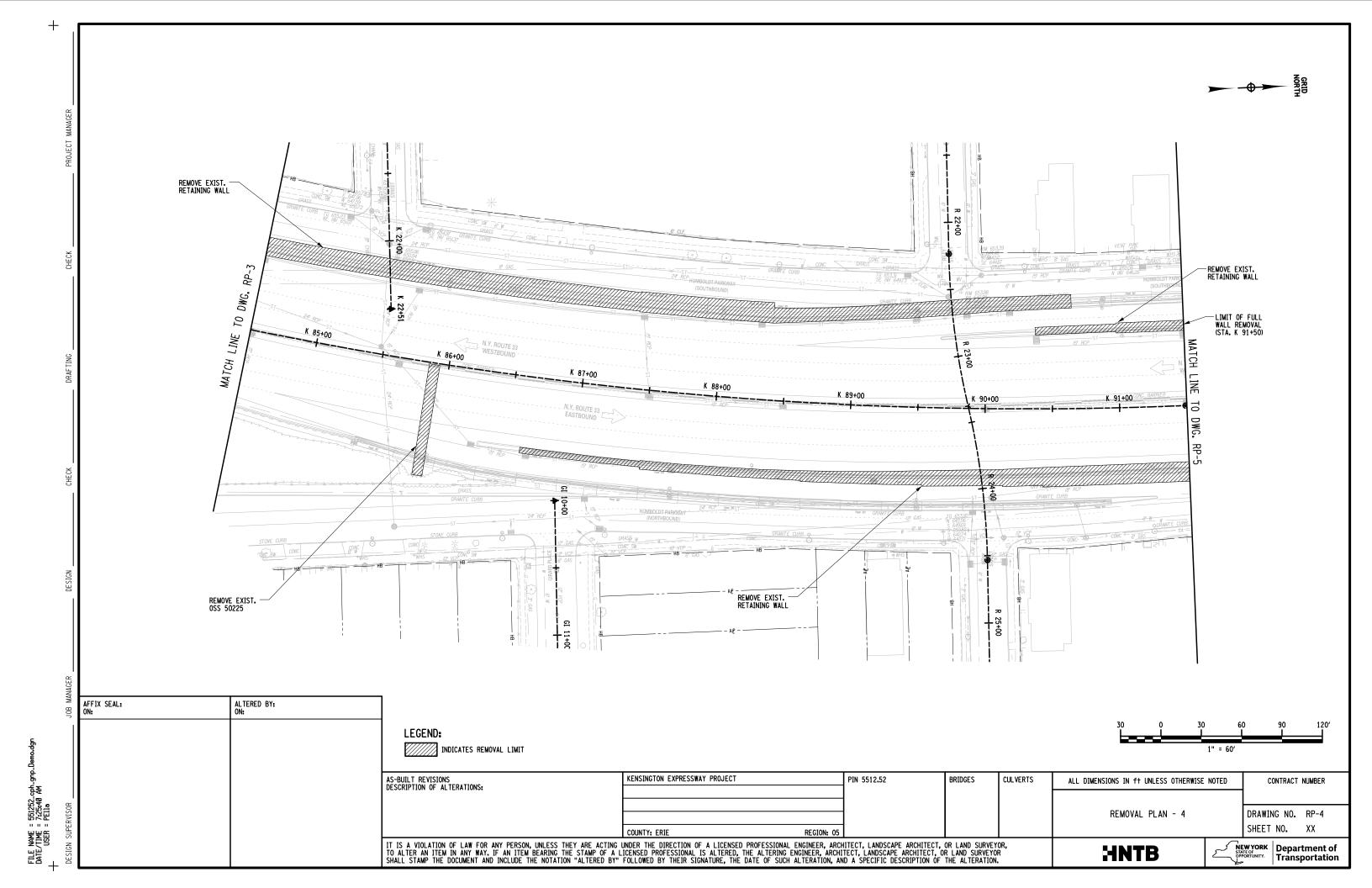


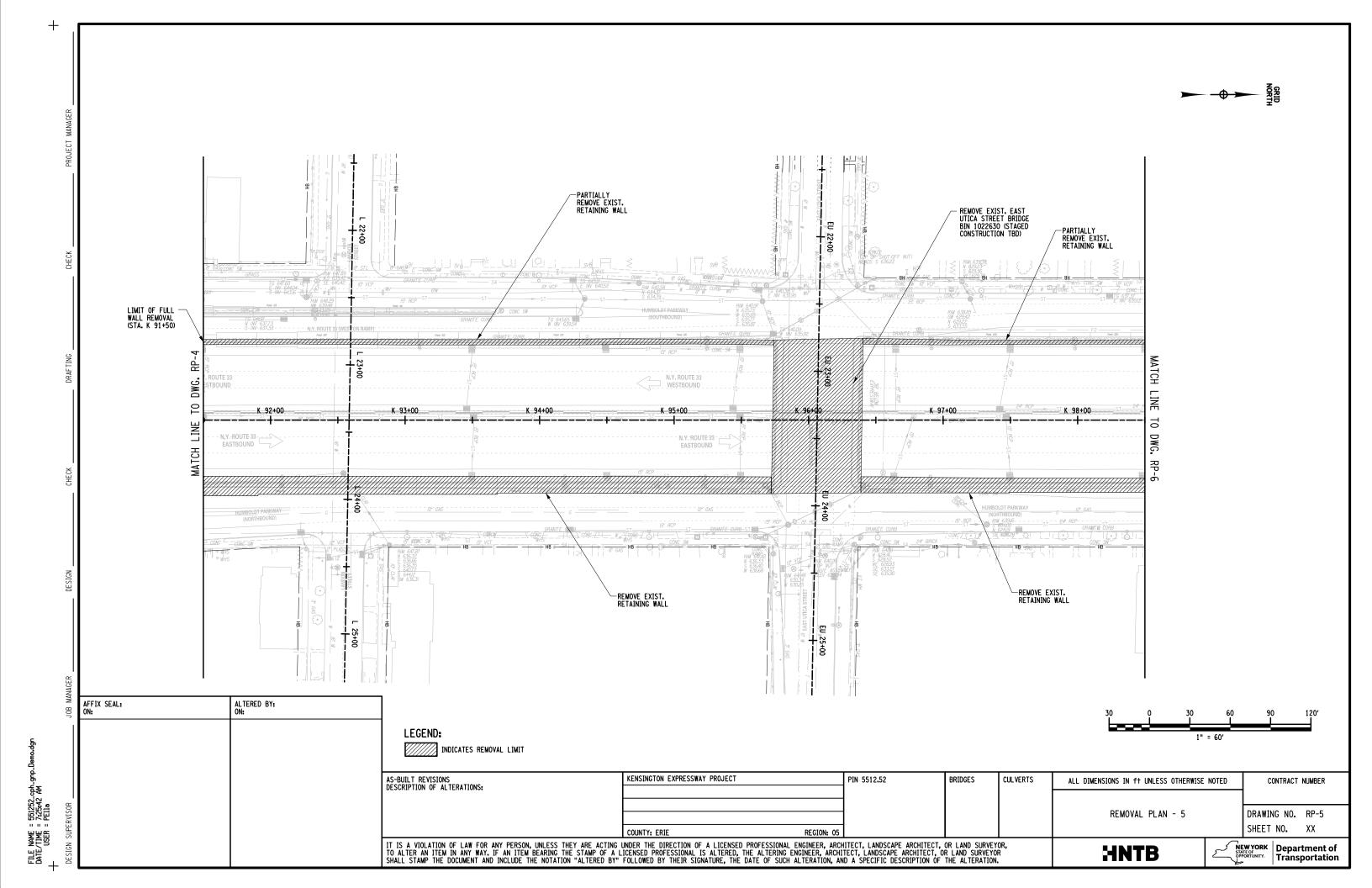


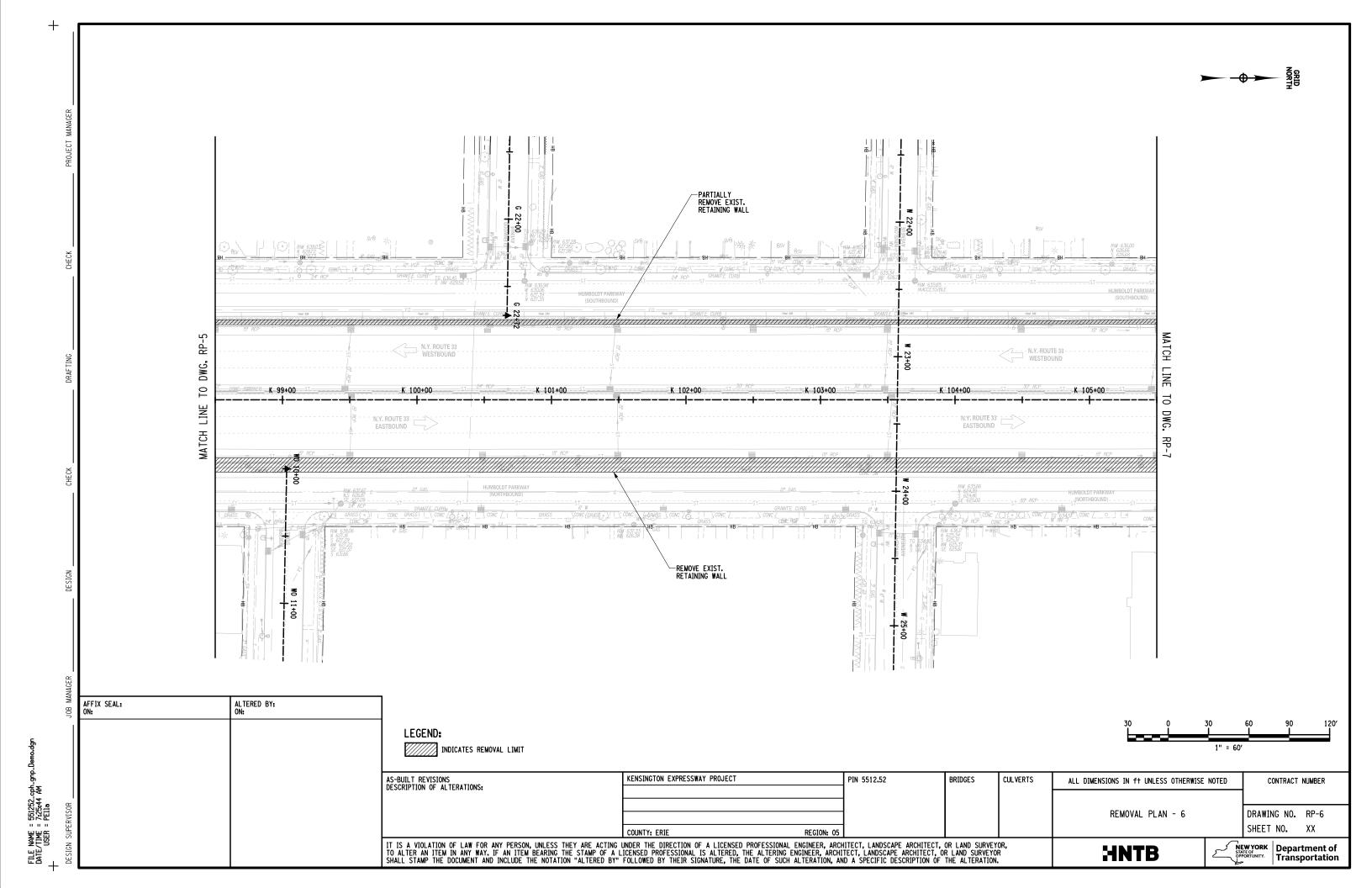


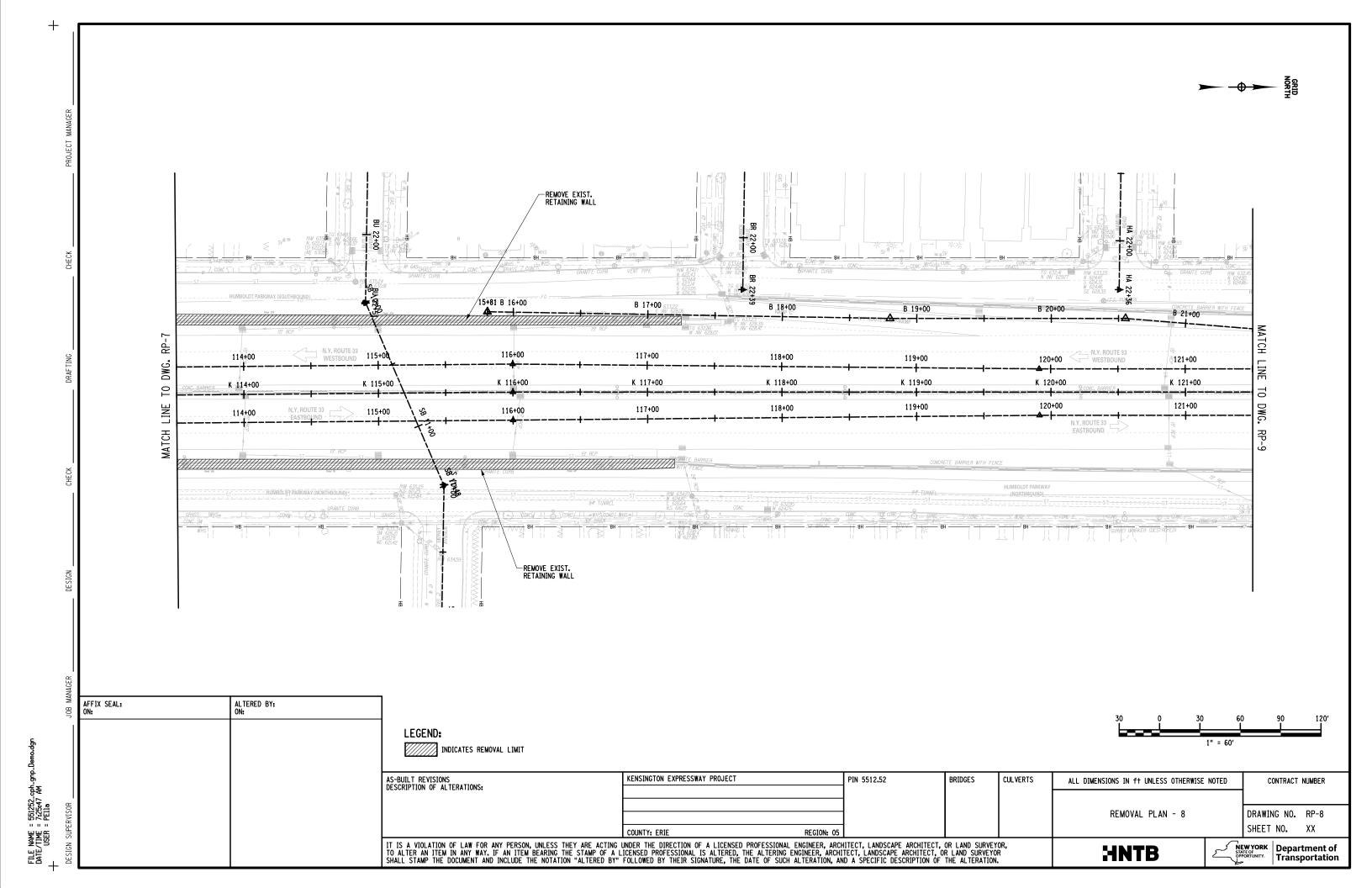


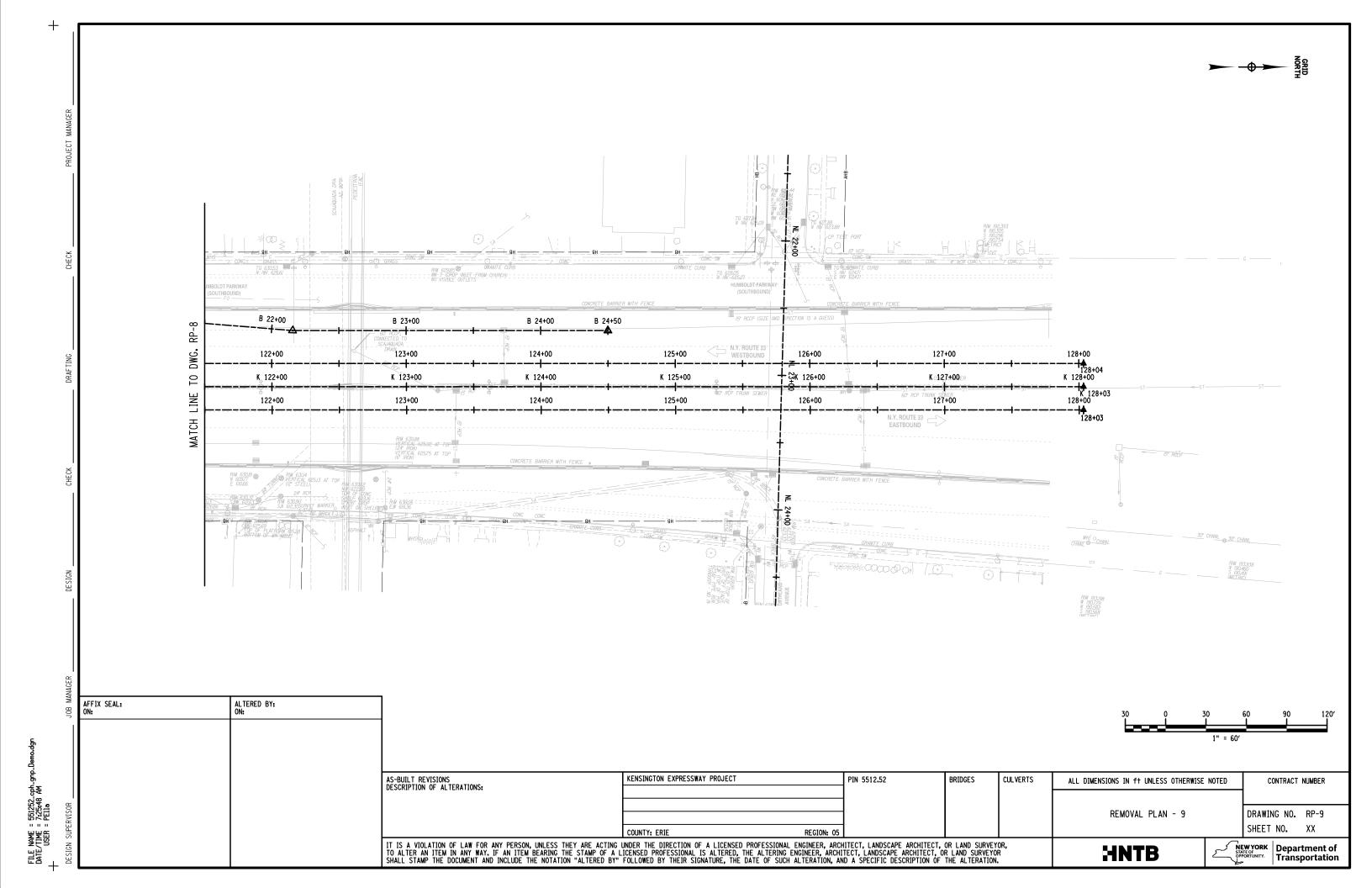


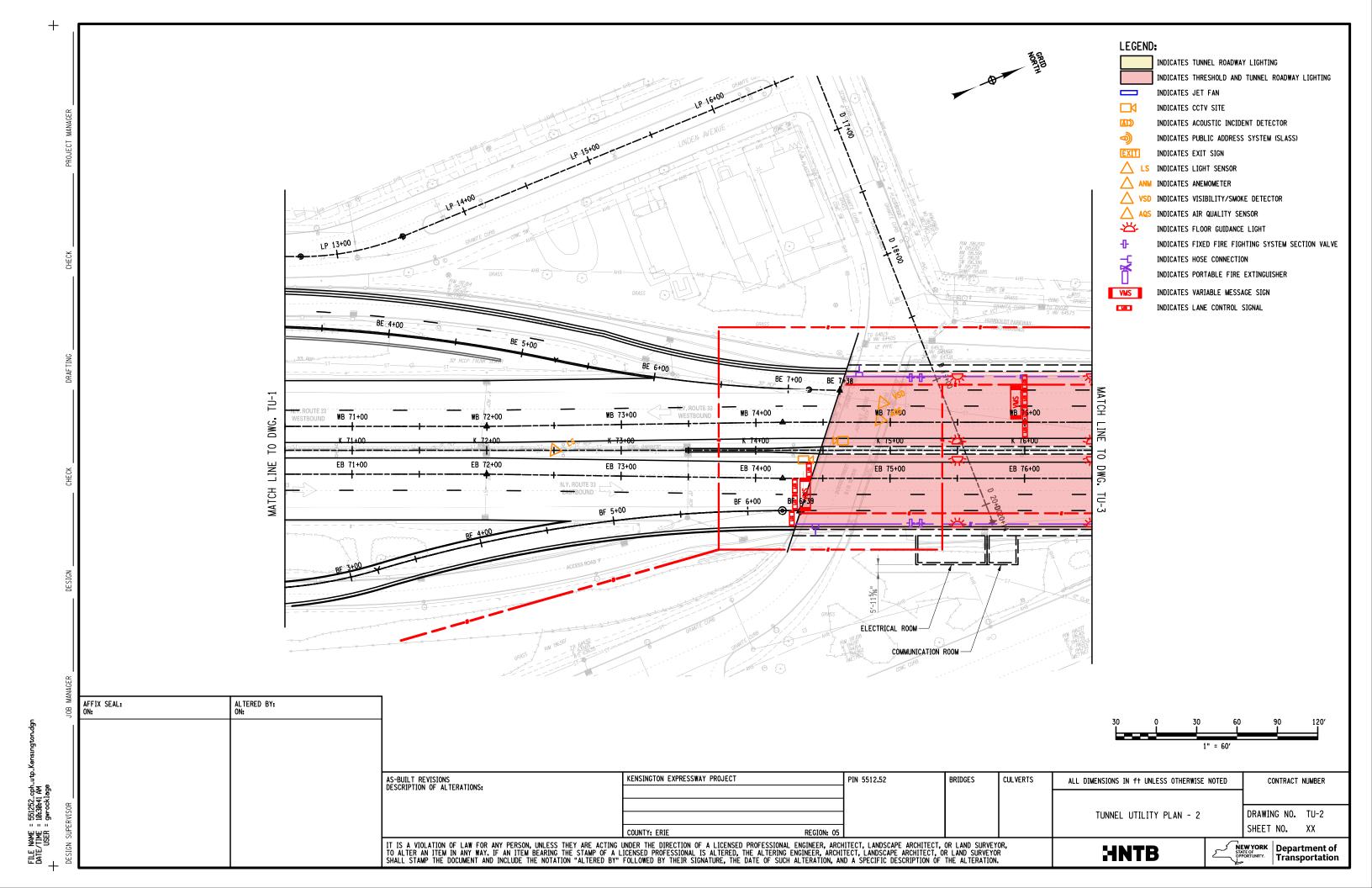


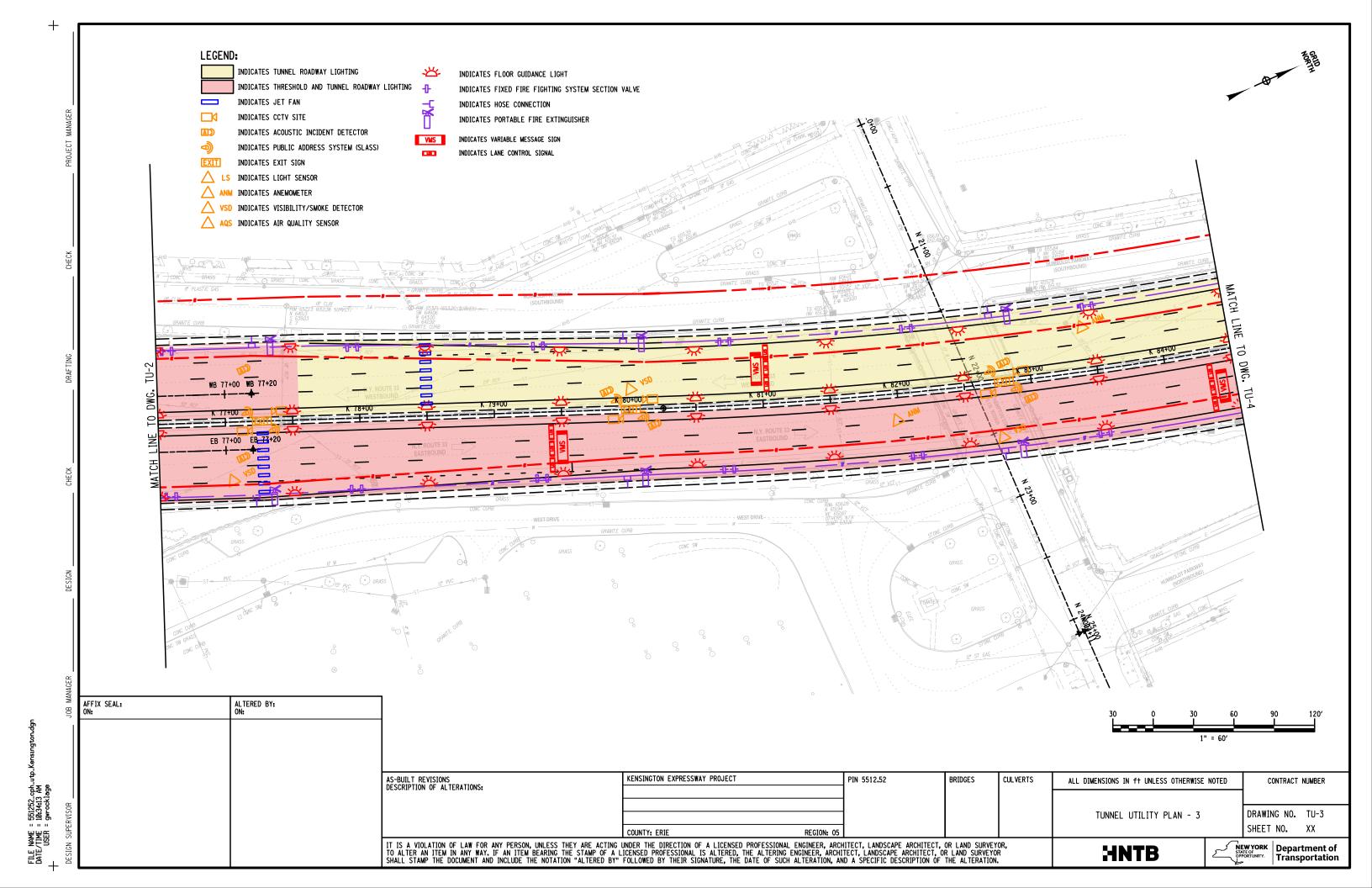


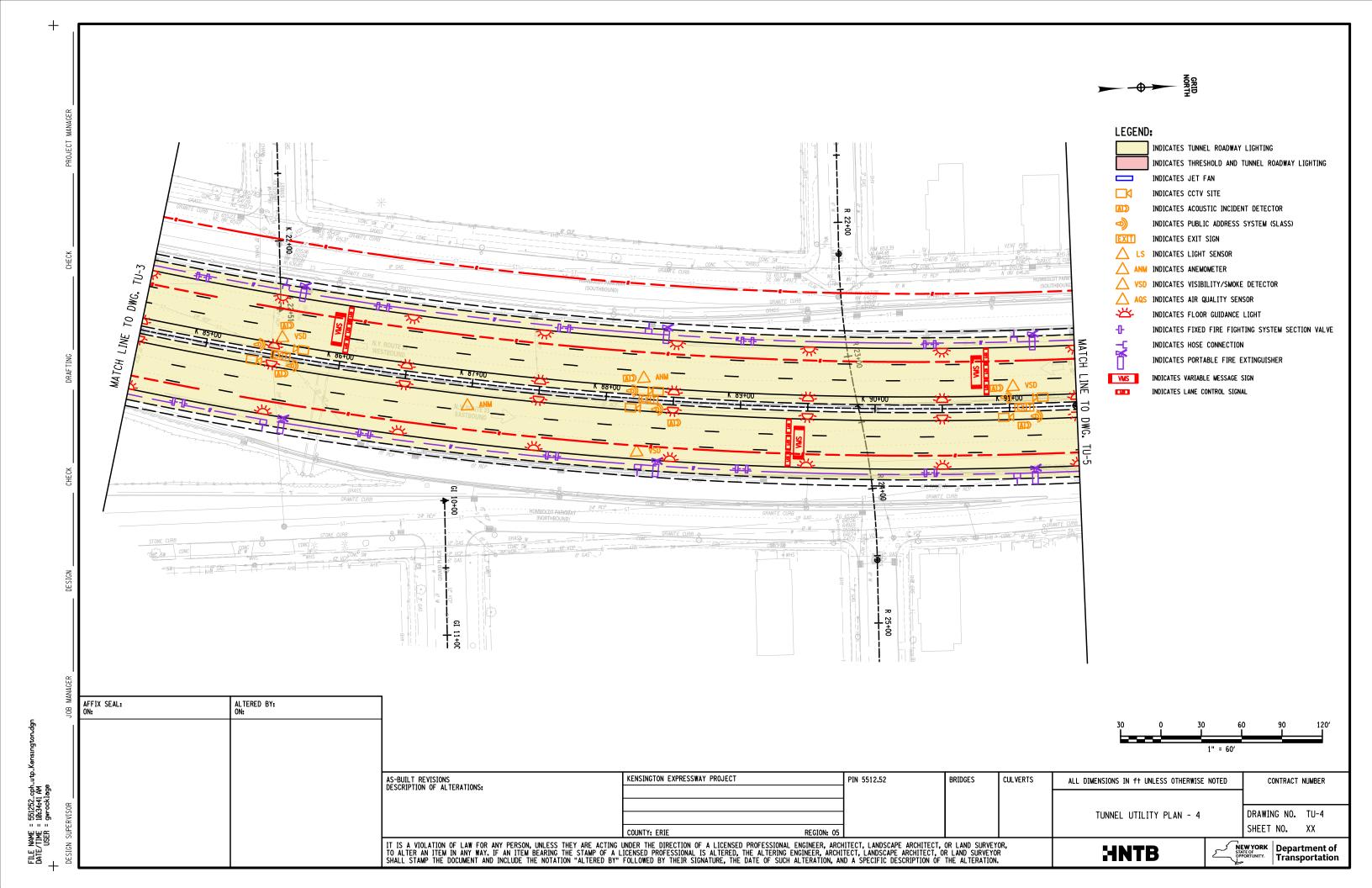


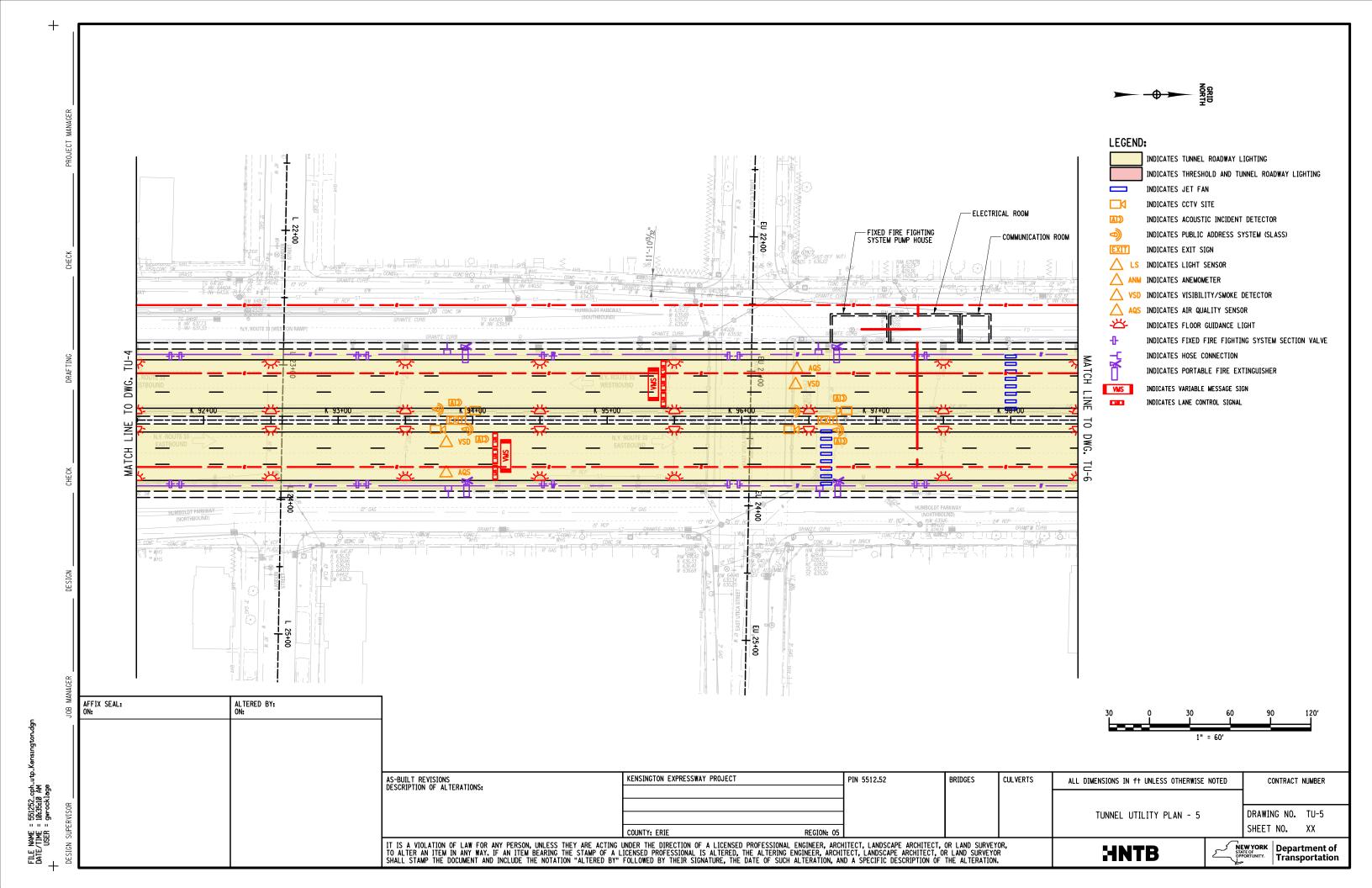


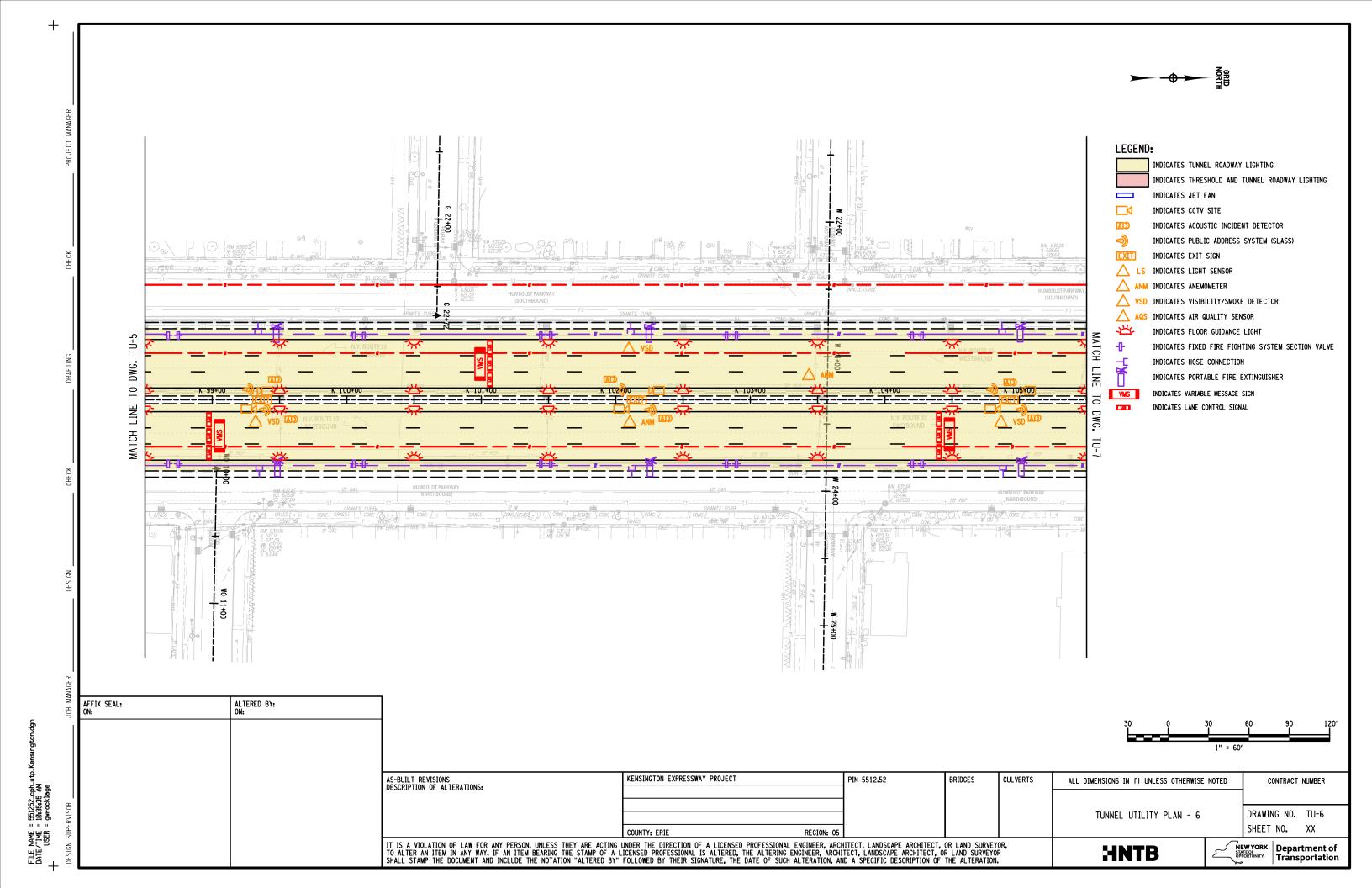


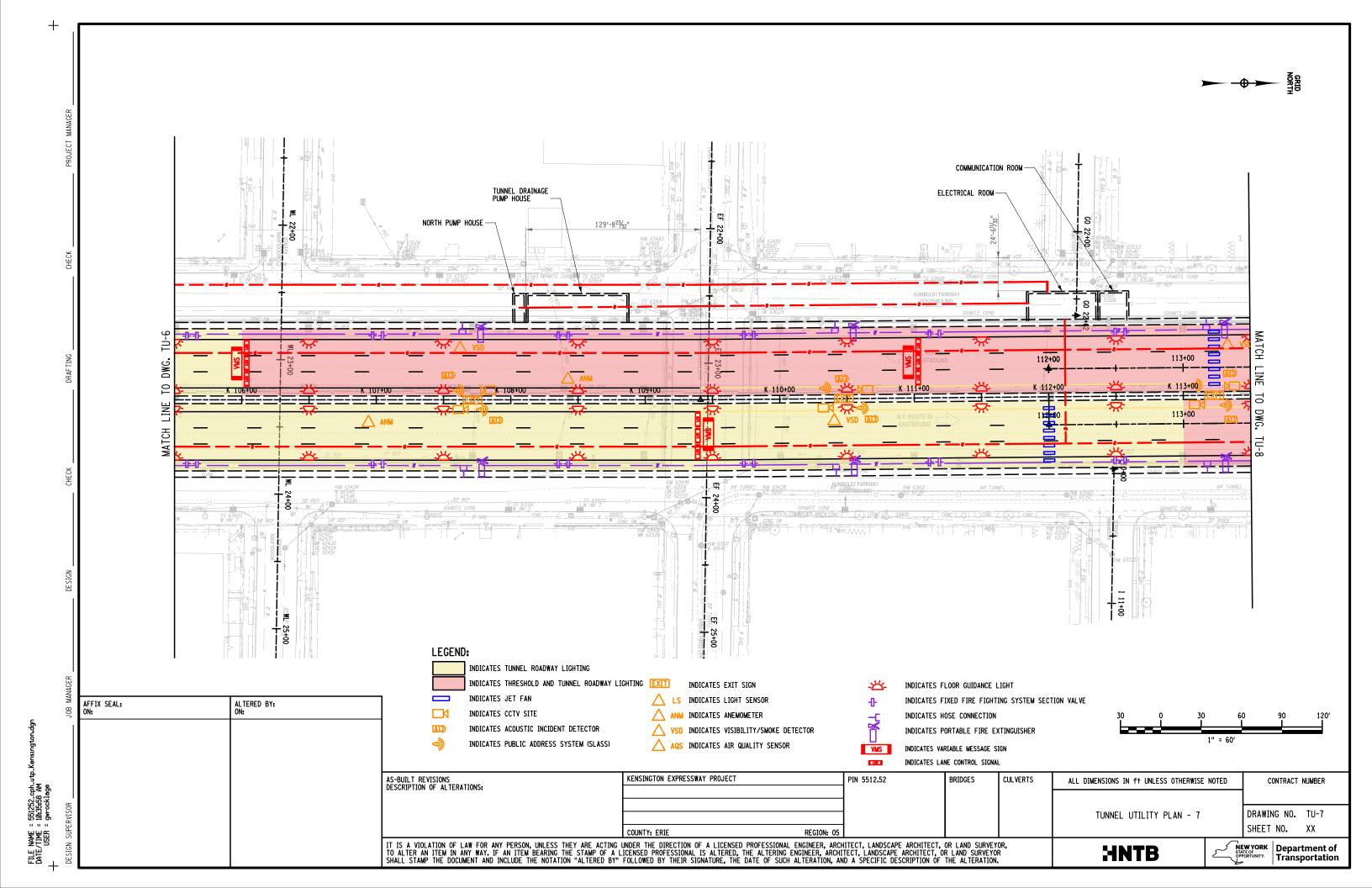


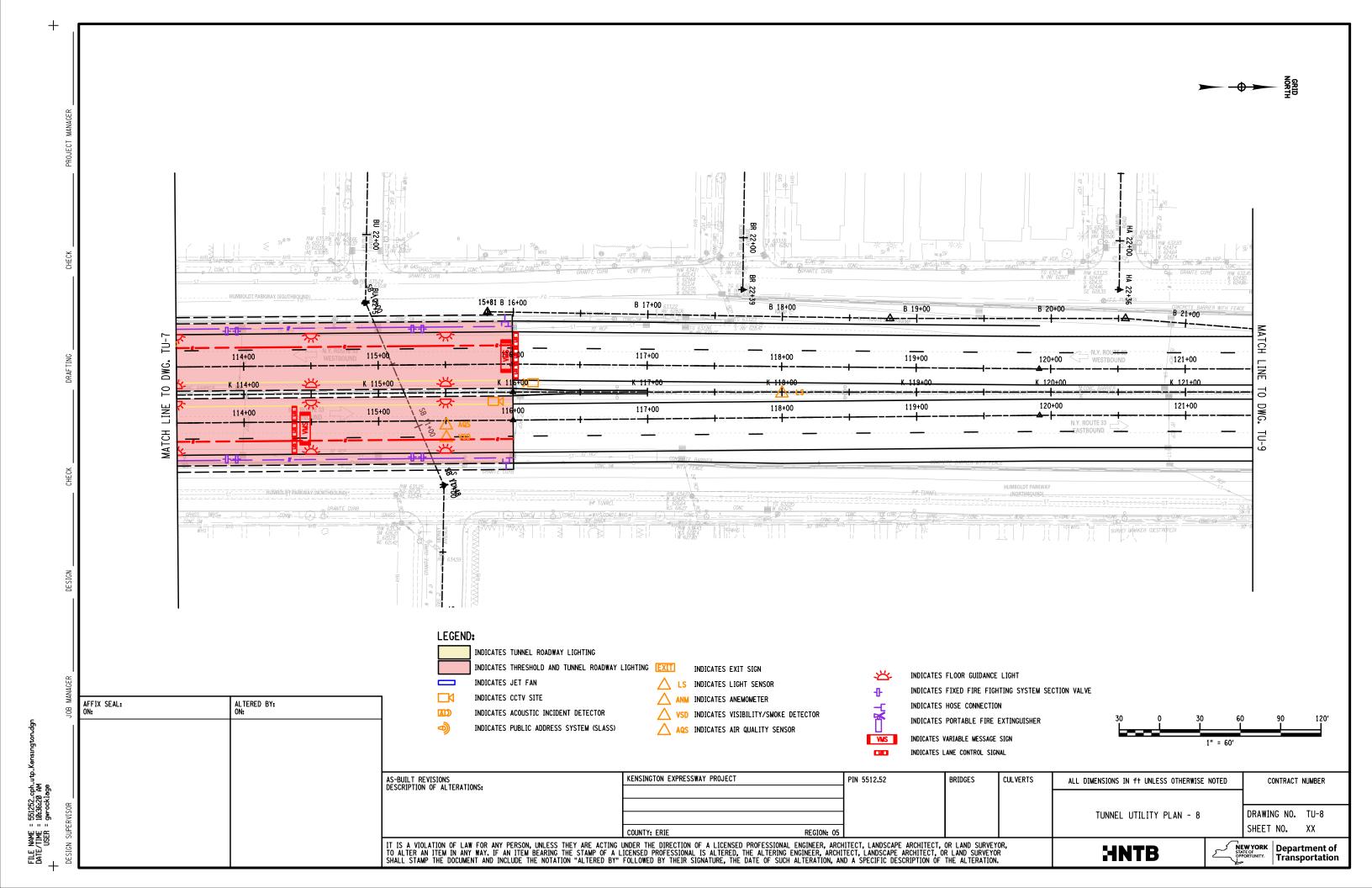


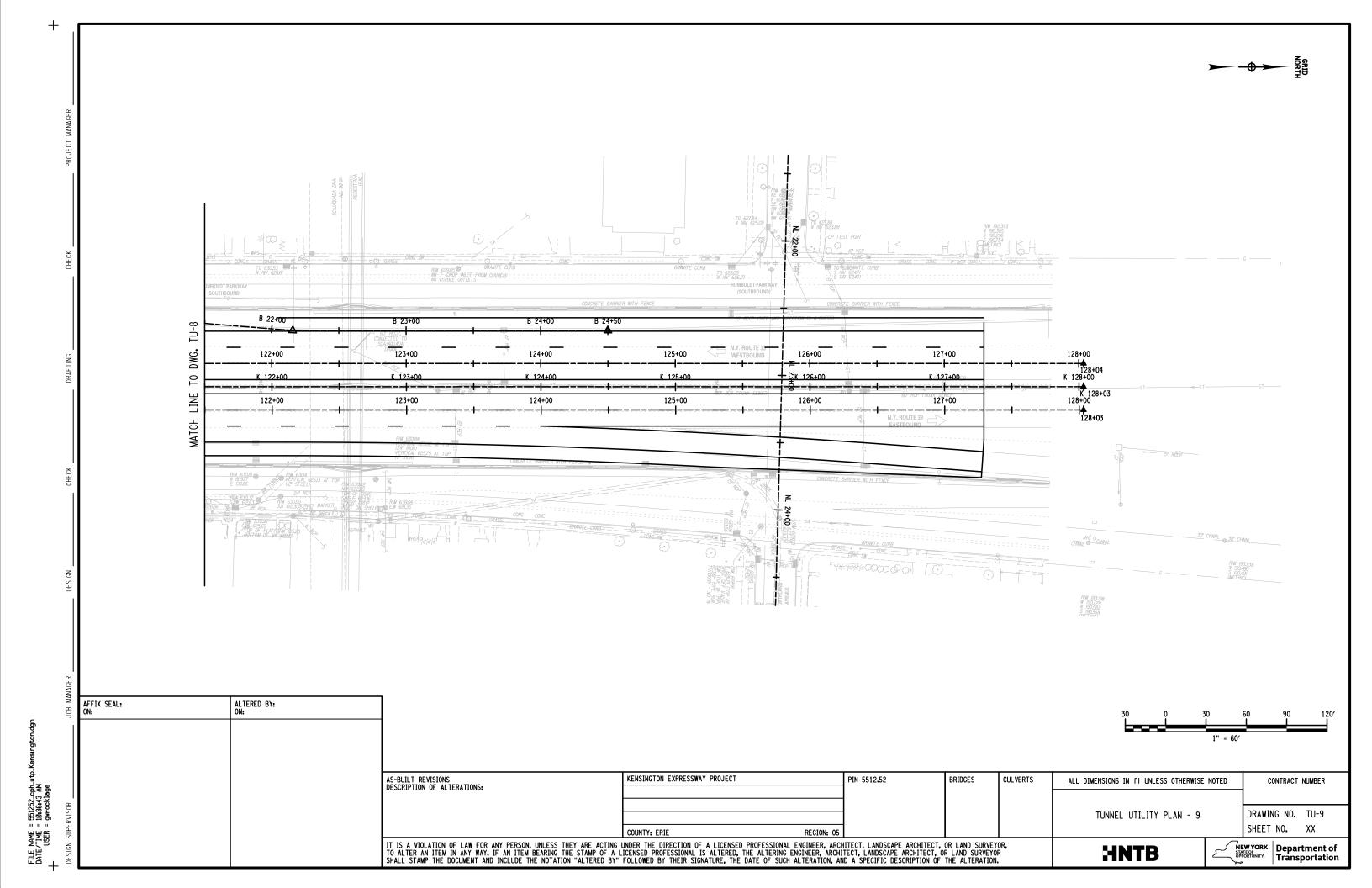


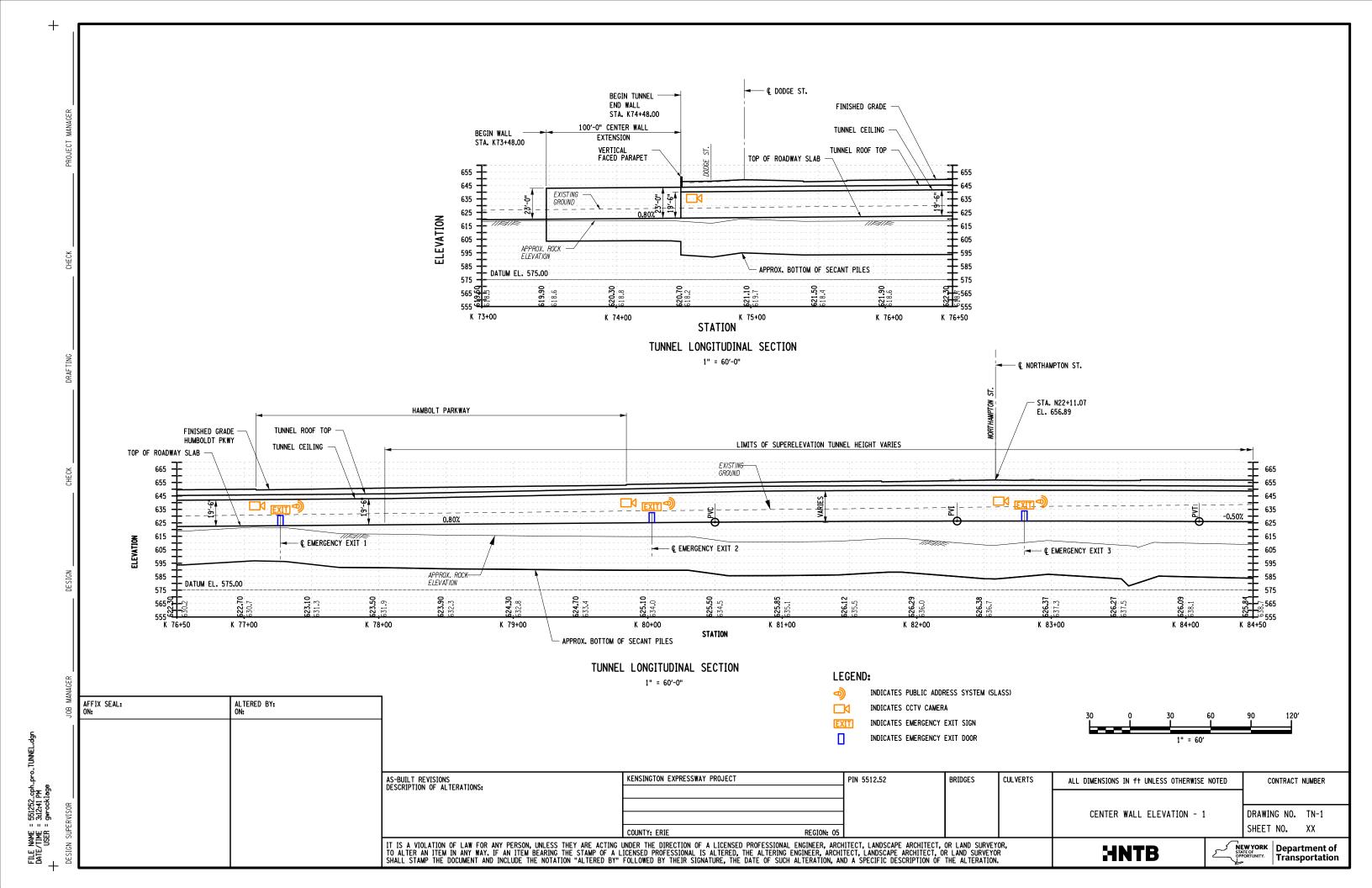


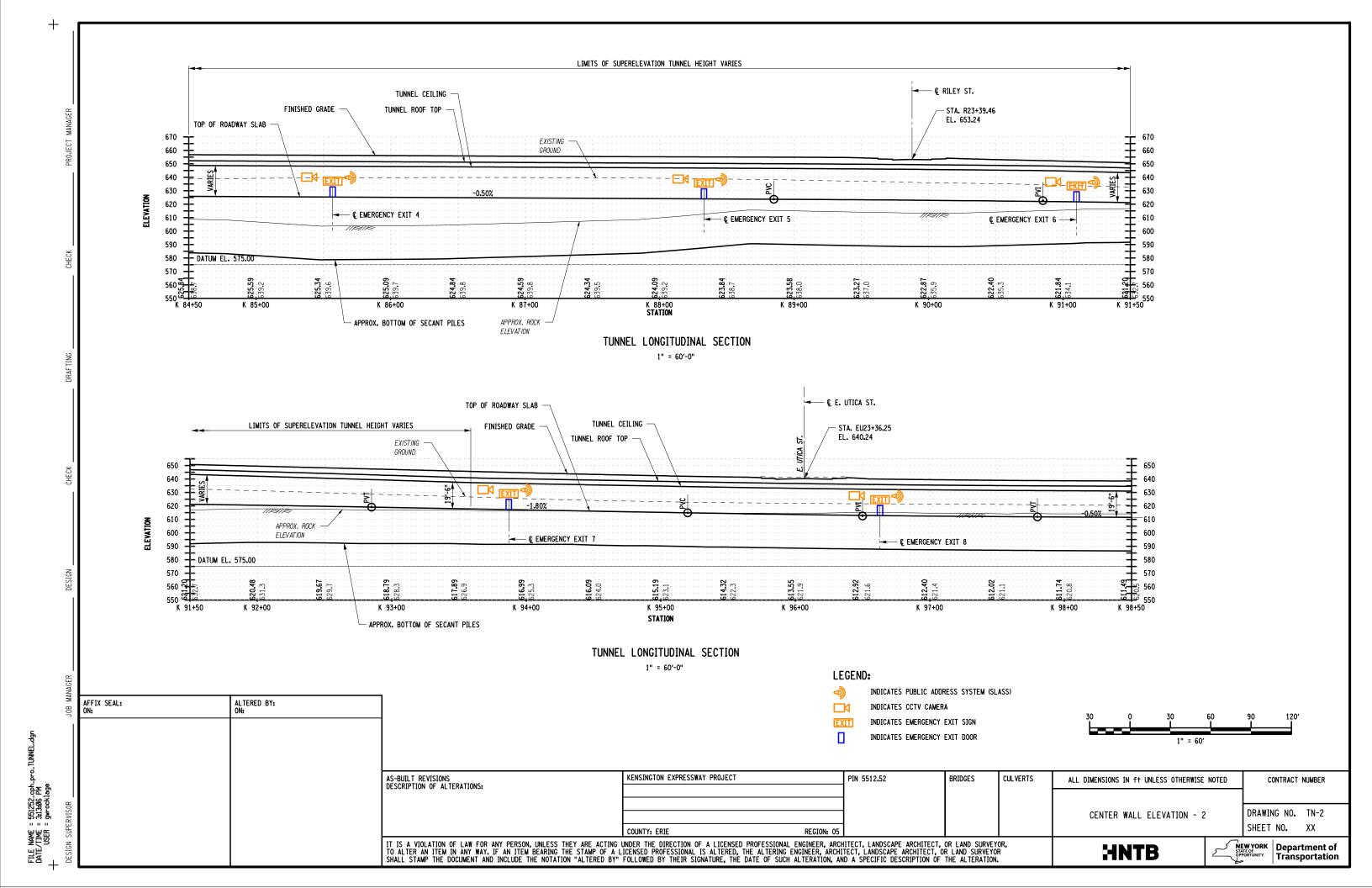


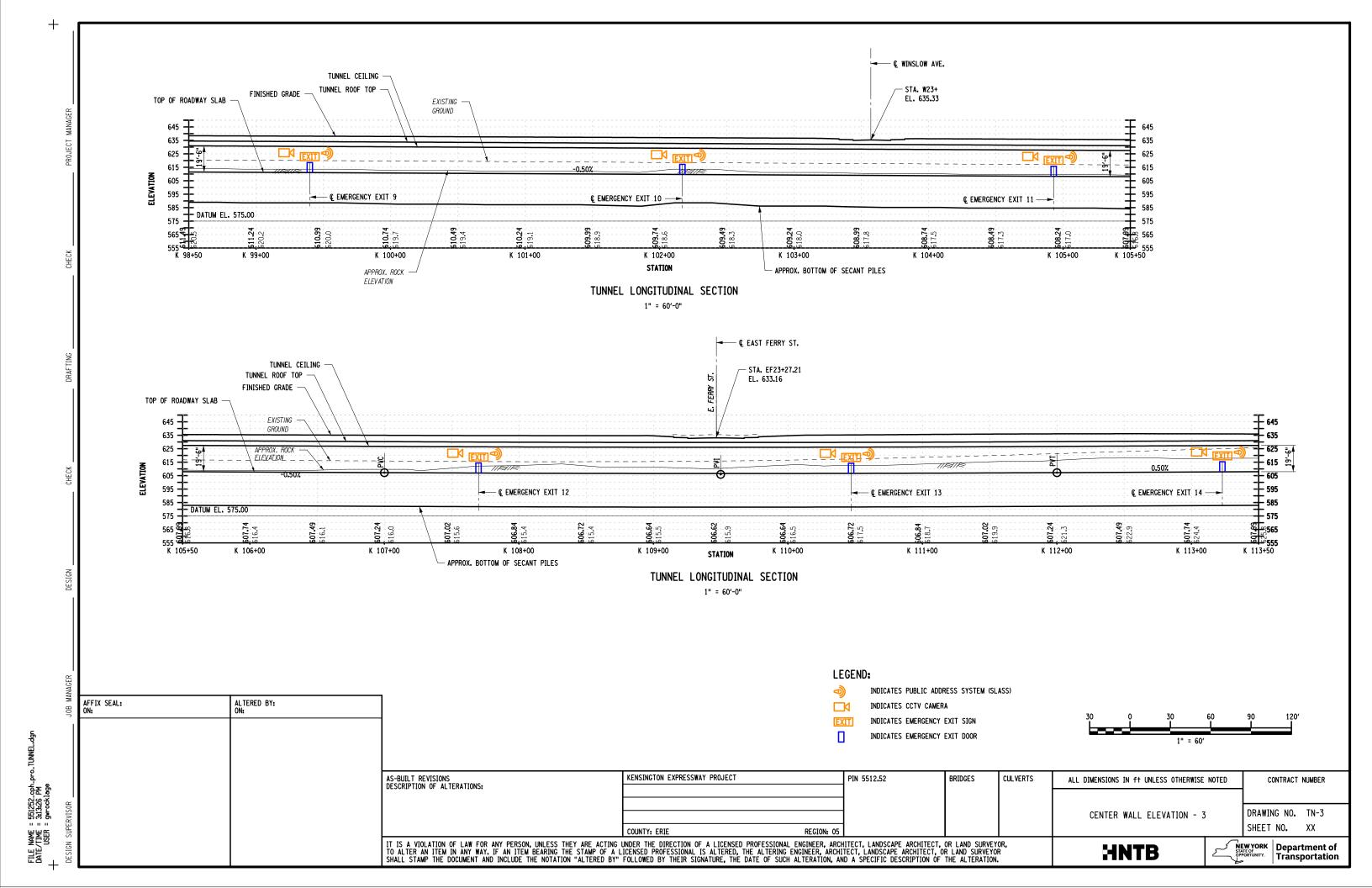


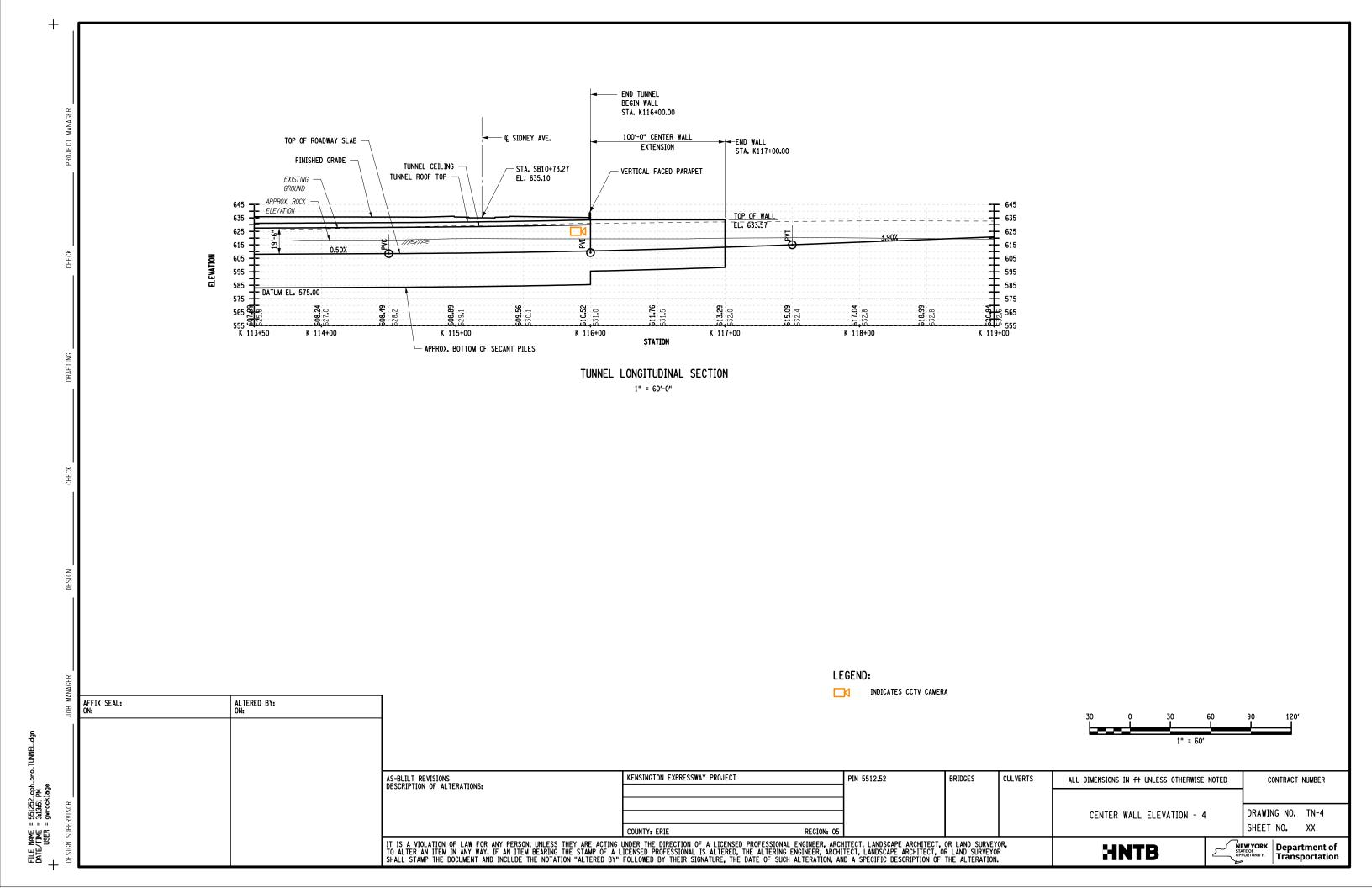


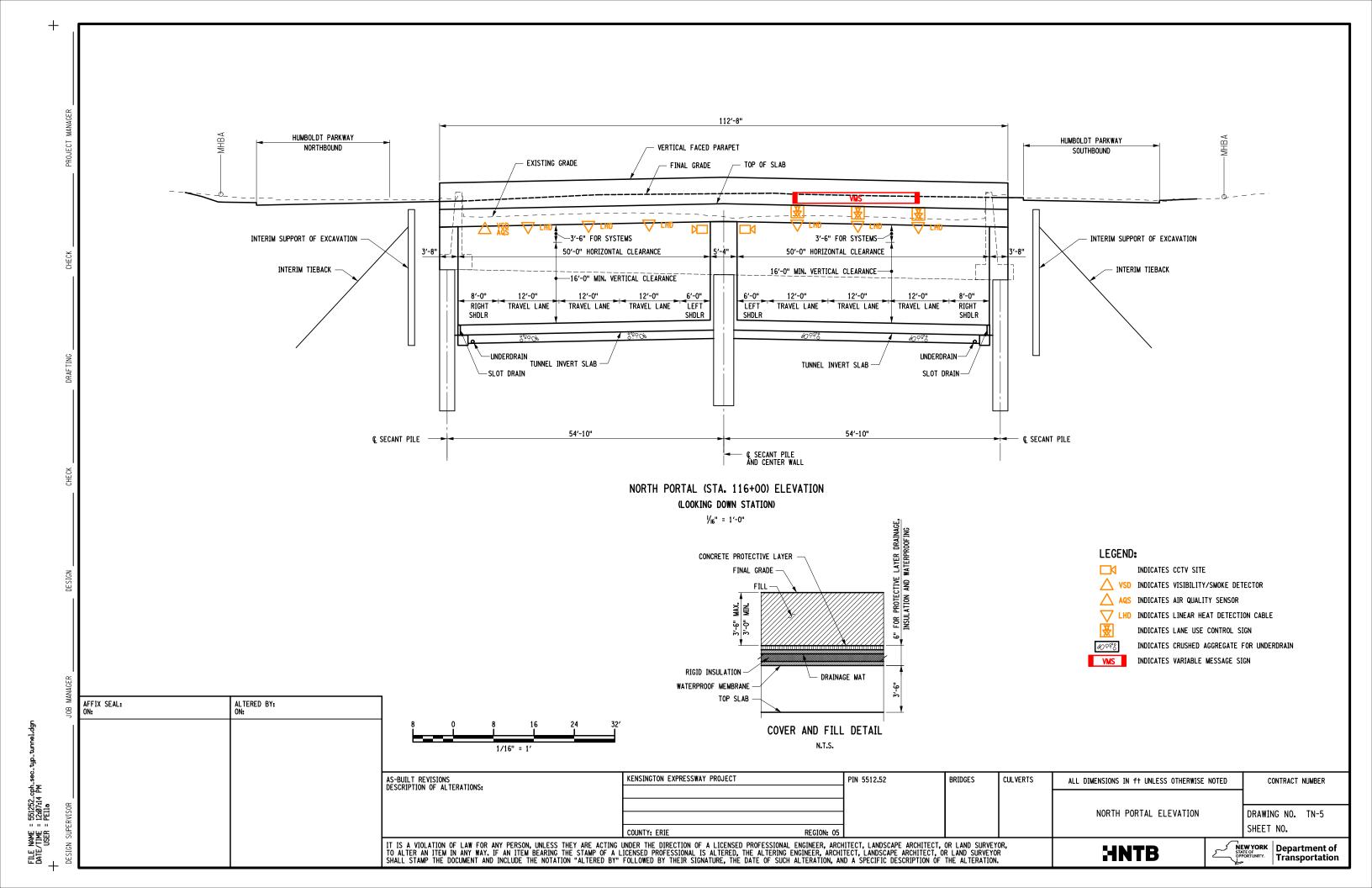


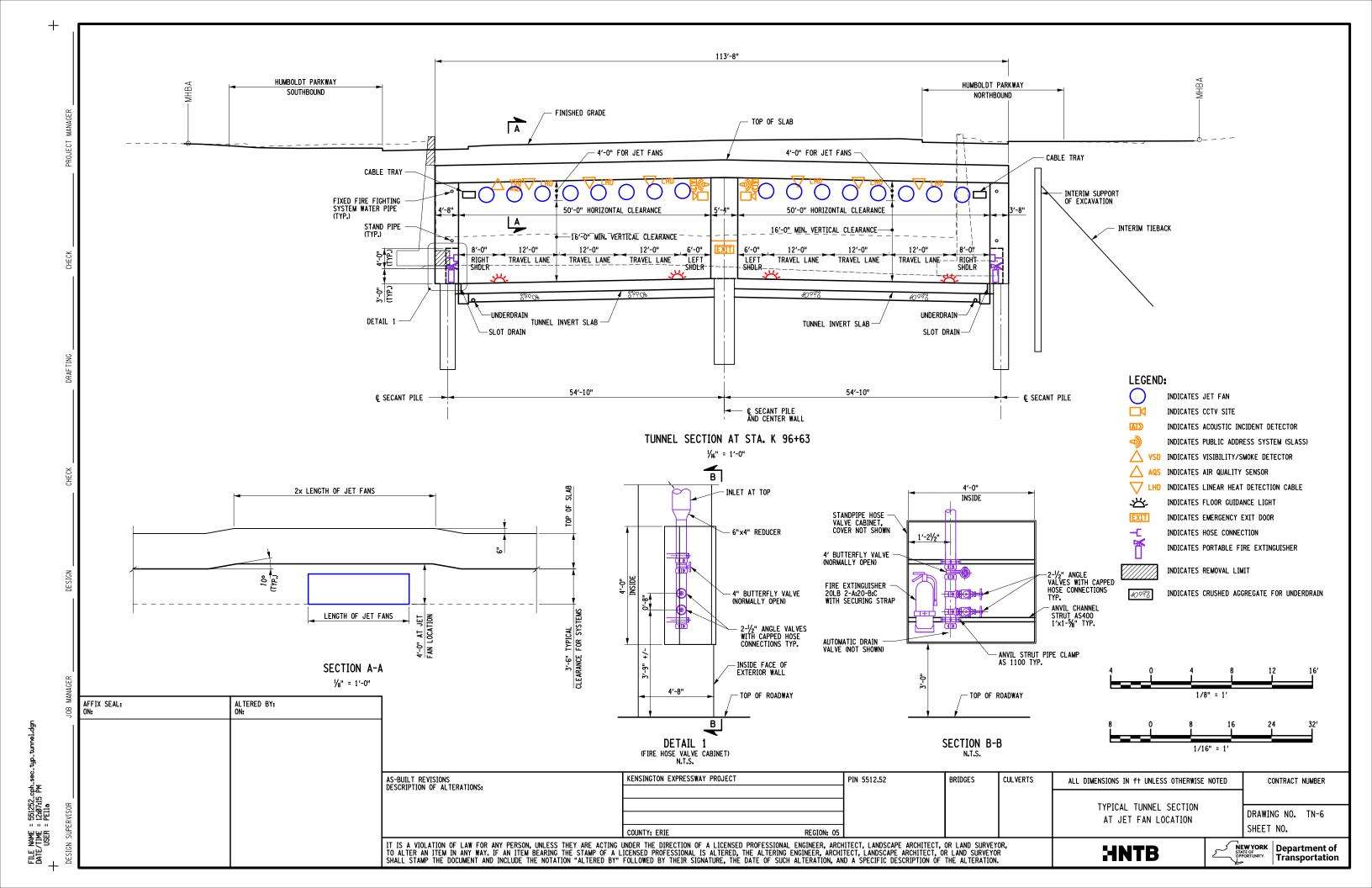


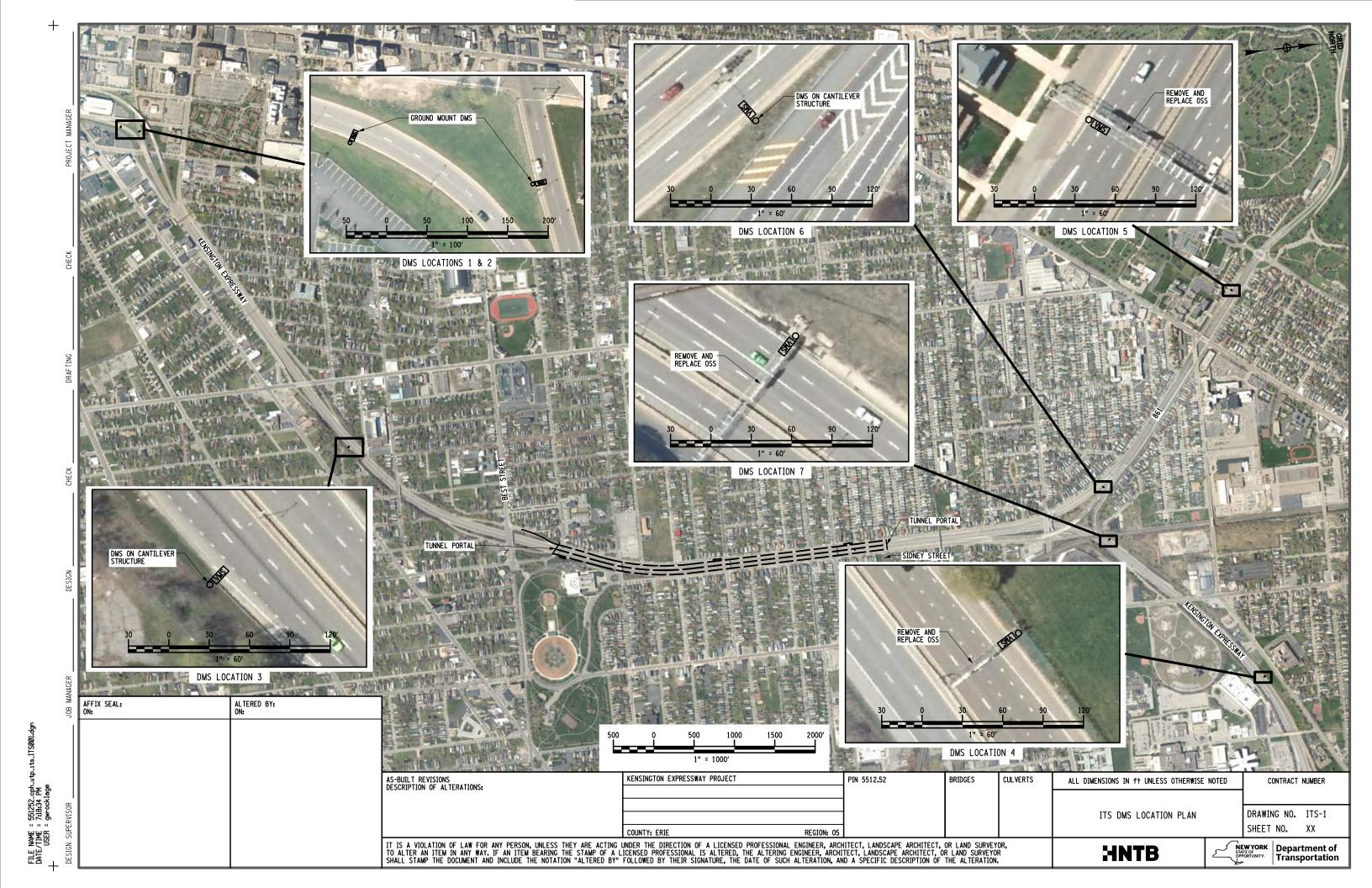












GRID NORTH LINDEN PARK T.G.L. & STA. LINE KENSINGTON EXPY. (WB) T.G.L. & STA. LINE HUMBOLDT T.G.L. & STA. LINE RAMP BE T.G.L. & STA. LINE KENSINGTON EXPY. (EB) T.G.L. & STA. LINE NORWAY PARK T.G.L. & STA. LINE 10TYP. © BRGS. END PIER. STA. B 19+52.93 = STA. K 68+45.48 0 | + RAMP BF T.G.L. & STA. LINE © BRGS. END ABUT. STA. B 20+11.60 © BRGS. BEGIN ABUT. STA. B 18+94.26 — BRIDGE ENDS STA. B 20+14.01 APPR. PAY'T BEGINS STA. B 20+39.01 BRIDGE BEGIN STA. B 18+91.84 APPR. PAV'T ENDS STA. B 18+66.84 T. 21-00 AZ. 368°-53′-16" B 22 WITHIN. 1 APPROX. HIGHWAY BOUNDARY BRIDGE FASCIA - APPROX. HIGHWAY BOUNDARY RAMP BD T.G.L. & STA. LINE © BRGS. TO © BRGS (SPAN 2) BRGS. TO @ BRGS. <u>PL AN</u> AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS: PIN 5512.52 BRIDGES CULVERTS STATE ROUTE 33 ALL DIMENSIONS IN ft UNLESS OTHERWISE NOTED CONTRACT NUMBER = 7/14/2023 R = 2:08:50 PM 1022609 KENSINGTON EXPRESSWAY ST-1 ROADWAY PLAN DRAWING NO. CITY OF BUFFALO SHEET NO. FILE NAME DATE/TIME USER REGION: 5 IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION. NEW YORK STATE OF OPPORTUNITY. Department of Transportation LaBella

